



Hartlepool
Borough Council

Neighbourhoods and Regulatory Services Committee

Agenda

2 March 2026

- Time:** 5pm
- Location:** Council Chamber
- Members:** Neighbourhoods and Regulatory Services Committee
Boddy, Dunbar, Little, Jorgeson, Napper, Riddle (C),
C Wallace (VC)
- Parish Council Co-opted Members:**
S Smith (Greatham Parish Council)
L Noble (Dalton Piercy Parish Council)

-
1. Apologies for absence
 2. To receive any declarations by members
 3. Minutes
 - 3.1. To receive the Minutes and Decision Record of the meeting held on 26 January 2026 (*previously circulated*).
 4. Budget and policy framework
 - 4.1. None



5. Key Decisions

5.1. Urban Forest Master Plan Consultation - *Director of Neighbourhoods and Regulatory Services*

5.2. Local Transport Plan Programme - *Director of Neighbourhoods and Regulatory Services*

5.3. Residential Design Code Supplementary Planning Document (SPD) - *Director of Neighbourhoods and Regulatory Services*

5.4. Planning Obligations Supplementary Planning Document (SPD) and Houses in Multiple Occupation (HMO) Article 4 Direction and SPD update - *Director of Neighbourhoods and Regulatory Services*

6. Other items requiring decision

6.1. Authority's Monitoring Report (AMR) 2024/25 – *Director of Neighbourhoods and Regulatory Services*

7. Items for information

7.1. Selective Licensing Update – *Assistant Director (Regulatory Services)*

8. Any other business which the chair considers urgent

For information

Date of next meeting - to be confirmed.





Neighbourhoods and Regulatory Services Committee

2 March 2026

Report of: Director of Neighbourhoods and Regulatory Services
Subject: URBAN FOREST MASTER PLAN CONSULTATION
Decision Type: Key Decision – NRS 107/26

1. Council Plan Priority

Hartlepool will be a place
where people live healthier, safe and independent lives. (People)
that is connected, sustainable, clean and green. (Place)
that is welcoming with an inclusive and growing economy providing opportunities for all. (Potential)
with a Council that is ambitious, fit for purpose and reflects the diversity of its community. (Organisation)

2. Purpose of Report

2.1. The purpose of this report is to outline the key challenges and targets for achieving tree equity across the borough and seek approval to commence public consultation on the Urban Forest Master Plan (UFMP), see **Appendix 1**.

3. Background

3.1. Hartlepool faces a significant challenge in terms of urban greening, with one of the lowest tree canopy coverages in England and the second-lowest Tree Equity Score nationally. This lack of tree equity means that many communities experience limited access to nature, contributing to potential

environmental and social inequalities such as exposure to air pollution, increased urban heat, and reduced wellbeing.

- 3.2. To address this, the Tree Equity for Hartlepool project, led by Trees for Cities, was launched as a multi-year initiative to close the “nature gap.” The project aims to plant at least 10 hectares of trees in nature-deprived areas and engage 1,500 residents through community planting and environmental activities. This work is supported by a £250,000 grant from the National Lottery Heritage Fund and The Linbury Trust, funding the first two years of what is intended to be a much longer-term transformation.
- 3.3. A key innovation of this project is its use of the Tree Equity Score UK tool on a borough-wide scale, the first time this has been done in the UK. This tool goes beyond measuring canopy cover; it incorporates factors such as heat severity and air quality to identify areas most in need of green investment. This evidence-based approach ensures resources are directed where they will have the greatest impact.
- 3.4. Short-term interventions alone cannot deliver lasting change for the borough. To secure the legacy of this work, the need for a strategic, long-term plan was identified early in the project. The UFMP provides this framework and is funded by the initial grant allocation. The document sets out a clear vision and actionable priorities for the planting, maintenance, and management of Hartlepool’s trees over the coming decades. The UFMP ensures that the benefits of increased tree cover, such as improved health, climate resilience, and biodiversity are sustained well beyond the initial project period.

4. Proposals

4.1. Vision Statement

- 4.1.1 “Hartlepool will be a place where every neighbourhood grows, enjoys and takes pride in its trees.”

4.2. Aim of the Urban Forest Master Plan

- 4.2.1. The Hartlepool UFMP sets out a strong vision which closely aligns with the council’s overall vision including People, Place, Potential and Organisation.
- **People** - A town where everyone can enjoy the benefits of trees: cleaner air, cooler streets and greener spaces and where schools, families and communities feel proud to help plant and care for them.
 - **Place** - Neighbourhoods that feel greener, more welcoming and full of life, with trees along streets, routes and local spaces, especially in the areas that need them most.
 - **Potential** - A long-term treescape that grows stronger every year: supporting wildlife, improving wellbeing, and creating new

opportunities for learning, working, creativity and community involvement.

- **Organisation** - A council that plans, plants and looks after trees in a smart, consistent and future-focused way, making space for nature in new developments and working with partners to get things done

4.2.2 Hartlepool has one of the lowest tree canopy covers in the UK, and the UFMP aims to change that. It seeks to embed trees into the very fabric of the borough: its streets, schools, parks, and developments, so that they become a defining feature of Hartlepool's identity. The ambition is not just about planting trees; it is about creating a treescape that grows stronger year after year, improving air quality, cooling streets, supporting wildlife, and enhancing wellbeing

4.3. How It will Be Delivered

4.3.1 Delivery will be guided by three interconnected pillars: Tree and Woodland Structure, Community Framework, and Sustainable Resource Management. These pillars guide our actions: improving canopy cover and tree diversity, engaging communities and businesses in planting and care, and building strong systems for governance, funding, and data to ensure lasting success. The masterplan seeks collaboration across Council departments, local organisations, and private landowners

4.3.2 Regular reviews of the masterplan, every five years will keep progress on track, while phased actions will prioritise areas with the greatest need. This is a masterplan designed not only to plant trees but to nurture them, protect them, and make them a permanent part of Hartlepool's future.

4.4. How It Will Be Used by Everyone

4.4.1 The masterplan is more than a council document; it is a shared vision for the whole community. For the council and its partners, it provides a strategic framework for planning, development, and resource allocation. For residents, schools, and volunteers, it offers opportunities to get involved through planting, caring for trees, and shaping local green spaces. Businesses and landowners will find clear standards and incentives for tree-friendly practices, while the public will have access to interactive tools such as a web-based tree map showing canopy cover, benefits, and opportunities for engagement

4.4.2 By working together, Hartlepool can create neighbourhoods that feel greener, and more welcoming. This is about changing the culture of the borough, moving from one of the lowest tree covers in the country to a national example of the journey to urban greening.

4.5 Initial Consultation to Inform the Urban Forest Master Plan

- 4.5.1 As part of the Tree Equity for Hartlepool project, a community survey was conducted to gather local views on trees and green spaces. The consultation received 579 responses specific to the UFMP, providing valuable insight into residents' priorities and aspirations. This feedback ensures the UFMP reflects community needs alongside environmental objectives, creating a masterplan that is inclusive and supported by the people of Hartlepool.
- 4.5.2 The survey revealed strong support for planting significantly more trees across the borough, with a preference for native species, fruit trees, and a diverse mix of varieties. Respondents also highlighted concerns about long-term maintenance, calling for regular pruning, health checks, and dedicated care teams. Location was another key theme, with suggestions for planting in urban areas, deprived communities, school grounds, and new developments, as well as creating wildlife corridors and community orchards.
- 4.5.3 Community involvement emerged as a priority, with enthusiasm for school engagement, volunteer programs, and creative initiatives such as tree adoption schemes and naming competitions. There was also a clear desire for stronger protection measures, including robust Tree Preservation Orders (TPOs), developer accountability, and planning conditions to safeguard trees. Finally, respondents emphasised the environmental and social benefits of trees, recognising their role in improving air quality, mental health, biodiversity, climate resilience, and even food access through fruit trees.
- 4.5.4 This consultation provides a strong foundation for the UFMP, ensuring that future actions align with both technical priorities and the values of the community.

4.6 Proposed Public Consultation

- 4.6.1 Partners have already carried out an initial consultation to gather community views and shape the early direction of the UFMP. This consultation gives residents, schools, businesses, and partners the chance to review the draft vision, aims, and proposed actions, and confirm that they reflect local priorities. It is an opportunity to validate the masterplan, identify any gaps or adjustments before final endorsement, and strengthen community ownership by involving people in the final stage of shaping Hartlepool's treescape. By doing this, the council builds confidence and transparency, showing that the UFMP is a shared vision, not just a council-led initiative. This step is critical to delivering a masterplan that is practical, inclusive, and future-focused.

5. Other Considerations/Implications

<p>Risk Implications</p>	<p>Failure to implement the UFMP and its associated public consultation carries significant strategic, financial, environmental, and reputational risks for the Council and the wider community.</p> <p>Without an adopted masterplan, tree-related priorities will hold less weight in planning and development decisions, weakening alignment with the council's overarching vision for People, Place, Potential, and Organisation. This lack of strategic direction will also limit Hartlepool's ability to secure external funding, as many grant programs require a formal, community-backed masterplan.</p> <p>In summary, not delivering the UFMP and its consultation process would result in diminished decision-making influence, missed funding opportunities, lack of community ownership, and environmental decline,</p>
<p>Financial Considerations</p>	<p>The development of the UFMP has been funded through the Tree Equity for Hartlepool project, supported by a grant from the National Lottery Heritage Fund.</p> <p>The delivery of actions identified within the UFMP will require future funding and resource commitments. These actions, such as large-scale planting, maintenance programs, community engagement initiatives, and monitoring systems must be fully costed to ensure feasibility and sustainability. Decisions regarding financial allocations will be subject to consideration and approval by a future council committee.</p> <p>The adoption of the UFMP will support the Council and its communities in any future grant funding applications.</p>
<p>Subsidy Control</p>	<p>N/A</p>
<p>Legal Considerations</p>	<p>The Environment Act 2021 established a legal duty for England to increase tree canopy and woodland cover to at least 16.5% of land area</p>

	<p>by 2050, a target set by the Environmental Targets (Woodland and Trees Outside Woodland) Regulations 2023. Hartlepool's tree canopy cover is 9.8% (2024).</p> <p>Town and Country Planning Act 1990 plays a crucial role in tree protection through several key sections, primarily focusing on Tree Preservation Orders (TPOs), planning conditions and obligations, conservation areas, and enforcement mechanisms.</p>
<p>Single Impact Assessment</p>	<p>See Appendix 2</p>
<p>Staff Considerations</p>	<p>The work on the UFMP has been delivered by Council officers in partnership with external consultants, Treeconomics. The upcoming public consultation will be managed by the council to ensure transparency and community engagement.</p> <p>Implementation and delivery of the UFMP will primarily be the responsibility of HBC. However, the masterplan is designed as a shared vision for the whole of Hartlepool, meaning that some actions can be undertaken by residents, community groups, and businesses. This collaborative approach reflects the ambition to embed trees into every aspect of Hartlepool's identity.</p> <p>Internally, trees within HBC fall under the remit of several different departments and teams. This became clear during internal discussions and highlights the need for strong coordination and clear accountability. Ownership of specific actions will rest with designated service areas to oversee progress and ensure delivery.</p>
<p>Asset Management Considerations</p>	<p>Trees owned by the council are a vital Hartlepool Borough Council asset, while the trees across Hartlepool belong to everyone; they are a shared community asset. The most recent borough-wide tree survey identified that trees managed by Hartlepool Borough Council across highways, parks, cemeteries, and open spaces have an estimated value of £288 million,</p>

	<p>calculated using the Capital Asset Value for Amenity Trees (CAVAT) methodology. CAVAT is a nationally recognised system that assigns a monetary value to urban trees, treating them as vital public assets for purposes such as compensation claims, strategic urban forestry, and long-term asset management.</p> <p>Trees are living infrastructure that grow, change, and if neglected, decline over time. This dynamic nature means they require proactive management, adequate funding, and skilled resources to ensure safety and sustainability. Trees are unique in that they not only represent financial value but also deliver benefits such as: cleaner air, cooler streets, biodiversity, and improved wellbeing. Yet, they also carry inherent risks if unmanaged, including structural failure that can lead to property damage or, in the most serious cases, loss of life.</p> <p>What makes tree assets different to other assets is its potential for growth. Unlike most council assets that depreciate over time, the value of trees increases as they mature.</p>
<p>Environment, Sustainability and Climate Change Considerations</p>	<p>Trees provide numerous benefits for the environment and are vital to reducing the impact of climate change. They absorb carbon, cool our streets, reduce flood risk, and filter harmful pollutants from the air. They create wildlife corridors, boost biodiversity, and improve mental and physical wellbeing. The overall contribution of trees to urban and rural environments cannot be overstated, making their protection and expansion essential for a sustainable future.</p>
<p>Consultation</p>	<p>The development of the UFMP has been shaped by extensive internal and external engagement to ensure it reflects both technical priorities and community aspirations.</p> <p>(a) Internal Consultation with Elected Members Councillors were engaged through a dedicated members seminar, providing an opportunity to review the vision, aims, and strategic direction of the UFMP.</p>

	<p>(b) Internal and Cross-Departmental Officer Engagement Council officers from multiple departments contributed throughout the process, ensuring the masterplan aligns with operational realities and service priorities. A dedicated working group brought together key expertise, including:</p> <ul style="list-style-type: none"> • Arboricultural Officer • Ecologist • Horticulture Operations Officer • Horticulture Technical Officer • Parks and Open Spaces Team Leader • Public Rights of Way Officer • Team Leader – Cleansing & Grounds Maintenance • Team Leader – Highways Asset Management • Senior Urban Forest Consultant – Treeconomics • Urban Forest Consultant – Treeconomics • Tree Equity for Hartlepool Project Manager – Trees for Cities • Tree Equity for Hartlepool Project Coordinator – Trees for Cities • Senior Development Manager – Trees for Cities • Tree Equity Programme Officer – Woodland Trust <p>(c) External Stakeholder Engagement Key partners, including parish councils, social housing providers, charities, and community groups, were consulted through a dedicated workshop day to gather insights and strengthen collaboration.</p> <p>(d) Public Consultation An initial public consultation was conducted via an online survey at the start of the project, capturing 579 responses.</p>
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6. Recommendations

- 6.1. It is recommended that Neighbourhoods and Regulatory Services Committee notes the content of this report and grants permission to commence public consultation on the UFMP for an eight-week period commencing in Spring

2026. The outcome of this consultation will be reported to a future Committee and prior to the UFMP being adop.

7. Reasons for Recommendations

7.1. The proposed public consultation on the UFMP is essential to ensure the plan is credible and deliverable. Consultation will strengthen community ownership, ensure transparency and allow external stakeholders to buy into the masterplan's aims.

8. Background Papers

- 8.1. Draft Urban Forest Master Plan 2025 – 2050
- 8.2. Hartlepool Regeneration Plan 2026 - 2036
- 8.3. Hartlepool Net Zero Strategy and Action Plan 2023
- 8.4. Hartlepool Tree Strategy 2020 – 2030
- 8.5. Hartlepool Tree Strategy 2011 – 2016
- 8.6. Hartlepool Tree Risk Evaluation and Management Policy 2014
- 8.7. Hartlepool Trees and Development Guidelines Supplementary Planning Document 2013

9. Contact Officers

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Sign Off:-

Chief Executive	Date: 23/01/2026
Director of Finance, IT and Digital	Date: 23/01/2026
Director of Legal, Governance and HR	Date: 23/01/2026

TREE EQUITY FOR HARTLEPOOL

Hartlepool's Urban Forest Master Plan 2026 - 2050



Hartlepool
Borough Council

Acknowledgements

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Elwick Parish Council
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Greatham Parish Council
Rural Plan Working Group
Tees Community Forest
The Greatham Foundation
Thirteen Housing Group
Hartlepool Civic Society
Woodland Trust
Hartlepool Borough Councillors

Foreword

Hartlepool is a town shaped by strong communities and a sense of pride in where we live. Over recent months, we've heard a clear message from residents, schools and local groups: we want a greener town, with more trees in the places we walk, live and spend time. This Master Plan reflects our shared ambition that Hartlepool will be a place that is connected, sustainable, clean and green.

This Urban Forest Master Plan sets out how we can make that vision real. It brings together ideas shared across the borough – planting trees where they're needed most, giving young people a genuine role in shaping their surroundings, and creating green routes that connect neighbourhoods. It reflects a shared belief that trees make our town healthier, more welcoming and more hopeful.

The plan also focuses on what it takes to make change last. Planting is only the beginning. Choosing the right species, caring for young trees, protecting what we already have and making space for nature in new developments will help create a treescape that grows stronger year after year.

Everyone can play a part – schools, families, volunteers, community groups and local businesses. Through planting, caring or simply enjoying greener streets, we can help nurture something that benefits us all.

By working together, we can grow a brighter, greener future for Hartlepool.

Kieran Bostock

Director for Neighbourhood and Regulatory Services
Hartlepool Borough Council



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1. Introduction

1.1 Background

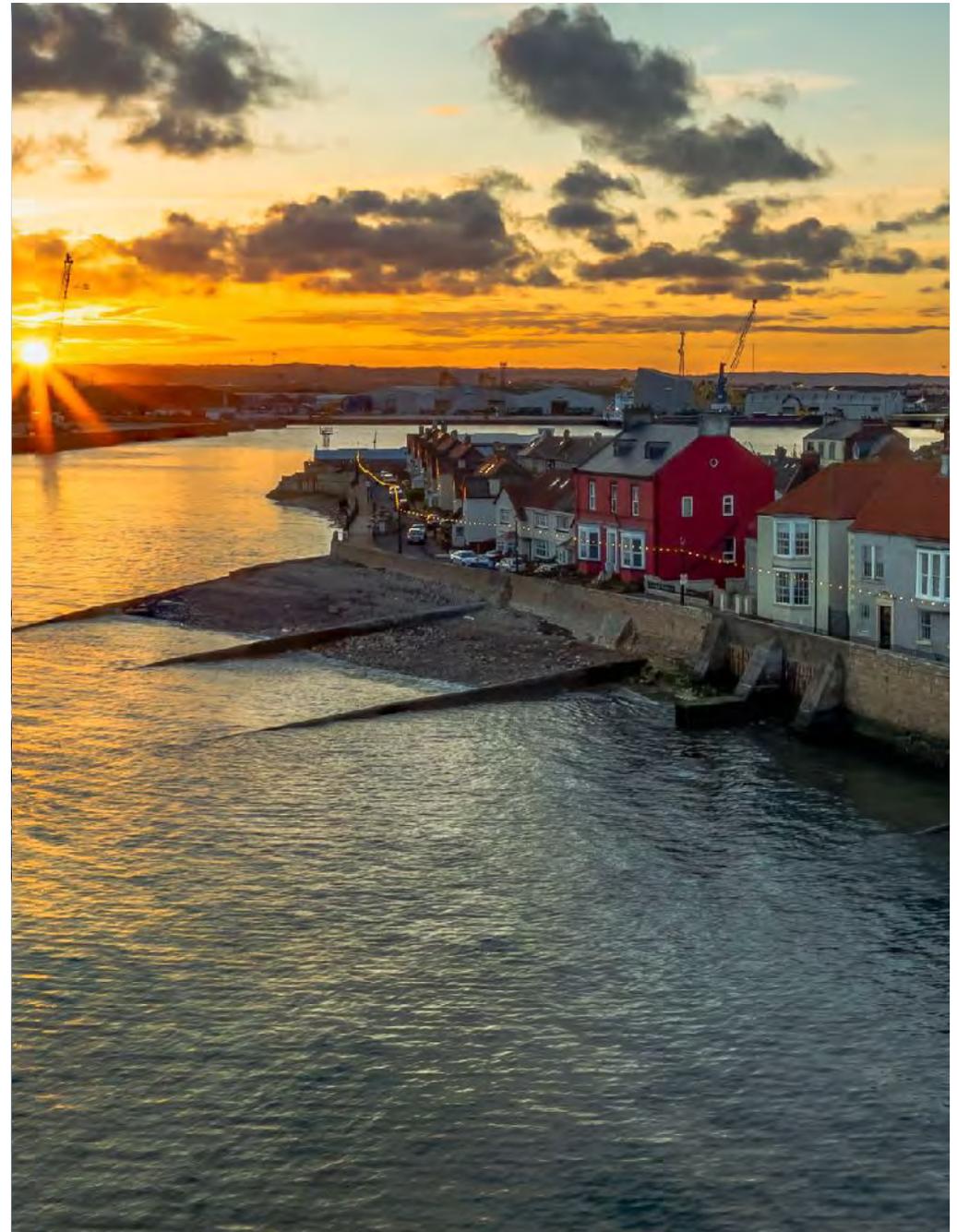
Hartlepool sits right on the North East coast, and its history has shaped the town you see today. From shipbuilding to heavy industry, the past has left its mark on the way the town looks and feels. But while Hartlepool's identity is tied to the sea and its industrial heritage, there's growing interest in something that's been missing for a long time: trees.

Right now, Hartlepool has one of the lowest amounts of tree cover in the whole country. In some neighbourhoods, only around 2% of the area has trees. That lack of green space affects everything from the quality of the air you breathe to how hot streets get in summer, and even how pleasant an area feels to live and work in.

To tackle this, the **Tree Equity for Hartlepool** project began in February 2025. Its goal is simple: understand what trees the town already has, figure out where they're needed most, and make a proper plan to improve things. It's the first time anywhere in the UK has used the Tree Equity Score across an entire town, helping show exactly which areas would benefit most from new trees.

This Urban Forest Master Plan builds on that work. It sets out how Hartlepool can grow and look after its urban forest over the long term, making trees a normal part of how the town is planned, managed, and improved. The benefits are real: cleaner air, cooler streets, more wildlife, and greener neighbourhoods for everyone.

The plan has been put together by Hartlepool Borough Council, Trees for Cities, and several local and national partners. Working together, the aim is to create a Hartlepool that's greener, healthier, and better for the people who live here now — and for those who'll call it home in the future.



1.2 Benefits of Trees

Most of us know trees are “a good thing,” but the benefits they bring are way bigger than people often realise. In environmental work, these benefits are called **ecosystem services**—basically, all the ways nature quietly supports our lives. Scientists across the world have mapped out over a hundred different ecosystem services, and they’re still working out exactly how to measure and value them.

Trees are a huge part of this. They help create healthier, more resilient places for both people and wildlife. On a day-to-day level, they improve the air we breathe by soaking up pollutants, help cool down streets during hot weather, and slow the flow of rainwater so our towns are less likely to flood. They also make neighbourhoods look and feel better, give people places to relax, and support our mental health.

Research continues to show even more benefits from trees—many of them linked to quality of life. These include things like boosting house prices, improving local amenities, and reducing pressure on health services.

Here are some of the key impacts:

Climate adaptation – cooling our towns: Trees can help cut peak summer temperatures by up to 7°C¹, which is why hotter countries have relied on green infrastructure for years. Current research is even comparing which tree species are the best at cooling their surroundings.²

Noise reduction: Trees, hedges, and woodlands act like natural sound barriers. They help block out noise from roads, industry, and busy urban areas, making neighbourhoods quieter and more pleasant.

Higher property and rental values: International studies show that having more trees nearby can increase property prices by 5% to 18%.³

Boosted local spending: People tend to spend more money—around **9% to 12% more**—in shopping areas that have big, well-looked-after trees. It simply feels nicer to be there.⁴

Better mental health: Forest Research has valued the mental-health benefits of trees and woodlands across the UK at **£185 million** a year. Living near more trees (within about 100 metres) is linked to a lower chance of being prescribed antidepressants, especially for people in more deprived areas.⁵

Improved focus and school performance: Schools with more greenery consistently see better academic results. Middle-school students especially benefit when trees are planted within **250 metres** of the school.⁶

Faster recovery from illness: Patients recovering in hospital rooms with a view of nature tend to get better sooner than those looking out onto buildings.⁷

Green infrastructure (GI) is the planned network of green and natural spaces that run through a town or city. This includes parks, woodlands, rivers, and even features like green roofs and living walls. Together, these spaces work alongside roads, buildings, and other “grey” infrastructure to support everyday life. By planning these green spaces as a connected system, towns and cities can become more resilient, healthier, and better places to live and work.

¹ Doick, K. Hutchings, T. (2012)

² Oliveira, J.D.D, Biondi, D. And Reis, A.R.N.D. (2022)

³ Wolf, K.L. (2007)

⁴ Wolf, K.L. (2005)

⁵ Forest Research. (2021)

⁶ Kuo, M., et al. (2021)

⁷ Ulrich. R. (1984)

1.3 Today's Trees and Woodlands

Hartlepool's urban forest is a big part of what makes the town feel alive, even if most people don't think about it very often. It includes all the trees, woodlands, hedges, shrubs, and bits of greenery that you see around the town. Together, they play a huge role in keeping Hartlepool healthier, greener, and better prepared for the effects of climate change.

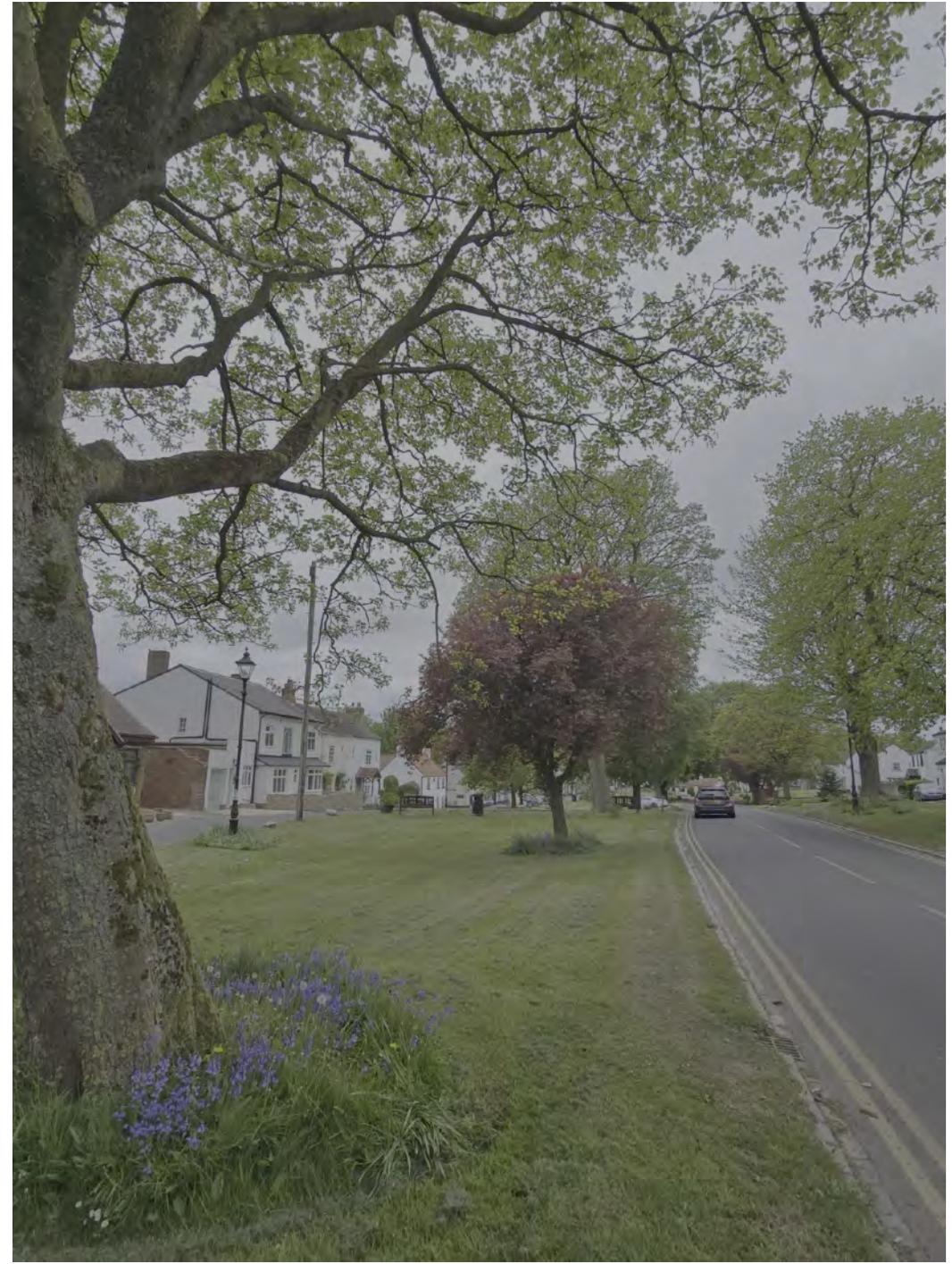
Right now, Hartlepool's tree canopy cover is **9.8% (2024)**. That's lower than the levels recommended for towns like ours. Forest Research suggests at least **15%** tree cover for coastal towns and **20%** for most urban areas. It's also far below the **30%** recommended by the 3-30-300 rule, which is used around the world as a guide for creating greener, fairer places to live.

This Urban Forest Masterplan is designed to change that. It sets out a long-term plan for growing Hartlepool's tree cover, improving green spaces, and making sure trees are spread more evenly across all neighbourhoods—not just a lucky few. The plan highlights actions such as:

- Finding the areas that need trees the most
- Focusing planting in places with low canopy cover
- Looking after the trees we already have
- Making sure new trees are planted, maintained, and protected properly

At the heart of this Masterplan is a simple ambition: to shift Hartlepool from reacting to problems to actively planning for a greener future. That means the council, local organisations, private landowners, and residents all working together to make urban forestry a priority.

By committing to a greener Hartlepool, the town can improve people's health, tackle climate challenges, and make sure everyone—no matter where they live or what their circumstances are—benefits from the shade, beauty, and wellbeing that trees bring.



1.4 Using This Document

This document sets out a straightforward plan for how Hartlepool can strengthen and grow its urban forest. It outlines how we can better understand the trees and woodlands we already have, build strong partnerships across both the public and private sectors, and make sure that priorities and resources are managed fairly and sustainably. To keep things simple and easy to follow, the targets, priorities, and actions are grouped into three main sections:

- **Tree and Woodland Structure** – understanding what we have and what needs improving
- **Community Framework** – involving local people, organisations, and partners
- **Sustainable Resource Management** – making sure the work is achievable, long-lasting, and cost-effective

3.1 Targets, Priorities & Actions: Tree and Woodland Structure

T1. Tree Canopy Cover

Tree canopy cover is the amount of ground shaded by the leaves and branches of trees when viewed from above. It doesn't show the full health or diversity of the urban forest, but it's a useful way to track how tree cover is changing over time.

Data from 2024 (Google Environmental Insights Explorer) shows that Hartlepool's average canopy cover is **9.4%**. Forest Research recommends at least 15% for coastal towns and cities, so there is a clear gap to bridge. Because trees grow slowly—and some will be lost over time to age, disease, or removal—it's important to set realistic milestones to make sure long-term canopy targets can be reached.

Some parts of Hartlepool have canopy cover as low as 3.4%, although areas such as Seaton Common National Nature Reserve or industrial land naturally have limitations on where trees can be planted.



Figure 1. Canopy cover across Hartlepool

Policy Links

Actions

1. Carry out a detailed canopy cover assessment (i-Tree Eco) to establish accurate potential canopy cover and the amount of tree cover also provided by woodland and hedges.
2. Agree a long-term borough-wide target for tree canopy cover to include all land public and private.
3. Review every 5 years by carrying out a canopy cover assessment.

Resp

Review

Priority	Key Performance Indicators showing current position			
	Low	Moderate	Good	Optimal
	The existing canopy cover equals 0-25% of the potential.	The existing canopy cover equals 25-50% of the potential.	The existing canopy cover equals 50-75% of the potential.	The existing canopy cover equals 75-100% of the potential.

Today's position (2025)

Future position (2035)

Name of the Target

Explanation

Links to other policy areas, to show how trees have a role to play

Priority* of performance indicator

High, Medium, Low

Supporting image

Actions to be undertaken

Who will make sure this is done

When we check progress

Key Performance Indicators

*Note: Whilst in theory, 'Optimal' is the desirable state for each target, the goal for each has been determined, taking into account the limitations presented by time and resources. One consequence of this is the prioritisation of some activities over others, which manifests itself as a lower target for lower priority areas.

2.

Vision

2.1 A Vision for the Trees of Hartlepool

This section shows how the vision for the trees of Hartlepool is drawn from the overall vision within the Council Plan 2025

2.1 Vision & Delivery

Hartlepool will be

PEOPLE

... a place where people live healthier, safe and independent lives.

PLACE

... a place that is connected, sustainable, clean and green town.

POTENTIAL

... a place that is welcoming with an inclusive and growing economy providing opportunities for all.

ORGANISATION

... a place with a Council that is ambitious, fit for purpose and reflects the diversity of its community.

A town where everyone can enjoy the benefits of trees—cleaner air, cooler streets and greener spaces—and where schools, families and communities feel proud to help plant and care for them.

Neighbourhoods that feel greener, more welcoming and full of life, with trees along streets, routes and local spaces, especially in the areas that need them most.

A long-term treescape that grows stronger every year—supporting wildlife, improving wellbeing, and creating new opportunities for learning, working, creativity and community involvement.

A council that plans, plants and looks after trees in a smart, consistent and future-focused way, making space for nature in new developments and working with partners to get things done.

... a place where **every** neighbourhood grows, enjoys and takes pride in its trees.

3.

Targets, Priorities and Actions

3.1

Tree and Woodland Structure

This section looks at the trees and woodlands we have today. It focuses on what the urban forest is made of, rather than how it is managed. Guidance on the management, care, and long-term maintenance of these trees is provided in a later section.

3.1 Targets, Priorities & Actions: Tree and Woodland Structure

T1. Tree Canopy Cover

Tree canopy cover is the amount of ground shaded by the leaves and branches of trees when viewed from above. It doesn't show the full health or diversity of the urban forest, but it's a useful way to track how tree cover is changing over time.

Data from 2024 (Google Environmental Insights Explorer) shows that Hartlepool's average canopy cover is **9.4%**. Forest Research recommends at least **15%** for coastal towns and cities, so there is a clear gap to bridge. Because trees grow slowly—and some will be lost over time to age, disease, or removal—it's important to set realistic milestones to make sure long-term canopy targets can be reached.

Some parts of Hartlepool have canopy cover as low as **3.4%**, although areas such as Seaton Common National Nature Reserve or industrial land naturally have limitations on where trees can be planted.

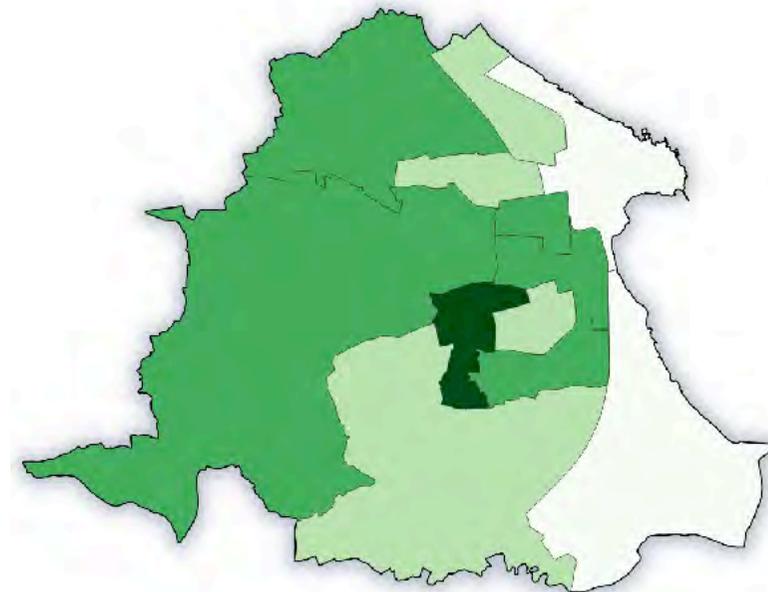


Figure 1. Canopy cover across Hartlepool

Policy Links	Actions	Resp	Review
Pride in Place Hartlepool Regeneration Plan 2026-2036, Environmental Targets (W & ToW) Regulations 2023	<ol style="list-style-type: none"> 1. Carry out a detailed canopy cover assessment (i-Tree Eco) to establish accurate potential canopy cover and the amount of tree cover also provided by woodland and hedges. 2. Agree a long-term borough-wide target for tree canopy cover to include all land public and private. 3. Review every 5 years by carrying out a canopy cover assessment. 	Planning	2026

Priority	Key Performance Indicators showing current position			
	Low	Moderate	Good	Optimal
	The existing canopy cover equals 0–25% of the potential.	The existing canopy cover equals 25–50% of the potential.	The existing canopy cover equals 50–75% of the potential.	The existing canopy cover equals 75–100% of the potential.

3.1 Targets, Priorities & Actions: Tree and Woodland Structure

T2. Size (Age) Diversity

Size and age diversity are essential for maintaining a healthy, robust tree population. Sufficient young trees must be planted or growing to at least match the overall mortality and removal rate. To expand the urban forest, planting must exceed these losses.

Size is a useful way to understand age when managing large tree populations. Larger, older trees typically offer greater benefits due to their extensive canopies and must be protected and well managed. Younger trees are vital to long-term sustainability, providing understory canopy and replacing those lost to natural decline or safety concerns.

Understanding this picture at neighbourhood, ward and even street level can help to inform long term tree establishment strategy

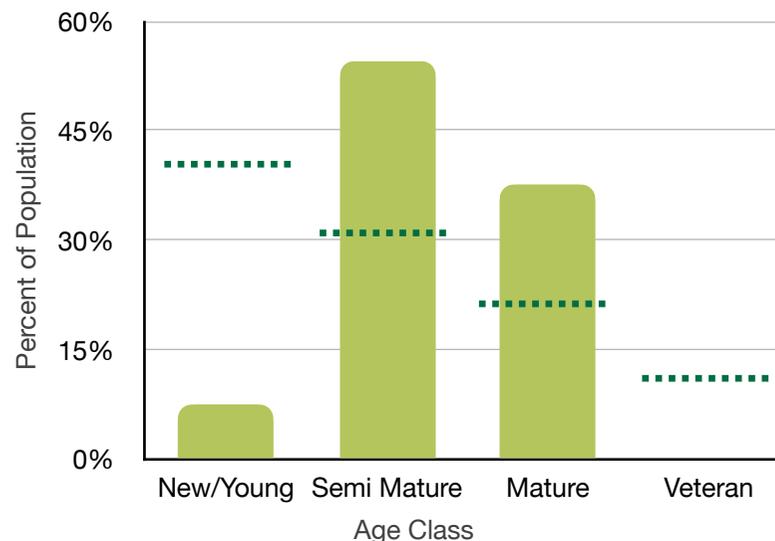


Figure 2. Age class distribution of trees in Hartlepool.

Source: HBC Tree Safety Report

Dotted line represents “ideal” proportion in each age class (Richards. N.A, 1982).

Policy Links	Actions	Resp	Review
Hartlepool Regeneration Plan 2026-2036	<ol style="list-style-type: none"> 1. Conduct annual ward-based inventory analysis/forecast to understand gaps and impact of new planting at neighbourhood level. 2. Review after each full inventory survey to identify trends and inform gaps. 3. Results to inform planting / site selection in tandem with R10 (opportunity mapping output). 	Planning	2028

Priority	Key Performance Indicators showing current position			
	Low	Moderate	Good	Optimal
	Even age distribution or highly skewed toward a single age class.	Some uneven distribution, but most of the tree population falls into a single age class.	Approaching ‘ideal’ age distribution of 40% juvenile, 30% semi-mature, 20% mature, and 10% senescent.	Total population approaches that ideal distribution at neighbourhood level as well as borough-wide.

3.1 Targets, Priorities & Actions: Tree and Woodland Structure

T3. Species Diversity

Diversity is a critical aspect to monitor. Trees are grouped into families, genera, species, and varieties and diversity at all levels is essential to create a healthy and resilient urban forest. Santamour's 10-20-30⁸ rule of thumb for species, genus and family is a useful tool for assessing and providing targets for species diversity in the urban forest. Species diversity can be measured through a combination of richness (the number of species present) and evenness (the relative abundance of each species). These can be combined into a single indicator, such as Hubbell's dominance diversity curve⁹ (figure 3), or the Shannon-Wiener Index¹⁰.

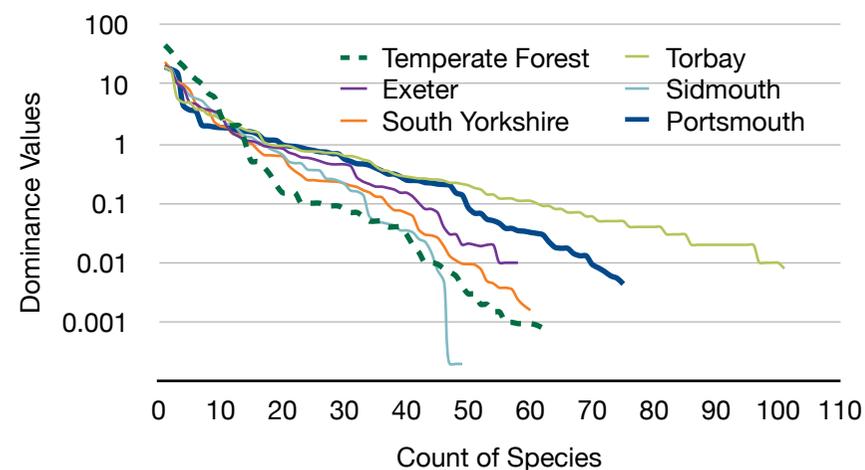


Figure 3. Dominance Diversity Curve showing example temperate forest and selected UK cities & towns.

The longer and shallower the curve, the more species and the more evenly distributed they are within the population, therefore (generally speaking) the better the diversity.

8. Santamour, F.S. (1990)

9. Rosindell, J., Hubbell, S.P. and Etienne, R.S. (2011)

10. Shannon, C.E. & Weaver, W. (1949)

Policy Links	Actions	Resp	Review
Trees & Devt SPD	<ol style="list-style-type: none"> Assess Hartlepool Borough Council's current tree stock diversity at neighbourhood and even street level e.g. Santamour's Rule. Improve species diversity by including guidance into Hartlepool's Trees and Development Guidelines SPD. 	Planning	Annually 2027 & every 10 years

Priority	Key Performance Indicators showing current position			
	Low	Moderate	Good	Optimal
	Five or fewer species dominate the entire tree population across district.	No species >10% of all trees no genus >20%; no family >30%.	No species >5% of all trees no genus >10%; no family >15%.	As 'Good' Borough wide Plus at least "Moderate" at the neighbourhood level.

3.1 Targets, Priorities & Actions: Tree and Woodland Structure

T4. Species Suitability

Species suitability involves selecting a broad array of species which are well suited to the local climate and their particular site context, complicated as it is by our urban living.

All trees have different environmental tolerances which allow them to thrive in their natural environment; however the urban landscape presents challenges which not all trees are equipped to manage.

These urban stresses can limit their lifespan and leaving them susceptible to pests and disease. By promoting the planting of suitable species in suitable locations, trees will be more resilient against these stresses and are therefore more capable of reaching their full potential.

The determination of species suitability should take into account concerns such as tolerance to drought, water-logging, salt, shading, and soil types, as well future climate suitability, pest and disease risk, invasiveness, and any other site-specific considerations. Table 1 uses the BGCI¹¹ climate tool to give some indication for the top 5 species locally.

11. Botanic Gardens Conservation International (2024)

Species	% of Population	Current Climate	RCP 4.5 Scenario	RCP 7.0 Scenario
Sycamore	13.1%	Green	Green	Orange
Ash	10.6%	Green	Green	Orange
Hawthorn	5.7%	Green	Green	Orange
Norway maple	5.5%	Green	Green	Orange
Lawsons cyprus	4.5%	Green	Green	Green

Table 1. Current and future species suitability under 3 climate scenarios for most common tree taxa in Hartlepool based on BGCI₁₁ climate tool

Current scenario is based on the climate of 2020.

RCP4.5 (or SSP2) is the 2050 limited emission scenario as laid out by the IPCC.

RCP7.0 (or SSP3) is the 2090 usual emission scenario as laid out by the IPCC.

- ◆ Species mostly occurs at this temperature
- ◆ Species is known to occur at this temperature
- ◆ The edge of the known temperature for this species
- ◆ Species not known to occur at this temperature

Policy Links	Actions	Resp	Review
Trees & Devt SPD	<ol style="list-style-type: none"> Understand current/planned species mix in terms of future viability. Maintain current evaluation references - BGCI, Kew, FC, TDAG, TfC Coastal Guidance. Establish Native - Non-Native Guidelines for urban, rural and ecologically sensitive areas. Integrate 'Right tree, right place & right reason' protocols into the Trees & Devt SPD. 	Planning Parks	2028

Priority	Key Performance Indicators showing current position			
	Low	Moderate	Good	Optimal
	<50% of trees from species considered suitable for the area and its projected climate.	>50%-75% of trees are from species suitable for the area and its projected climate.	>75% of trees are suitable for the area and its projected climate.	Virtually all trees are suitable for the area and for projected climate.

3.1 Targets, Priorities & Actions: Tree and Woodland Structure

T5. Publicly owned trees (trees managed “intensively”)

Street trees and individual park trees in Hartlepool need a high level of care because of the pressures of the urban environment. Compacted soils, limited growing space, pollution, and their closeness to people, buildings, and roads all place extra stress on trees. Regular monitoring and maintenance are essential to keep them safe and healthy.

Hartlepool Borough Council carries out planned, routine inspections, with safety and risk management as the main priority. This approach helps reduce hazards, spot problems early, and limit the spread of disease.

Managing trees proactively lowers the risk of failure and helps deal with everyday issues such as overhanging branches, surface roots creating trip hazards, and concerns about leaf fall. It also supports newly planted trees, which are particularly vulnerable in busy urban areas. Regular checks and timely actions—like pruning, watering, or improving soil conditions—greatly increase survival rates and help trees establish successfully for the long term.



Policy Links	Actions	Resp	Review
Hartlepool Tree Risk Evaluation and Management Policy	<ol style="list-style-type: none"> Maintain programme of periodic tree condition assessments which includes detailed tree condition with an adopted risk methodology. Deliver a publicly-viewable GIS-based tree inventory of council-managed trees within Highways, Open Spaces, Parks and Cemeteries. 	Highways/ Parks/Hort/ Cemeteries Planning	Annually 2027

Priority	Key Performance Indicators showing current position			
	Low	Moderate	Good	Optimal
	Condition of treescape is unknown.	Sample-based tree inventory indicating tree condition and risk level.	Complete tree inventory that includes detailed tree condition ratings.	Complete tree inventory that is GIS-based and includes detailed tree condition as well as risk ratings.

3.1 Targets, Priorities & Actions: Tree and Woodland Structure

T6. Publicly owned natural areas (managed “extensively”)

Understanding how many trees are managed in parks and woodlands and what this type of management entails will help provide information for improving future ‘extensive’ practices.

Natural area surveys that incorporate patterns and structures of ecological functions would be useful. Woodland fragmentation, recreational overuse, disturbance and invasives are often highlighted as issues of serious concern, which are as yet unquantified.

Developing individual management plans and a web map for these areas could be a useful tool for both management, community engagement and connectivity. Current ‘extensive’ management methods should be reviewed and updated if necessary.

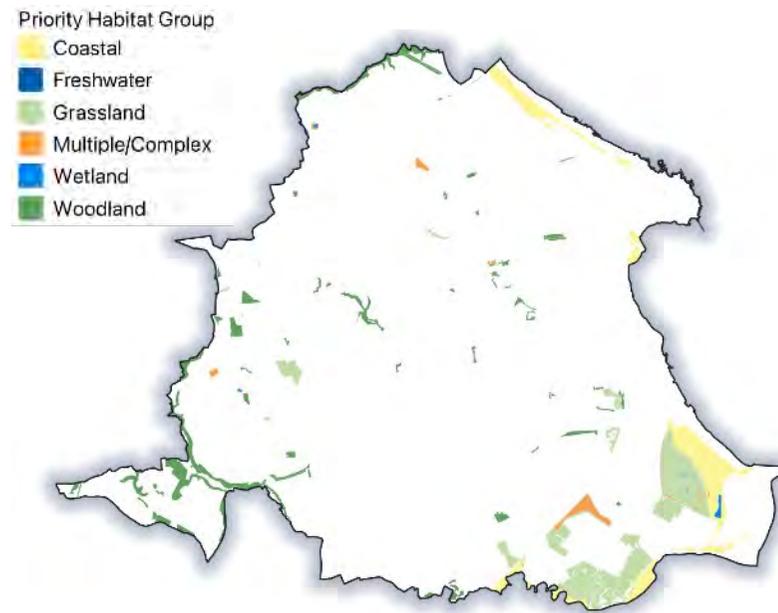


Figure 4. Priority habitat map of Hartlepool

Policy Links	Actions	Resp	Review
LNRS	<ol style="list-style-type: none"> 1. Deliver a publicly viewable GIS based map with details of nature areas, parks, SSSI's etc. 2. Review and update management plans for these areas. 	Parks	2027

Priority	Key Performance Indicators showing current position			
	Low	Moderate	Good	Optimal
	No information about publicly owned natural areas.	Publicly owned natural areas identified in a downloadable document or on webmap.	Survey document also tracks level and type of public use in publicly owned natural areas.	In addition, usage patterns, ecological structure and function of all publicly owned natural areas are also assessed and documented.

3.1 Targets, Priorities & Actions: Tree and Woodland Structure

T7. Trees on private property

Privately owned and managed trees are a huge part of any urban forest. They may be in private residences, industrial sites, schools or universities, and other such locations.

Collaborating with large private landholders and local residents alike is crucial to improving tree and shrub care by improving public knowledge of tree maintenance, planting practices, awareness of local policies such as tree preservation orders (TPO's), and generally encouraging people to make their properties a little bit greener.

Finding ways to incentivise this and provide open information and recommendations on species choice, tree planting and tree care would help residents make informed decisions.



Policy Links	Actions	Resp	Review
Local Plan Trees in Devt SPD	<ol style="list-style-type: none"> 1. Review and update current TPO's within Hartlepool with inclusion on Council's GIS system. 2. Carry out a detailed tree inventory sample survey (i-Tree Eco) within 5 years. 3. Engage local residents in surveys online to contribute. 4. Improve awareness around protected trees. 	Planning	2027 2030 2026 2028

Priority	Key Performance Indicators showing current position			
	Low	Moderate	Good	Optimal
	No information about privately owned trees.	Canopy cover study broken down into public and private ownership.	Sample-based assessment of trees on private property as well as canopy cover survey.	Borough-wide data on private trees - sample survey plus canopy available within GIS system.

3.1 Targets, Priorities & Actions: Tree and Woodland Structure

T8. Other Elements of the Treescape

Other elements of the urban forest include shrubs, hedges, green walls and roofs, plants, wildlife, and water. These elements, along with trees, provide a wide range of benefits, including ecosystem services and amenity value.

Whilst shrubs, like trees, can be surveyed (for example as in the i-Tree Eco project), and green walls and roofs can be counted and measured, many of these other elements are very difficult to quantify, such as biodiversity for example. Assessing and quantifying these environmental impacts and services is even more complex. However, tools like the Urban Greening Factor (UGF) as used in London can provide a guideline and targets for urban greening.

In order to maintain and enhance the urban forest, Hartlepool needs to consider these other elements and to identify opportunities around new greening, biodiversity enhancement and development. This will help the borough to make the most of its urban forest so that it continues to provide benefits to the community into the future in both a meaningful and quantifiable way.



Policy Links	Actions	Resp	Review
Highways Public Safety Safe space for girls	<ol style="list-style-type: none"> 1. Develop local policy/strategy re. non-tree asset mgt / green open spaces (safety / nature) 2. Baseline survey of other elements of the tree scape. (Hedgerows/ shrubs/ SUD's etc) 3. Determining action plan to improve other elements. 	Highways / Parks / Hort/ Cemeteries	2028

Priority	Key Performance Indicators showing current position			
	Low	Moderate	Good	Optimal
	No information about other elements of the treescape.	Assessment of existing elements of the treescape in place. Green infrastructure baseline has been established.	Establish relevant local targets. Identify opportunities for new greening in development.	A relevant local policy has been developed and implemented. Monitoring is ongoing.

3.1 Targets, Priorities & Actions: Tree and Woodland Structure

T9. Tree Benefits

Trees in cities bring with them both benefits and costs. Whilst many of the costs are well known, the benefits can be difficult to quantify or justify. Nevertheless, a considerable and expanding body of research exists on the benefits that urban trees provide to those who live and work in our towns and cities, to green infrastructure and to the wider urban ecosystem.

Trees provide a ‘sense of place’, moderate extremes of high temperature in urban areas, improve air quality, reduce rainwater runoff, and act as a carbon sink.¹² Understanding and valuing these services allows us to make more informed planting and management decisions for the benefit of current and future generations. It can also help communicate the importance of trees to the public, other landholders, and to those in the planning sector, encouraging the protection and management of existing trees as well as new planting.

i-Tree Eco is a tool which can be used to quantify tree benefits whilst also giving an overview of the structure of the urban forest. i-Tree Eco Technical reports provide a detailed analysis of many of the quantifiable tree benefits. Not all benefits are quantifiable, however amenity valuations (i.e., CAVAT) can help to value urban forests.

12. Doick, K. J., et al. (2018)

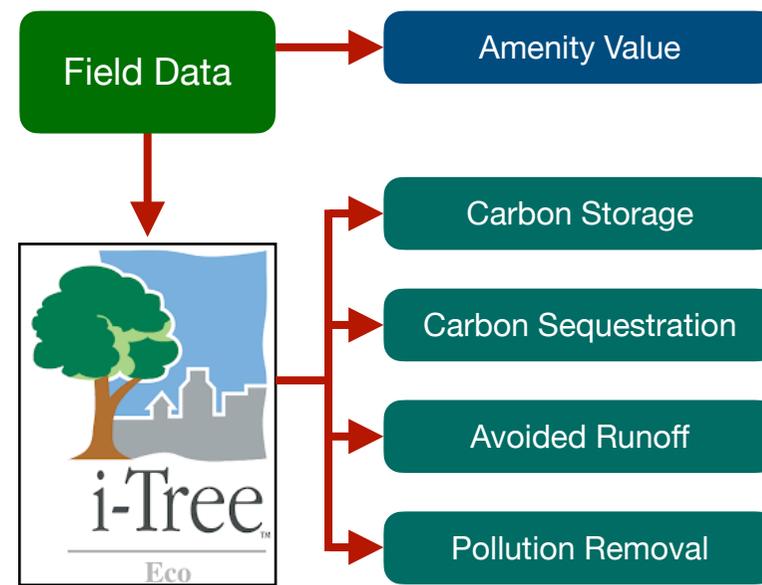


Figure 5. Types of tree benefit which can be measured and valued using i-Tree Eco and CAVAT tools.

Policy Links	Actions	Resp	Review
	1. Comprehensive information available on all tree benefits across the district. 2. Carry out a borough-wide detailed i-Tree Eco assessment within 5 years. 3. Resurvey in 10 years.	Planning	2030

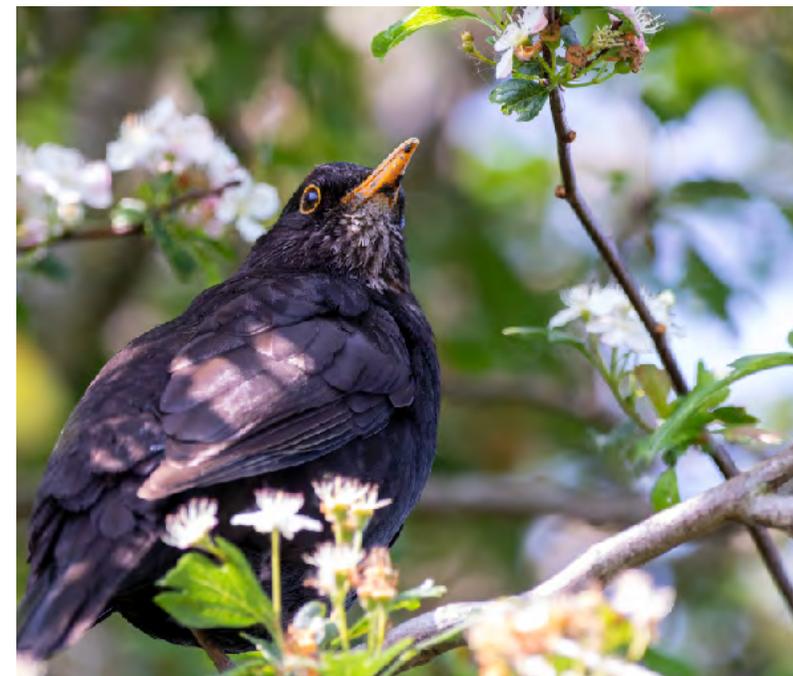
Priority	Key Performance Indicators showing current position			
	Low	Moderate	Good	Optimal
	No comprehensive information available about tree benefits.	Some information available on key tree benefits.	Sound information available on a key set of tree benefits, such as biodiversity, recreation, environmental services.	Comprehensive information available on all tree benefits across the borough.

3.1 Targets, Priorities & Actions: Tree and Woodland Structure

T10. Wider Environmental Considerations

The urban forest provides a vast array of ecosystem services which benefit the local area and the wider environment. Trees play a vital part in the fight against climate change and can be part of adaptation and mitigation strategies. Urban trees are particularly important as a way of reducing the urban heat island (UHI) effect, and in removing air pollution from built up areas and highways. In certain situations, trees can also reduce the energy use of buildings by providing shade in summer and insulation from cold winds in winter.

With the UK target of carbon net neutrality by 2050, understanding the contribution of trees to these types of goals has never been more important. The i-Tree Eco project (2024) has provided a baseline of tree carbon storage and sequestration, air pollution removal, and stormwater attenuation. However, GIS mapping can provide a more detailed spatial analysis of local factors such as UHI, air pollution emissions, surface water flooding, green equity, etc.; allowing future tree planting to target neighbourhoods likely to be worst affected by the challenges of the urban environment and future climate change



Policy Links	Actions	Resp	Review
	<ol style="list-style-type: none"> 1. Full consideration of environmental aspects in relation to trees, based on comprehensive, state-of-the-art information. 2. Trees included in other plans and strategies: Net Zero, Drainage, Design Code etc. 3. Trees integrated in procurement for local solutions to social value obligations. 	Parks	2027
		Planning Procurement	2027

Priority	Key Performance Indicators showing current position			
	Low	Moderate	Good	Optimal
	No consideration/information that relates treescape to climate change, air quality, water.	Some consideration of environmental aspects in relation to treescape, e.g. looking at climate change.	Consideration of at least major environmental aspects in relation to trees.	Full consideration of environmental aspects in relation to trees, based on comprehensive, state-of-the-art information.

3.1 Targets, Priorities & Actions: Tree and Woodland Structure

T11. Natural Regeneration & Habitat Restoration

Natural regeneration and habitat restoration play a vital role by supporting biodiversity, improving soil health, and expanding canopy cover. Natural regeneration relies on the re-establishment of native vegetation and can be encouraged through reduced mowing and minimal disturbance.

Habitat restoration involves active interventions such as removing invasive species and reintroducing native plants and habitat features.

Together, they create ecologically valuable, low-maintenance green spaces. Integrating these approaches into urban planning enhances ecological connectivity, reduces costs, and supports climate resilience, making them key strategies for sustainable, nature-based urban regeneration.



Policy Links	Actions	Resp	Review
Teeside Local Nature Recovery Strategy	<ol style="list-style-type: none"> 1. Establish and maintain GIS-based mapping of NR+HR areas, detailed biodiversity index, high survival rates of NR species, and extensive monitoring and reporting. 2. Identify areas suitable for natural regeneration and prioritise areas for expansion. 3. Monitor progression through site visits, with more formal assessments every 3-5 years. 	Parks	2028 2026 2026

Priority	Key Performance Indicators showing current position			
	Low	Moderate	Good	Optimal
	Minimal efforts with no formal tracking of land area designated for natural regeneration and habitat restoration.	Initial baseline survey and identification of designated areas for NR+HR with basic monitoring of plant and animal species.	Established biodiversity index, expanding of NR+HR areas, regular surveys; detailed tracking of species' health and growth.	Comprehensive GIS-based mapping of NR+HR areas, detailed biodiversity index, high survival rates of NR species, and extensive monitoring and reporting.

3.1 Targets, Priorities & Actions: Tree and Woodland Structure

Tree and Woodland Structure - Summary of KPIs

Key Performance Indicator	Performance Level				Priority
	Low	Moderate	Good	Optimal	
T1 Relative tree canopy cover	Low →		◇		
T2 Size (Age) diversity		Moderate →	◇		
T3 Species diversity		Moderate →	◇		
T4 Species suitability		Moderate →	◇		
T5 Publicly owned trees (managed “intensively”)				Optimal ◇	
T6 Publicly owned natural areas (managed “extensively”)		Moderate →	◇		
T7 Trees on private property	Low →		◇		
T8 Other elements of the treescape	Low →	◇			
T9 Tree Benefits	Low →		◇		
T10 Wider Environmental Considerations	Low →		◇		
T11 Natural Regeneration & Habitat Restoration		Moderate →	◇		

This grid shows all KPIs - each line represents a page in the preceding section

3.2

Community Framework

This section considers the various communities that are required for a successful, long term approach to management of trees, hedges and woodlands. This covers not only the local residents, but local government in all its forms, NGOs and commercial entities.

3.2 Targets, Priorities & Actions: Community Framework

C1. Governance & leadership

The success of Hartlepool's Urban Forest Masterplan (UFMP) depends on strong leadership and cross-departmental collaboration. Understanding the long-term leadership for this plan will ensure continued development of the urban forest.

To ensure tree management aligns with broader borough goals the leadership team must work closely with directorates like planning, transport, environment, and public health as well as external land managers, stakeholders, NGOs, community groups and schools.

Clear communication of the UFMP's priorities and engagement is essential for disseminating information, kickstarting initiatives, supporting applications for funding, and cultivating positive community-focussed relationships

Identifying key internal and external stakeholders to champion the plan will share the vision and goals for Hartlepool's resilient treescape to a wider audience across the borough, within external organisations and throughout the community.



Policy Links	Actions	Resp	Review
Pride in Place Council Plan	<ol style="list-style-type: none"> 1. Demonstrate alignment with pride in place, engaging directly with Hartlepool Board. 2. Conduct a review of stakeholders, schemes and community groups on a 5 yearly basis. 3. Formalise internal governance with clear lead department & clear budgetary provision. 4. Establish a council urban forestry working group which meets regularly. 5. Establish a Hartlepool Tree Forum including council and other major stakeholder. 	Planning Community Hubs & Wellbeing	2026 2026 2027 2027 2027

Priority	Key Performance Indicators showing current position			
	Low	Moderate	Good	Optimal
	Actions taken impacting treescape with no cross-departmental coordination, consultation or consideration of the treescape resource. Leadership fragmented.	HBC works with other NGO's and Agencies on ad-hoc projects as and when they arise.	HBC regularly and frequently works with other NGO's and Agencies to establish projects and plans. There is a cultural champion in place.	Integrated treescape governance and leadership provided by a single dept and measured to plan that reflects local and international policies.

3.2 Targets, Priorities & Actions: Community Framework

C2. Council departmental co-operation

This target aims to encourage all departments within HBC to consult and collaborate with the urban forest managers on issues relating to the urban forest. Regular communication across departments and agencies will be key to ensuring that the urban forest is considered to the fullest extent throughout the council.

Opening communication channels and interdepartmental teams can help to coordinate urban forest management by providing knowledge and guidance to all council departments when required in order to ensure that trees and green infrastructure are considered in full at all stages of decision making.



Policy Links	Actions	Resp	Review
	<ol style="list-style-type: none"> 1. Establish/Record key areas of interdepartmental tree responsibility and processes. 2. Identify political tree champions - Councillors to carry the banner for urban trees and provide political weight behind cross departmental working group. 3. Establish a cross departmental urban trees working group led by a champion (as per C1). 4. Embed engagement through existing events via Community Hubs (Green libraries week). 	Planning/ Parks Community Hubs & Wellbeing	2026

Priority	Key Performance Indicators showing current position			
	Low	Moderate	Good	Optimal
	Actions taken impacting treescape with no cross-departmental coordination, consultation or consideration of the treescape resource.	Departments/agencies recognise potential conflicts and reach out to tree & woodland managers on an ad hoc basis – and vice versa.	Teams among departments and agencies communicate regularly and collaborate on a project-specific basis.	Teams among departments and agencies communicate regularly and collaborate both operationally and strategically.

3.2 Targets, Priorities & Actions: Community Framework

C3. Community involvement & neighbourhood action

For the Urban Forest Master Plan (UFMP) to succeed, its legacy should be a community that values and respects its green infrastructure, ensuring the borough's urban forest is protected, enhanced, and cherished for generations to come. Local organisations are vital partners in urban forest initiatives, helping to raise awareness, mobilise volunteers, and foster a strong sense of community ownership.

Together, community engagement and public support form the foundation for a thriving urban forest. By nurturing these connections, Hartlepool can ensure its green spaces and trees are a valued community asset long into the future.



HARTLEPOOL
COMMUNITY HUBS



Pool
Together



Hartlepool
Community
Trust
Supporting Local People

Policy Links	Actions	Resp	Review
Pride in Place Hartlepool Regeneration Plan 2026-2036 Council Plan Community hubs strategy	<ol style="list-style-type: none"> 1. Deepen and expand existing Hartlepool Tree Warden Scheme to all neighbourhoods. 2. Use Community Hubs for meaningful/active engagement (drop-in events, tree walks, talks). 3. Leverage Hartlepool environmental groups as champions of trees within the town. 4. Build and maintain Community directory of groups (building on TEFH). 5. Empower neighbourhood groups to collaborate/act independently - sharing borough plans. 	<p>Parks/ Planning</p> <p>Community Hubs & Wellbeing</p>	<p>2027</p> <p>2026</p> <p>2026</p>

Priority	Key Performance Indicators showing current position			
	Low	Moderate	Good	Optimal
	Little or no citizen involvement or neighbourhood action.	At neighbourhood level, citizens participate and groups collaborate with HBC/partner NGOs in tree & woodland management activities to advance HBC plans.	Some neighbourhood groups engaged in advancing tree & woodland goals, but with some overall coordination with or direction by HBC or its partnering NGOs.	Many active neighbourhood groups and a tree officer in every ward engaged across the community, with actions coordinated or led by HBC and/or its partnering NGOs.

3.2 Targets, Priorities & Actions: Community Framework

C4. General appreciation of trees as a community resource

For true, long term success of this master plan, one of its legacies can be that the people of Hartlepool love, respect and appreciate their trees. By engaging and encouraging the community, the urban forest can be better protected and enhanced for generations to come.

Changing peoples values can be difficult, but through education, celebration and engagement, the hope is that people will come to value the trees around them and the wider part which they play in the health of the borough, the nation, and the world.



Figure 6: Tree Tag Examples

Policy Links	Actions	Resp	Review
	<ol style="list-style-type: none"> 1. Annual Tree comms/engagement plan and budget - share and enable. 2. Enact regular Borough-wide public tree appreciation survey as a key metric. 3. Proactively share data in line with best practice open data guidelines (Open Data Institute). 4. Explore tree adoption schemes for public trees, incentivised tree campaigns/ competitions. 5. Identify council member champions of urban forest masterplan to secure stronger policy and protections. 	Parks/ Comms / Community Hubs & Wellbeing / Economic Growth (Tourism)	2026

Priority	Key Performance Indicators showing current position			
	Low	Moderate	Good	Optimal
	General ambivalence or negative attitudes about trees, which are perceived as neutral at best or as the source of problems. Actions harmful to trees may be taken deliberately.	Trees generally recognised as important and beneficial.	Trees widely acknowledged for their environmental, social, and economic services – resulting in some action or advocacy for trees.	Treescape recognised as vital to the community’s environmental, social, and economic well-being. Widespread public and political support and advocacy for trees, resulting in strong policies and plans that advance the viability and sustainability of the entire treescape. Furthermore an acceptance that a minority will be ‘anti’ trees but that these views are also considered and reflected on with the proponents also being consulted with.

3.2 Targets, Priorities & Actions: Community Framework

C5. Inspiring the next generation

Children and young people will live with the consequences of today's decisions about Hartlepool's urban forest for far longer than any current decision-maker. Inspiring the next generation is therefore not only about education or participation, but about building positive attitudes, confidence and a sense of ownership over their environment.

When children are meaningfully involved, they develop lifelong connections with trees, nature and place, shaping behaviours that support health, climate resilience and biodiversity.

By involving children in decisions today, Hartlepool can nurture future stewards who understand, value and actively champion the urban forest across generations.



"If children don't grow up knowing about and appreciating nature, they will not understand it. And if they don't understand it, they won't protect it. And if they don't protect it, who will?"

David Attenborough

Policy Links	Actions	Resp	Review
Hartlepool Regeneration Plan 2026-2036 Education	<ol style="list-style-type: none"> 1. Host and run a youth summit with trees at its heart. 2. Use schools as the hubs for a green connectivity network across the borough. 3. Roll out a 3 trees visible from every classroom planting program. 4. Embed trees into the curriculum at every stage . 5. Work with Hartlepool Youth Services. 6. Explore existing mobile/web applications to catalogue significant trees. 	Parks / Planning / Community Hubs & Wellbeing / Economic Growth (Tourism)	2026

Priority	Key Performance Indicators showing current position			
	Low	Moderate	Good	Optimal
	Children/young people's involvement is occasional and limited. Activities tend to be one-off, with little focus on building lasting interest	Children/young people are supported to build awareness and positive attitudes towards trees. There are structured opportunities for them to take part in activities.	Children/young people develop strong connections to the urban forest, alongside the skills and confidence to get involved. Clear routes in place for them to influence priorities, design and how projects are delivered.	Children and young people show real ownership and leadership. They help shape attitudes, decisions, and long-term investment in the urban forest, with care for trees becoming a normal and valued part of local culture.

3.2 Targets, Priorities & Actions: Community Framework

C6. Utilities co-operation

As landholders, stakeholders, and contractors, utilities organisations are frequently responsible for working with and around trees and other green infrastructure.

Utilities are required to follow certain standards for managing vegetation – including pruning branches, protecting roots, and performing overall management of trees and other vegetation that could impact their services. Council policies may also regulate certain utility management practices, such as overhead line clearance.

Introducing and enforcing best practice standards which protect trees and other elements of the urban forest will be key to advancing the goals and objectives of the Masterplan.



Policy Links	Actions	Resp	Review
Local plan, Highways, Trees & Devt SPD	<ol style="list-style-type: none"> 1. Identify the utilities companies and open dialogue on this plan. 2. Strengthen communication with utility companies to explore solutions to conflicts with trees and services at North East Highway Authorities and Utilities Committee Forum. 3. Add Utilities section to Trees&Devt SPD to provide clear advice for companies to buy in to. 4. Invite representatives from utility companies to sit on the Hartlepool Tree Forum. 	Highways	2027

Priority	Key Performance Indicators showing current position			
	Low	Moderate	Good	Optimal
	Utilities take actions impacting treescape with no council coordination or consideration of the treescape resource.	Utilities employ best management practices, recognise potential municipal conflicts, and reach out to tree & woodland managers on an ad hoc basis – and vice versa.	Utilities employ best management practices, recognise potential municipal conflicts, and reach out to tree & woodland managers on a regular basis to be aware of potential risks – and vice versa.	Utilities are included in informal council teams that communicate regularly and collaborate on a project-specific basis.

3.2 Targets, Priorities & Actions: Community Framework

C7. Green industry co-operation

The “green industry” encompasses all professions and businesses that routinely support or engage in tree and vegetation management activities. Among others, these can include landscapers, nurseries, garden centres, contractors, maintenance professionals, tree care companies, landscape architects, foresters, planners, and developers.

Green industry must work together with the council to advance borough-wide urban forest goals and objectives, and adhere to high professional standards. HBC should aim to support tree nurseries to follow biosecurity protocols, and signpost resources for best practice.

Close co-operation with the green industry presents an excellent opportunity for borough urban forest managers to influence management of the forest resource on private property.



Policy Links	Actions	Resp	Review
Net Zero	<ol style="list-style-type: none"> 1. Establish working relationships with key green industry companies across the Borough 2. Engage with industry forums (Association of Public Service Excellence - APSE, NE Chemicals Forum) 3. Ensure green industry companies are represented on Hartlepool Tree Forum 	Net Zero Planning	2027

Priority	Key Performance Indicators showing current position			
	Low	Moderate	Good	Optimal
	Little or no cooperation among segments of green industry or awareness of district-wide treescape goals and objectives.	Some cooperation among green industry as well as general awareness and acceptance of district-wide goals and objectives.	Specific collaborative arrangements across segments of green industry in support of district-wide goals and objectives.	Shared vision and goals and extensive committed partnerships in place. Solid adherence to high professional standards.

3.2 Targets, Priorities & Actions: Community Framework

C8. Involvement of large private & institutional landholders

As a proportion of land within the borough is owned by private individuals, organisations and institutions, enlisting their help in enhancing and protecting the urban forest is paramount.

The goal of helping large private landholders embrace and advance borough-wide urban forest goals and objectives by providing training and developing resource management plans so that they can manage trees and green infrastructure on their property in a way which benefits the whole of Hartlepool.



Policy Links	Actions	Resp	Review
Estates. Highways (Public right of way)	<ol style="list-style-type: none"> 1. Identify and engage large landowners - Housing Assocs, Church Commission, Hospitals etc (Engagement plan via their forums). 2. Coordinate and signpost collaborative arrangements / support tools / grant options to enable landowners to play a full role to meet the objectives of the plan. 3. Ensure major landowners are represented on the Hartlepool Tree Forum. 	Parks	2028

Priority	Key Performance Indicators showing current position			
	Low	Moderate	Good	Optimal
	Large private landholders are generally uninformed about tree & woodland issues and opportunities.	HBC conducts outreach directly to landholders with educational materials and technical assistance, providing clear goals and incentives for managing their tree resource.	Landholders develop comprehensive tree management plans (including funding strategies) that advance district-wide tree & woodland goals.	As described in "Good" rating, plus active community engagement and access to the property's forest resource.

3.2 Targets, Priorities & Actions: Community Framework

C9. Trees & Agriculture

Integrating trees into agricultural systems - known as agroforestry - offers multiple benefits within peri-urban and fringe areas of an urban forest. Trees improve soil health, reduce erosion, enhance water retention, and support biodiversity.

They can provide shade for livestock, shelter for crops, and alternative sources of income through timber, fruit, or nut production.

In urban contexts, tree planting on farmland also contributes to landscape connectivity, carbon sequestration, and visual amenity.

Promoting agroforestry practices near urban boundaries strengthens the relationship between the borough and countryside, supporting sustainable land use while contributing to broader environmental, social, and climate resilience goals.



Policy Links	Actions	Resp	Review
Sustainable Farming Incentive Countryside Stewardship	<ol style="list-style-type: none"> 1. Facilitate an annual agroforestry best practice/learning event - options for co-creation/coordination with Community Forest and/or Forestry Commission. 2. Encourage agro-forestry projects with access to support tools inc grant application. 3. Leverage each new project as part of local knowledge base and as a case study. 4. Engage via relevant forums - Regional NFU. 	Tees Community Forest, Estates, Public Rights of Way	2028

Priority	Key Performance Indicators showing current position			
	Low	Moderate	Good	Optimal
	No integration of agroforestry practices; soil health and agricultural yields are not monitored.	Up to 10% of farms practicing agroforestry with initial soil health and yield assessments.	10-25% of farms adopting agroforestry, regular soil health tests, and noticeable improvements in soil health and yields.	Over 25% of farms adopting agroforestry, comprehensive soil health metrics, significant yield improvements, and integrated GIS-based tracking and analysis.

3.2 Targets, Priorities & Actions: Community Framework

C10. Tourism & Trees

Trees and woodlands significantly enhance tourism by creating attractive, accessible green spaces that support recreation, wellbeing, and local economies. Strategically planned tree cover can improve the visual appeal of heritage sites, parks, and walking or cycling routes.

Urban forests contribute to placemaking, drawing visitors and supporting nature-based tourism. Seasonal interest, biodiversity, and shade increase the usability of outdoor spaces year-round. Investing in treed landscapes near cultural landmarks, hospitality zones, and transport hubs can boost footfall, extend visitor stays, and promote environmental awareness, making trees a vital asset in sustainable urban tourism.



Policy Links	Actions	Resp	Review
Hartlepool Regeneration Plan Hartlepool Waterfront Micro-masterplan	<ol style="list-style-type: none"> Integrating trees into Pride in Place and the Hartlepool waterfront regeneration work to enhance Hartlepool as a tourism hotspot. Introduce naming of important trees within Hartlepool as a features people can visit through the Ancient Tree Inventory and local directory. Create a Hartlepool tree walk route and promote it through Explore Hartlepool. Running seasonal incentivized campaigns and competitions which are tree related. 	Economic Growth (Tourism) Planning Community Hubs & Wellbeing	2027

Priority	Key Performance Indicators showing current position			
	Low	Moderate	Good	Optimal
	Limited green spaces for tourism, minimal visitor engagement, and no tracking of economic benefits from eco-tourism.	Some expansion of green spaces, initial visitor satisfaction surveys, and basic economic impact assessments.	Regular expansion of accessible green spaces, improved visitor satisfaction ratings, and noticeable economic benefits from eco-tourism activities.	Extensive, GIS-mapped green spaces, high visitor satisfaction, substantial economic growth from eco-tourism, and comprehensive tracking and reporting systems.

3.2 Targets, Priorities & Actions: Community Framework

C11. Regional collaboration

Environmental issues do not respect political boundaries, making collaboration with neighbouring local and county authorities crucial. Effective regional cooperation allows for shared solutions to challenges like water management, coastal erosion, biodiversity preservation, and habitat protection.

It is essential to consider the strategies and initiatives of surrounding areas. Regional policies, have been considered in the development of this plan, ensuring that our efforts align with broader environmental objectives in the region.



Policy Links	Actions	Resp	Review
Hartlepool Regeneration Plan	<ol style="list-style-type: none"> 1. Join the Association of Tree Officers (ATO). 2. Establish a regional Tees Valley tree officers forum through the ATO to share best practice. 3. Proactively support the Community Forest development of a regional forest. 4. Seek alignment with adjoining authorities on environmental issues that cross boundaries 	Planning Parks	2027
		Hartlepool Board	2026

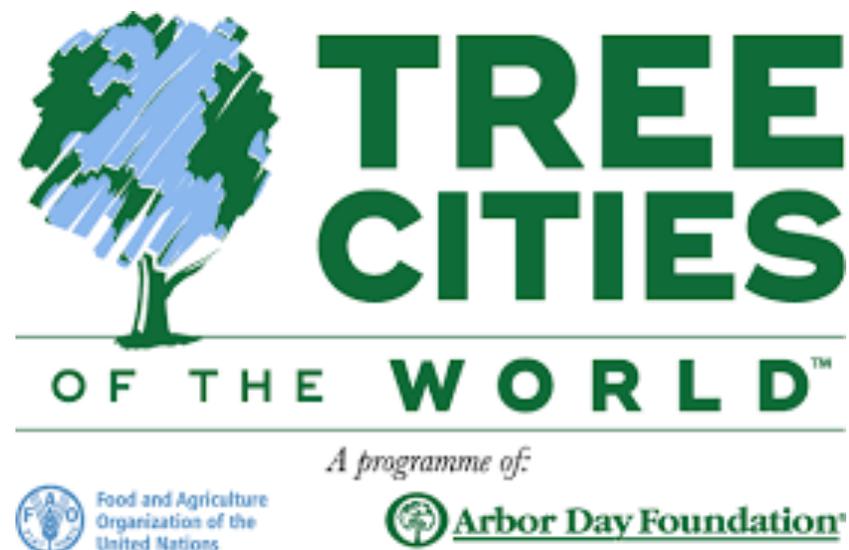
Priority	Key Performance Indicators showing current position			
	Low	Moderate	Good	Optimal
	No regional planning or coordination on trees & woodlands.	Some neighbouring authorities and regional agencies share similar policies and plans related to trees & woodlands.	Some tree & woodland planning and cooperation across authorities and regional agencies.	Widespread regional cooperation resulting in development and implementation of regional tree & woodland strategy.

3.2 Targets, Priorities & Actions: Community Framework

C12. International Reputation

Internationally recognised titles help to promote understanding that the urban forest and the management of it, is of a recognised and high international standard.

Not only will Hartlepool push to achieve international recognition, it shall endeavour to lead by example and provide guidance to other towns worldwide. To succeed in this, Hartlepool should promote its successes and continue to innovate its forest management strategies and Green Infrastructure development plans.



Policy Links	Actions	Resp	Review
	<ol style="list-style-type: none"> 1. Become a Tree cities of the World recognised city. 2. Work towards Natural Englands Nature Town & City Award. 3. Encourage submission to Tree of the Year award (Woodland Trust). 4. Seek participation in National and International initiatives, awards and schemes. 5. Tell Hartlepool's Urban Forest Journey - as loudly as possible - invite/secure media reports on Hartlepool's journey. 	Planning / Parks	2026 2027 2027 2028 2026

Priority	Key Performance Indicators showing current position			
	Low	Moderate	Good	Optimal
	There is no vision - aspiration or consideration of HBC as a treescape of International reputation.	HBC is sometimes considered for its leadership in international contexts, based on specific efforts and projects.	HBC has signed up to international programs such as Tree cities of the World and the Biophilic City Network and makes good use of these networks to further its tree & woodland program.	HBC is regarded as an international example and leader in its tree & woodland management and is prominent on the international stage for its treescape.

3.2 Targets, Priorities & Actions: Community Framework

Community Framework Summary

Key Performance Indicator	Performance Level				Priority
	Low	Moderate	Good	Optimal	
C1 Governance & leadership		Moderate		Optimal	
C2 Council departmental co-operation				Optimal	
C3 Community involvement & neighbourhood action		Moderate		Optimal	
C4 General appreciation of trees as a community resource	Low			Optimal	
C5 Inspiring the next generation	Low			Optimal	
C6 Highways & Utilities co-operation		Moderate		Optimal	
C7 Green industry co-operation	Low			Optimal	
C8 Involvement of large private & institutional landholders		Moderate		Optimal	
C9 Trees & Agriculture	Low			Optimal	
C10 Tourism & Trees	Low			Optimal	
C11 Regional collaboration	Low			Optimal	
C12 International Reputation	Low			Optimal	

3.3

Sustainable Resource Management

This section deals with the practical management of the trees and woodland resource. For much of the urban forest, this can mean seeking to engage and influence other land owners.

3.3 Targets, Priorities & Actions: Sustainable Resource Management

R1. Publicly Owned Tree & woodlands inventory

A tree and woodland inventory involves taking stock of the individual trees within the urban forest. It is time and labour intensive to compile a full tree inventory of all trees, however, plot/sample based inventories of woodland/parkland areas, combined with existing street tree and park tree inventories would be a good place to start.

Holding complete inventories enables HBC to make informed decisions on how to improve and enhance the urban forest based on live, up to date information, and centrally manage tree works and maintenance schedules.

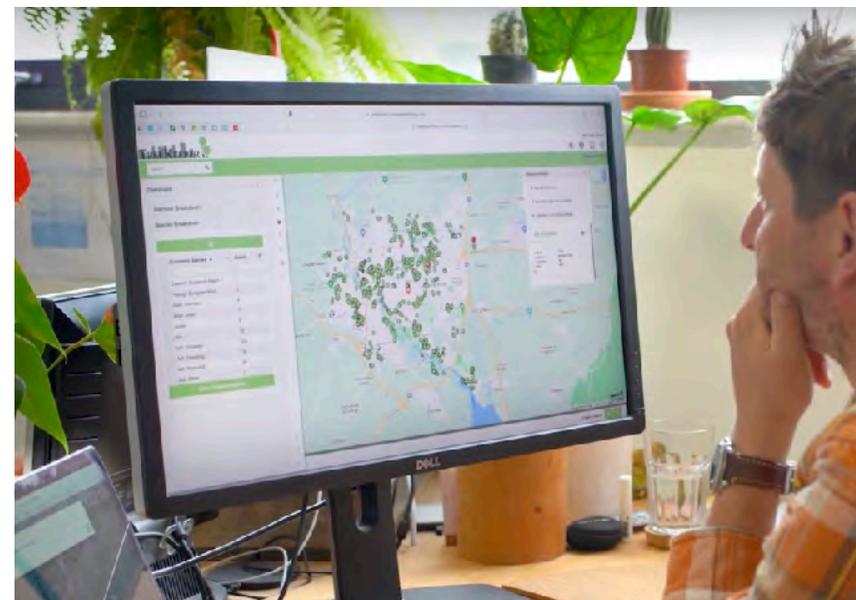


Figure 8. Tree inventory management system

Policy Links	Actions	Resp	Review
Tree Risk Evaluation and Management Policy	<ol style="list-style-type: none"> 1. Review current Tree Management software as fit for all trees and woodlands. 2. Periodically carry out tree condition assessments which includes detailed tree condition with an adopted risk methodology. 3. Establish clear responsibilities alongside internal protocols. 4. Deliver a publicly viewable GIS based tree inventory of ALL council managed trees. 	Highways/ Parks/Hort/ Cemeteries	2026

Priority	Key Performance Indicators showing current position			
	Low	Moderate	Good	Optimal
	No inventory.	Complete or sample-based inventory of publicly owned trees.	Complete or sample-based inventory of publicly owned trees that is guiding management decisions.	Systematic comprehensive inventory system of entire treescape – with information tailored to users and supported by mapping in district-wide GIS system.

3.3 Targets, Priorities & Actions: Sustainable Resource Management

R2. Privately Owned Tree & woodlands inventory

Privately owned trees and woodlands represent a significant but often under-recognised component of the urban forest. Including them in a comprehensive inventory is essential for understanding overall canopy cover, biodiversity value, and management needs. Mapping and assessing private trees - whether in gardens, estates, farms, or institutional grounds - supports more inclusive planning and highlights opportunities for partnership, stewardship, and incentive schemes.

Recognising the contribution of private landowners also encourages shared responsibility for tree health, biosecurity, and climate resilience. Integrating this data strengthens the urban forest evidence base, helping deliver coordinated, landscape-scale greening across public and private boundaries.

Large private landholders and institutions should be encouraged to create inventories of their own for management and maintenance.



Policy Links	Actions	Resp	Review
	<ol style="list-style-type: none"> 1. Review and update current TPO's within Hartlepool with inclusion on Council's GIS system. 2. Carry out a detailed canopy cover assessment (i-Tree Eco) within 5 years. 3. Engage local residents in surveys online to contribute. 	Planning Comms	2026

Priority	Key Performance Indicators showing current position			
	Low	Moderate	Good	Optimal
	No inventory.	Complete or sample-based inventory of privately owned trees.	Complete or sample-based inventory of privately owned trees that is guiding management decisions.	Systematic comprehensive inventory system of entire treescape – with information tailored to users and supported by mapping in district-wide GIS system.

3.3 Targets, Priorities & Actions: Sustainable Resource Management

R3. Tree valuation & asset management approach

Tree valuation is an important part of managing and promoting the urban forest. With the trees valued, local people, landowners/managers, and stakeholders can understand the value of trees beyond the material worth. With these figures to hand, advocating for trees becomes easier.

Capital Asset Valuation of Amenity Trees (CAVAT) is one of the principal methods of tree valuation in the UK. It aims to provide a method for managing trees as assets rather than liabilities.¹³ It can be used for individual trees or for the tree stock as a whole.

Tree valuation should be a part of all projects which affect trees, and should be considered prior to tree removals (except where safety is the primary concern).



13. Doick, K. J. et al. (2018)

Policy Links	Actions	Resp	Review
Medium Term Financial Strategy (MTFS)	<ol style="list-style-type: none"> 1. Add CAVAT valuations of each tree/group to the council tree inventory system as updated. 2. Provide each responsible council department with a CAVAT Valuation of their trees. 3. Include CAVAT assessment and valuation for Tree preservation Orders created & reviewed. 4. Include trees on the councils risk register and asset register. 	Planning / Insurance	2027

Priority	Key Performance Indicators showing current position			
	Low	Moderate	Good	Optimal
	Tree valuation nor assessment management are in place.	Some form of tree valuation is available.	Some form of tree valuation is used, at least for key projects involving public trees.	Tree valuation and asset management are implemented for all public trees - and in some cases also private trees.

3.3 Targets, Priorities & Actions: Sustainable Resource Management

R4. Canopy cover assessment & goals

Assessing canopy cover is vital, as this metric is used frequently as a figure which is clear and easy to compare with other areas. Whilst canopy cover is not a thorough study of the health and diversity and therefore overall benefit of the urban forest, it is an important aspect which should not be overlooked simply for its simplicity.

This target involves assessing the existing canopy cover in detail, and setting goals based on reasonable potential canopy cover and achievable steps to maximising cover. This leads into T1-‘Relative Tree Canopy Cover’- and would provide the necessary baseline for achieving that target. It is important that any tree canopy target is achievable within a reasonable time frame, and considered within the wider context of the master plan.

It should also be noted that tree planting does not necessarily provide an instant increase to canopy cover; in an urban setting trees are constantly being felled for any number of reasons, so insufficient planting can contribute to making up the deficit without actually increasing canopy cover.

City	Hartlepool	Belfast	London	Bristol	Plymouth
Existing Canopy Cover	9% (2025)	14.5% (2022)	21% (2015)	18% (2018)	18.5% (2017)
2050 Target	?	30%	30%	30%	20%

Table 2: Example Cities’ Canopy Cover Estimates and Goals

Policy Links
Environmental Targets (Woodland and Trees Outside Woodland) (England) Regulations 2023

Actions	Resp	Review
<ol style="list-style-type: none"> Once a basic assessment has been done, then T1 canopy targets can be established and further analysis undertaken. Review every 5 years by carrying out a canopy cover assessment. 	Planning	2028

Priority	Key Performance Indicators showing current position			
	Low	Moderate	Good	Optimal
	No assessment or goals.	Low-resolution and/or point-based sampling of canopy cover using aerial photographs or satellite imagery – and limited or no goal-setting.	Complete, detailed, and spatially explicit, high-resolution Urban Tree Canopy (UTC) assessment based on enhanced data (such as LiDAR, Satellite or NTM) – comprehensive set of goals by land use and other parameters.	As described for “Good” rating – and all utilised effectively to drive tree & woodland policy and practice district-wide and at neighbourhood or smaller management level.

3.3 Targets, Priorities & Actions: Sustainable Resource Management

R5. Tree Equity

The urban forest should reflect the people and cultures of Hartlepool’s neighbourhoods, with planting and management shaped by the views and values of the communities it serves.

Tree canopy cover is strongly linked to better health, higher income, higher education, and crime. Lower-income areas often have fewer trees, and this inequality needs to be tackled. In some parts of Hartlepool, limited space or dense housing makes planting difficult, so other types of greening—like green roofs or green walls—may be needed. The goal is to ensure everyone benefits from cleaner air, cooler streets, and more attractive surroundings.

This target focuses on directing planting and management to the places where they can have the biggest impact, especially areas with the lowest canopy cover. Work in these neighbourhoods should include meaningful community engagement so residents can help shape decisions. Recognising that trees hold different meanings for different people is vital to creating an urban forest that serves the whole borough.



Policy Links	Actions	Resp	Review
	<ol style="list-style-type: none"> 1. This master plan to include a community engagement plan/section to guide outreach. 2. Community/volunteer organisations identified in or near to all lowest Tree Equity locations. 3. Housing providers and major landowners provided with TE scores and how they can work to change these including 3-3-300 rule. 4. Develop Tree Warden network as local community advocates for tree equity in their area. 	Parks / Planning	2026

Priority	Key Performance Indicators showing current position			
	Low	Moderate	Good	Optimal
	Tree planting and outreach is not determined equitably by canopy cover or need for benefits.	Planting and outreach includes attention to low canopy neighborhoods or areas.	Planting and outreach targets neighbourhoods with low canopy and a high need for tree benefits.	Equitable planting and outreach at the neighbourhood level is guided by strong citizen engagement in those low-canopy/high-need areas.

3.3 Targets, Priorities & Actions: Sustainable Resource Management

R6. Trees & Development

A sixty year old tree takes sixty years to replace. A simple concept, often ignored, treating these giant of nature the same as other - manufactured - elements of a development: interchangeable and replaceable. Yet, it is these existing sixty-year old and well beyond trees that provide the ecosystem services in a neighbourhood.

Holding on to as many of them as possible is vital to the viability of our towns as liveable places, as reflected in industry best practice for trees and development - BS 5837.

In both design and construction phases, best practice for dealing with trees is well established, now augmented with tools such as the Biodiversity Net Gain (BNG) calculator, i-Tree Eco, and CAVAT.

Guidance for developers and contractors, and training and support for enforcement officers would help to improve both retention and establishment rates.



Link to corporate	Actions	Resp	Review
Local Plan Trees and Development SPD	<ol style="list-style-type: none"> 1. Provide informal pre application advice with the councils aims of improving canopy cover and desire to align with the 3-30-300 rule. 2. Adopt the 3-30-300 rule and Canopy cover targets into Hartlepool's Trees and Development Guidelines SPD. 	Planning / Parks	2028

Priority	Key Performance Indicators showing current position			
	Low	Moderate	Good	Optimal
	Tree planting in new developments will achieve less than 10% potential tree canopy cover. 30% success of new tree establishment after 5 years.	Tree planting in new developments will achieve less than 20% potential canopy cover. 50% success of new tree establishment after 5 years.	Tree planting in new developments will achieve potential 25% canopy cover. 70% success of new tree establishment after 5 years.	Extensive tree planting and maintenance in new developments. New developments achieve the 3-30-300 rule. 90% success of new tree establishment after 5 years.

3.3 Targets, Priorities & Actions: Sustainable Resource Management

R7. Tree protection policy development & enforcement

Urban trees are sometimes viewed as irritating and costly, dropping leaves on lawns, causing blocked drains, and damaging foundations and pipes with their roots. They may also be considered dangerous as branches or whole trees can fall during storms. For all these reasons and more, people may want to remove trees from public land or private properties.

Some private trees are protected by conservation areas and Tree Preservation Orders (TPO's). Trees may also be protected as part of planning conditions associated with planning approval. Further protections for individual mature tree specimens of significant social, cultural and environmental benefit (at the council's discretion) may be beneficial. Policies must be enforced in both public and private sectors. The public must be made aware of these policies (and their consequences if violated) to ensure they are kept informed of best practice guidance and ensure they know where to find further information before undertaking any work. Working with private landowners, community groups and utilities will open vital channels of communication and collaboration.



Policy Links	Actions	Resp	Review
Local Plan Trees and Development SPD	<ol style="list-style-type: none"> 1. Ensure HBC planning department are appropriately resourced with qualified tree advice, enforcement officers. 2. Make TPO's and conservation areas publicly viewable through the councils GIS system. 3. Publicise successful prosecutions as a deterrent. 	Planning / Comms	2026

Priority	Key Performance Indicators showing current position			
	Low	Moderate	Good	Optimal
	No tree protection policy.	Policies in place to protect public trees and employ industry best management practices, but inconsistently enforced.	Policies and practices in place to protect public and private trees, generally enforced.	Integrated district-wide policies and practices to protect public and private trees, consistently enforced and supported by significant deterrents.

3.3 Targets, Priorities & Actions: Sustainable Resource Management

R8. Tree & Woodland funding

Funding may be internal or external. Typically internal funding should cover baseline costs of tree management, staffing, standard levels of tree replacement, etc., whilst external funding is often available on a project basis, or for a specific use.

Funding is often readily available for tree planting schemes, but establishment and maintenance funding is more difficult to find. Therefore, internal funding may need to cover more of this.

External funds may come from:

- Central government and governmental organisations,
- Lottery and major NGOs,
- Biodiversity Net Gain and other planning related funding (Section 106 Agreements),
- Green finance initiatives and private investment.



Policy Links	Actions	Resp	Review
Hartlepool Regeneration Plan	<ol style="list-style-type: none"> 1. Complete a Cost Benefit Analysis on Hartlepool's Urban Forest. 2. Measure whether funding is sufficient and how it can be compared Regionally. 3. Ensure trees are included in any budgeting and resource planning. 4. Cost up the actions arising from this master plan and secure funding. 5. Identify and review external funding sources, mechanisms - including Community Forest. 	Planning	2026

Priority	Key Performance Indicators showing current position			
	Low	Moderate	Good	Optimal
	Little or no dedicated funding.	Funding only for emergency, reactive management.	Funding sufficient for some proactive management based on tree & woodland management plan.	Sustained funding from public and private sources to fully implement the master plan.

3.3 Targets, Priorities & Actions: Sustainable Resource Management

R9. Tree & Woodland program capacity & staffing

Adequate staffing means having enough staff with the correct training and experience to carry out all necessary tasks relating to the implementation and day-to-day running of the urban forestry programme. This may mean hiring new staff or arranging further training for current staff.

This includes anyone involved in the delivery of tree planting, establishment, management, planning and implementation, including Tree Wardens and other volunteers, tree officers, parks and countryside staff, etc.



Policy Links	Actions	Resp	Review
	<ol style="list-style-type: none"> 1. Complete a Cost Benefit Analysis on Hartlepool's Urban Forest. 2. Conduct a review of staffing levels and how it can be compared regionally to ensure it is suitable for the needs of the urban forest and this master plan. 3. Utilise the Hartlepool Tree Warden scheme for projects and minor low risk works. 	Planning Hort Parks	2026

Priority	Key Performance Indicators showing current position			
	Low	Moderate	Good	Optimal
	Team severely limited by lack of personnel and/or access to adequate equipment. Unable to perform adequate maintenance, let alone implement new goals.	Team limited by lack of staff and/or access to adequate equipment.	Team able to implement many of the goals and objectives of the tree & woodland management plan.	Team able to implement all of the goals and objectives of the tree & woodland management plan.

3.3 Targets, Priorities & Actions: Sustainable Resource Management

R10. Tree establishment planning & implementation

Urban environments are challenging for trees, so selecting the right species for each site is essential, and should follow Right Tree, Right Place, Right Reason, Right Care. Assessing local conditions - such as soil, root space & water availability - is vital for selecting suitable species. A species selection guide, tailored to Hartlepool's current and future climate, and its geography will support more consistent and effective planting across the borough. For use by HBC, external land managers and community groups, a guide will foster species diversity and support resilience within the treescape to climate change and pests & disease.

BS 8545 (Trees: from nursery to independence in the landscape) and BS 5837 (Trees in relation to design, demolition and construction) should be used to guide decisions around site suitability and be used within developments at the planning stage, rather than as an afterthought.



Policy Links	Actions	Resp	Review
	<ol style="list-style-type: none"> 1. Complete a comprehensive prioritised tree planting plan based on opportunity mapping. 2. Ensure all stakeholders use right tree, right place philosophy with it embedded in all future HBC tree planting programmes. 3. Provide tailored tree planting advice to different stakeholders (corporate, utility, community etc) on tree planting to include a list of 'preferred' species, size, planting and maintenance requirements. 4. Report success and communicate tree establishment - not just planting. 	<p>Hort</p> <p>Planning</p> <p>Comms</p>	<p>2027</p>

Priority	Key Performance Indicators showing current position			
	Low	Moderate	Good	Optimal
	Little or no tree planting; tree establishment is ad hoc.	Some tree planting and establishment occurs, but with limited overall district-wide planning and post-planting care.	Tree planting plan is guided by district-wide goals, with some post-planting establishment care.	Comprehensive tree establishment plan is guided by needs derived from canopy and other assessments, maintains species and age diversity, includes both planting and young tree care, and is sufficient to make progress toward canopy cover objectives. Engages the community to care for thier local trees.

3.3 Targets, Priorities & Actions: Sustainable Resource Management

R11. Tree growing site suitability

Trees are often chosen for how they look, but if site conditions aren't right, they can struggle or fail. Successful planting starts with understanding the place where a tree will grow, not just its appearance.

Assessment should begin below ground. Urban soils are often poor quality or missing altogether, so it's vital to know what is present and what improvements may be needed before planting. Other important factors include the amount of sunlight a site receives, how much hard surface surrounds it (which affects water reaching the roots), and whether there is enough space both above and below ground for a tree to grow to maturity.

Once these conditions are understood, the right tree can be matched to the site. Clear planting guidelines should be in place to help trees reach their full potential and deliver maximum benefit. Trees must be treated as a priority in both public and private development, rather than an afterthought, so they are given the space and conditions they need to thrive long term.



Policy Links	Actions	Resp	Review
	<ol style="list-style-type: none"> 1. Ensure all stakeholders use right tree, right place, right reason philosophy with it embedded in all future HBC tree planting programmes. 2. Promote the selection of tree species best suited to the site and local conditions, prioritising suitability over whether the tree is native or non native. 3. Develop a phased plan of sites identified by opportunity mapping . 4. Develop borough-wide guidelines for the improvement of planting site conditions and selection of suitable species. 	Hort / Planning	2027

Priority	Key Performance Indicators showing current position			
	Low	Moderate	Good	Optimal
	Trees selected and planted without consideration of site conditions.	Appropriate tree species are considered in site selection.	District-wide guidelines in place for the improvement of planting site conditions and selection of suitable species.	All trees planted in sites with adequate soil quality and quantity, and with sufficient growing space and overall site conditions to achieve their genetic potential and thus provide maximum ecosystem services.

3.3 Targets, Priorities & Actions: Sustainable Resource Management

R12. Maintenance of publicly owned trees

Intensively managed trees include street trees and solitary park trees, which require more care and attention due to the additional stresses and challenges of urban life. In order to ensure the safety of public trees, routine maintenance must be carried out. This includes planned cyclical inspections and appropriate maintenance. Tree inventories will also help when recording maintenance schedules and works carried out.

The frequency of land use and target value are key drivers for inspection frequency, therefore an understanding of public land use may be a useful tool in establishing an inspection rota. This could help prioritise monitoring and management.

Monitoring trees could help prevent the spread of diseases, the likelihood of falling limbs, and resolve issues such as roots heaving pavements, or tree guards and stakes being left too long and causing extensive damage.



Policy Links	Actions	Resp	Review
Tree Risk Evaluation and Management Policy	<ol style="list-style-type: none"> 1. Update and develop the Hartlepool Tree Risk Evaluation and Management Policy to include current Tree Risk Methodologies. 2. Training and standardisation of tree maintenance methods for HBC arborists. 3. Decisions on work, prioritised on tree risk, informed by a recognised tree risk methodology. 4. Create and adopt a publicly accessible FAQs sheet for frequent tree requests. 	Planning / Hort	2026

Priority	Key Performance Indicators showing current position			
	Low	Moderate	Good	Optimal
	No maintenance of publicly owned trees, or on a reactive basis only.	Publicly owned trees receive only periodic inspection and maintenance.	Publicly owned trees are inspected and proactively maintained on a cyclical basis.	All publicly owned, intensively managed trees are routinely and thoroughly maintained on ongoing basis according to a comprehensive management plan.

3.3 Targets, Priorities & Actions: Sustainable Resource Management

R13. Management of publicly owned natural areas

Extensively managed trees are trees in parks, woodlands and other natural areas which are often allowed to grow more naturally and freely than intensively managed trees. These areas still require management to provide a healthy and diverse green space. They are often used by the public and therefore risk management is a key consideration. The frequency of land use and target value are key drivers for inspection frequency, and any inspection rota should accommodate this to prioritise monitoring and management.

Monitoring and managing these areas could help prevent the spread of diseases, reduce the level of risk, and resolve other issues associated with 'unmanaged' areas.



Policy Links	Actions	Resp	Review
Hartlepool Tree Risk Evaluation and Management Policy NTSG Common sense risk management of trees	1. Update and develop the Hartlepool Tree Risk Evaluation and Management Policy to include current Tree Risk Methodologies.	Planning	2026

Priority	Key Performance Indicators showing current position			
	Low	Moderate	Good	Optimal
	No natural areas management plans or implementation in effect.	Only reactive management efforts to facilitate public use (e.g., hazard abatement, trail maintenance).	Management plan in place for each publicly owned natural area to facilitate appropriate public use.	Management plan for each publicly owned natural area focused on sustaining and, where possible, improving overall ecological integrity (i.e., structure and function) – while facilitating public use.

3.3 Targets, Priorities & Actions: Sustainable Resource Management

R14. Tree risk management

Risk management is vital in an urban setting; the number of people interacting with trees on a daily basis increases the likelihood of incidents involving trees. Risks of trees include falling branches, toxic/poisonous leaves, berries, or seeds, pollen (inducing hay-fever), roots uplifting pavements (trip hazards) and pests (i.e. Oak processionary moth). These risks must be assessed and a Tree Risk Management Strategy implemented.

One way to reduce risk is to avoid selecting certain species; avoid planting trees with toxic fruits in areas frequented by children and dog-walkers, and avoid high pollen trees near schools and hospitals. Mitigate other risks by allowing suitable root space, managing pests, and respond quickly to reports of risks. Existing trees and new plantings should be monitored to spot these issues early and prevent any further risks developing. Zoning is a practice whereby landowners and managers define areas of land according to levels of use. This helps to prioritise the most used areas, and contributes to a cost-effective approach to tree inspection, focusing resources where most needed. Inspection methods and frequency may vary depending on whether the management strategy is intensive or extensive.



Policy Links	Actions	Resp	Review
Hartlepool Tree Risk Evaluation and Management Policy NTSG Common sense risk management of trees	<ol style="list-style-type: none"> Update and develop the Hartlepool Tree Risk Evaluation and Management Policy to include current Tree Risk Methodologies. Decisions on work, prioritised on tree risk, informed by a recognised tree risk methodology. 	Planning / Hort	2026

Priority	Key Performance Indicators showing current position			
	Low	Moderate	Good	Optimal
	No tree risk assessment or risk management program. Response is on a reactive basis only.	Level I (limited visual assessment) inspection and follow-up conducted periodically.	Level II (basic assessment) conducted periodically, resulting in scheduled follow-ups.	Level II (basic assessment) conducted routinely, according to defined cycle and intensive follow-up (i.e., priorities and timelines for mitigation established based on risk).

3.3 Targets, Priorities & Actions: Sustainable Resource Management

R15. Biosecurity

Biosecurity refers to the need to prevent new pests and diseases being introduced into the UK, or any healthy population from abroad. This measure is necessary to stop the spread of potentially devastating organisms and protect forestry, agriculture, horticulture, and biodiversity as a whole. Pests and diseases have had and will continue to have a profound effect on trees and woodlands in Britain.

The importation of trees, particularly large landscape trees, from across Europe and elsewhere can increase the risk of diseases being introduced to the UK. The 'Plant Health Portal' run by Defra provides information, guidance and management strategies, and actions to protect plant health are set out in 'Protecting Plant Health - A Plant Biosecurity Strategy for Great Britain'.

Hartlepool must take responsibility for the sourcing of its trees, and actively work to eliminate pests and diseases from its urban forest and prevent new diseases becoming prolific through regular inspections and active management.

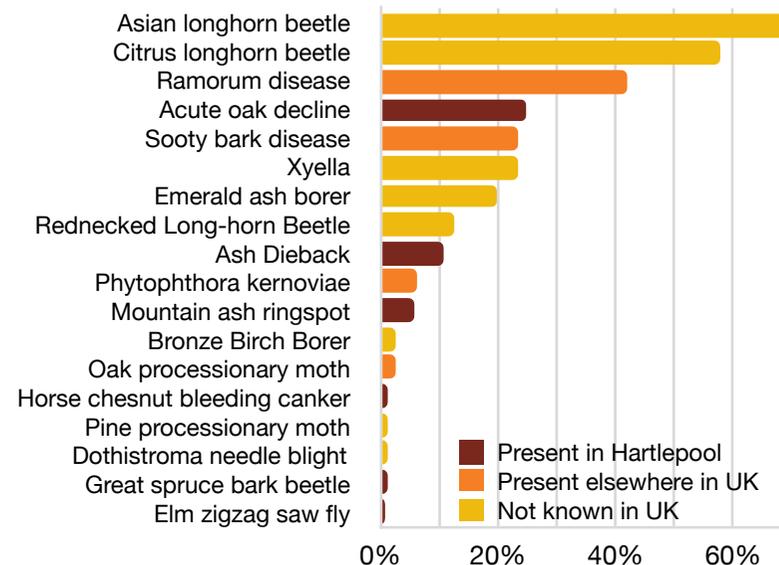


Figure 9. Share Hartlepool's trees at risk from pathogens.

Policy Links	Actions	Resp	Review
Hartlepool Tree Risk Evaluation and Management Policy, NTSG Common sense risk management of trees	<ol style="list-style-type: none"> Incorporate biosecurity into the Hartlepool Tree Risk Evaluation and Management Policy. Utilize existing seed hubs within community hubs for seed exchange. Take a citizen science approach to early warning / monitoring for pests and diseases (Observatree, HBC Tree Wardens etc) 	Planning Community Hubs & Wellbeing Planning	2026 2027 2027

Priority	Key Performance Indicators showing current position			
	Low	Moderate	Good	Optimal
	No biosecurity strategy or actions in place for urban trees.	Some consideration of biosecurity aspects, e.g., through pest management.	Biosecurity management actions are integrated in tree & woodland management.	Biosecurity strategy developed and implemented, with emphasis on urban trees. Integration of this into day-to-day tree & woodland management.

3.3 Targets, Priorities & Actions: Sustainable Resource Management

R16. Timber, Non-wood Forest Products, Green Waste

Trees are a resource in many ways, from providing ecosystem services like carbon storage and pollution removal to bearing fruits and supporting wildlife. However a trees value as a resource does not necessarily end when the tree dies or requires felling.

When appropriately managed, felled or fallen trees can continue to deliver value through the reuse of timber, the recovery of non-wood forest products, and the responsible processing of green waste. Timber can be retained within local supply chains for construction, furniture, or community projects, supporting circular economy principles. Arisings such as woodchip and mulch can contribute to soil improvement, habitat creation, and sustainable energy generation.

Recognising trees as long-term assets encourages better stewardship, reduces waste, and maximises environmental, social, and economic benefits across their entire lifecycle.



Figure 10: Uses of Urban Wood

Policy Links	Actions	Resp	Review
	<ol style="list-style-type: none"> All newly planted trees are thoroughly mulched and composted with recycled green waste. Identify artists/customers for selling timber to. Liaise with contacts to identify potential buyers eg. Northern School of Art 	Highways / Parks / Hort/ Cemeteries	2026

Priority	Key Performance Indicators showing current position			
	Low	Moderate	Good	Optimal
	No utilisation plan; wood and other green waste goes to landfill with little or no recycling and reuse.	While most green waste does not go to landfill, uses are limited to chips or mulch.	The majority of green waste is reused or recycled – for energy, products, and other purposes beyond chips or mulch.	Comprehensive plan and processes in place to utilise all green waste one way or another, to the fullest extent possible.

3.3 Targets, Priorities & Actions: Sustainable Resource Management

R17. Native / Non-native vegetation

The urban forest is a diverse place, consisting of a range of trees and plants from all over the world. Whilst diversity is key to a healthy treescape, native plants should be selected above non-native species whenever appropriate. Native species of trees, shrubs, fungi, ferns, insects, mammals, birds and more have evolved together in the UK, each finding and filling an ecological niche in which to thrive. The balance between species in an ecosystem is delicate, and just one missing link could tip the system out of balance.

Trees provide habitats for a whole range of species. In the UK, English oak (*Quercus robur*) supports more organisms than any other tree; more than 1,000 insect species have been identified to date on oaks in central Europe, with at total of 2,300 different species relying oak in the UK. Common Ash (*Fraxinus excelsior*) is frequently used as a nesting site for birds such as the redstart, nuthatch and owls, and the bark is important for certain lichen due to its pH. Without native trees, other native species may struggle to find suitable habitats and therefore suffer.



Policy Links	Actions	Resp	Review
Trees & Devt SPD	<ol style="list-style-type: none"> Promote the selection of tree species best suited to the site and local conditions, prioritising suitability over whether the tree is native or non native. Establish Native - Non-Native Guidelines for urban, rural and ecologically sensitive areas into Hartlepool's Trees & Development SPD. 	Parks / Planning	2026

Priority	Key Performance Indicators showing current position			
	Low	Moderate	Good	Optimal
	No coordinated focus on vegetation selection.	Voluntary use of native species on publicly and privately owned lands; invasive species are recognised.	Use of native species is encouraged on a project-appropriate basis in all areas; invasive species are recognised and discouraged on public and private lands.	Native species are widely used on a project-appropriate basis.

3.3 Targets, Priorities & Actions: Sustainable Resource Management

R18. Open Tree & Woodland data & Web-map

Management and assessment tools are fundamental and indispensable when considering a resource as vast as the urban forest. Being able to clearly see tree data would help the urban forest management team keep track of the trees, and make the public aware of them and their value.

A Web-map is an interactive tool for displaying information to the public about the urban forest, and data can be broken down by ward. They can display a range of things including tree benefits such as canopy cover, annual ecosystem benefits (avoided runoff, carbon sequestration, air pollution removal), and tree condition, to name a few. They are an excellent way to engage with the public and communicate benefits of trees.

In creating a public web-map, tree data could be easily communicated and compared. Ideally the tool will be accessible, easy to use and comprehensive for the entire urban forest of Hartlepool. The data should be kept up to date with the most recent reviews of the urban forest.

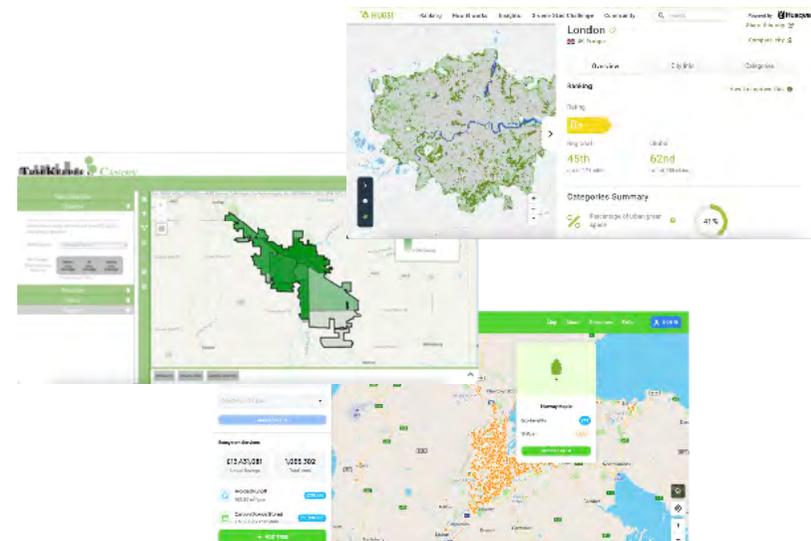


Figure 11. A selection of online mapping tools.

Top right - Hugsi (<https://www.hugsi.green/city/?London>)
 Top left - TreeKeeper Canopy (<https://canopy.treekeepersoftware.com/goshenin>)
 Bottom right - Treezilla (<https://treezilla.org>)

Policy Links	Actions	Resp	Review
	1. Create a publicly accessible and up to date inventory of TPO's. 2. Work towards a publicly viewable GIS based tree inventory of council managed trees within Highways, Open Spaces, Parks and Cemeteries.	Planning	2026 2027

Priority	Key Performance Indicators showing current position			
	Low	Moderate	Good	Optimal
	No open source mapping resource is available to the public.	Information on some elements of the treescape is available through a web map.	Large parts of the district's treescape are presented through web maps.	HBC has a clear and openly accessible tree management portal for all tree data.

3.3 Targets, Priorities & Actions: Sustainable Resource Management

R19. Reviewing & updating the Master Plan

The Urban Forest Masterplan outlines a vision for the development of the urban tree resource. It provides a long-term framework in which strategic plans can be developed. With this UFMP in place, tree establishment programmes and long term management approaches can be focused and strategised, as well as better guiding the achievement of the long-term vision.

With agreement on an ambitious vision, the UFMP can be divided into management periods, with goals and targets clearly outlined. A series of performance indicators are then be put into place to monitor performance and help progress towards the achievement of goals and the wider vision. It is important that progress is monitored and reviewed on a regular basis, and actions modified as necessary. This way, HBC can focus on the most relevant and urgent areas going forward.

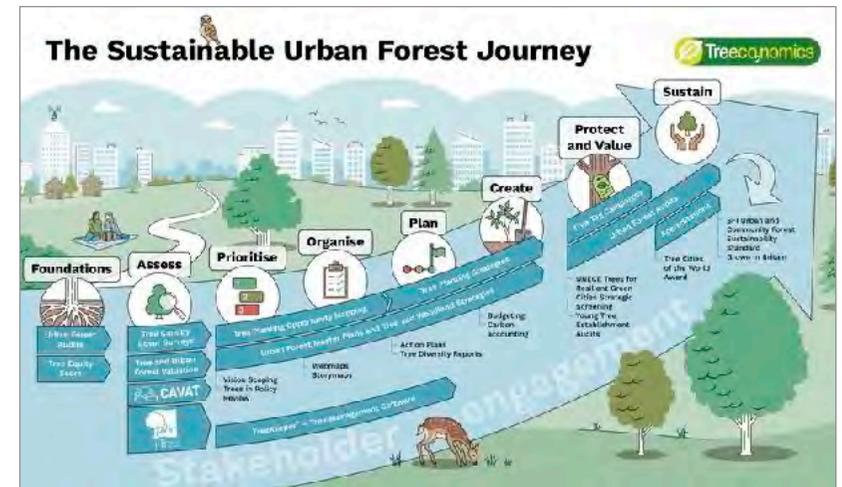


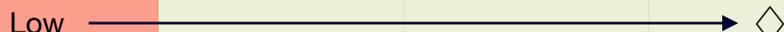
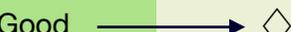
Figure 12. Poster of 'The Sustainable Urban Forest Journey' from Treeconomics

Policy Links	Actions	Resp	Review
Council Plan	1. 5 yearly review of this master plan to ensure it is fit for purpose.	Planning	2031

Priority	Key Performance Indicators showing current position			
	Low	Moderate	Good	Optimal
	No plan.	Existing plan limited in scope and implementation.	Recent comprehensive plan developed and implemented for publicly owned forest resources, including trees managed intensively (or individually) and those managed extensively, as a population (e.g., trees in natural areas).	Strategic, multi-tiered plan with built-in adaptive management mechanisms developed and implemented for public and private forest resources.

3.3 Targets, Priorities & Actions: Sustainable Resource Management

This document represents the first step of an action plan, and will be subject to ongoing improvement and updating. For each target within the plan further detail will be added, projects will be planned and actioned to take HBC towards its vision.

Key Performance Indicator	Performance Level				Priority
	Low	Moderate	Good	Optimal	
R1 Publicly Owned Tree & woodlands inventory				Optimal 	
R2 Privately Owned Tree & woodlands inventory	Low 				
R3 Tree valuation & asset management approach		Moderate 			
R4 Canopy cover assessment & goals		Moderate 			
R5 Tree Equity	Low 				
R6 Trees & Development		Moderate 			
R7 Tree protection policy development & enforcement			Good 		
R8 Tree & Woodland funding		Moderate 			
R9 Tree & Woodland program capacity & staffing		Moderate 			
R10 Tree establishment planning & implementation		Moderate 			

3.3 Targets, Priorities & Actions: Sustainable Resource Management

Sustainable Resource Management Summary

Key Performance Indicator	Performance Level				Priority
	Low	Moderate	Good	Optimal	
R11 Tree growing site suitability		Moderate		→ ◇	
R12 Maintenance of publicly owned trees		Moderate	→ ◇		
R13 Management of publicly owned natural areas			Good → ◇		
R14 Tree risk management		Moderate		→ ◇	
R15 Biosecurity		Moderate		→ ◇	
R16 Timber, Non-wood Forest Products & Green Waste			Good → ◇		
R17 Native / non-native vegetation					
R18 Open Tree & Woodland data & Web-map	Low → ◇				
R19 Reviewing & updating the Master Plan	Low → ◇				

4. Appendices

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4.2 Glossary

Abbreviations

BAP - Biodiversity Action Plan

BSI - British Standards Institution

CAVAT - Capital Asset Valuation for Amenity Trees

ES - Ecosystem Services

ISA - International Society of Arboriculture

LPA - Local Planning Authority

NFI - National Forest Inventory

NGO - Non-Governmental Organisation

NTSG - National Tree Safety Group

NTM - National Tree Map

RTC - Relative Tree Canopy

SSSI - Site of Special Scientific Interest

TES - Tree Establishment Strategy

TPO - Tree Preservation Order

TDAG - Trees and Design Action Group

UFMP - Urban Forest Master Plan

USDA - United States Department of Agriculture

Glossary of Terms

Arboriculture- The selection, production, planting, maintenance, and removal of all woody plants for amenity purposes.

Biodiversity- A measure of biological variation, whether represented by gene, species, habitats or ecosystems.

Biosecurity- A set of precautions to reduce the risk of accidentally introducing or spreading alien invasive species, including potential pests and pathogens.

Canopy Cover- A 2-dimensional metric quantifying the area of ground covered by tree canopy when viewed from above, where tree canopy is the collective branches and foliage of the tree.

Carbon sequestration- Processes that remove carbon from the atmosphere.

Carbon storage - The amount of carbon bound up in the above-ground and below-ground parts of woody vegetation.

Community forestry- Addresses the social benefits of the urban forest: community pride, community planting and care projects, reduction of violent crimes and a sense of safety.

Conservation- Use, management and protection of natural resources that insures use and enjoyment for future generations

Ecosystem- A unit of ecology consisting of a more or less discrete community of species, interacting with each other and their physical environment.

Ecosystem services- The ways in which humanity relies on ecosystems for the continued provision of clean air, drinking water, an equitable climate, the productivity of agriculture, forestry and oceans, control of flooding, soil erosion, coastal erosion, carbon sequestration etc.

Environment- The prevailing conditions which reflect the combined influence of climate, soil, topography and biology (other plants and animals) present in an area.

GIS (Geographic information system)- A collection of computer hardware, software, and geographic data for capturing, storing, updating, manipulating, analysing and displaying all forms of geographically referenced information.

4.2 Glossary

Green infrastructure (GI)- An interconnected network of waterways, wetlands, woodlands, greenways, parks, forests, and other open spaces that support native species, maintain natural ecological processes, sustain air and water resources and contribute to health and quality of life. Includes parks, parkways, riparian buffers, residential landscaping, street trees, rain gardens, green roofs, and window boxes.

Green roof- A specially designed roof that incorporates plants. Depending on the structural capacity of the building, depth and type of soil, and desired maintenance. Green roofs can be planted with anything from sedums to trees.

Green space- Any vegetated land or water within an urban area that serves as recreation or open space. This includes neighbourhood and regional parks, gardens, cemeteries, playing fields, bike and walking paths, and urban landscaping.

Greenway/green corridor- Corridor composed of natural vegetation. Greenways can be used to create connected networks of open space that include traditional parks and natural areas.

Habitat- Food, water, shelter and space that supports plant or animal life.

Infiltration- The downward movement of water from the land surface into the soil.

Inventory, Tree- Gathering of accurate information on the health and diversity of the community forest which can include: listing and description of trees and planting sites.

Microclimate- The climate of a site as modified by local site factors.

Native species- Species present in a defined region for a certain amount of time without having been brought by humans (cf. exotic), for instance in Britain since the English Channel was flooded around 6,000 years ago.

Non-native species- A species that due to direct or indirect human activity occurs in locations beyond its known historical or potential natural range. Refers to species from another continent, region, ecosystem, or habitat.

Pollution- Substances introduced into the environment by human actions that contaminate the environment.

Stormwater runoff- Precipitation that falls on impervious surfaces (such as roofs and roads). Because it is not absorbed by soil and vegetation, it flows into storm drains.



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Neighbourhoods and Regulatory Services Committee

2 March 2026

Report of: Director for Neighbourhood and Regulatory Services

Subject: PLANNING OBLIGATIONS SUPPLEMENTARY PLANNING DOCUMENT (SPD) AND HOUSES IN MULTIPLE OCCUPATION (HMO) ARTICLE 4 DIRECTION AND SPD UPDATE.

Decision Type: Key decision (DNRS 114 / 26)

1. Council Plan Priority

Hartlepool will be a place:
where people live healthier, safe and independent lives. (People)
that is connected, sustainable, clean and green. (Place)
that is welcoming with an inclusive and growing economy providing opportunities for all. (Potential)
with a Council that is ambitious, fit for purpose and reflects the diversity of its community. (Organisation)

2. Purpose of Report

2.1. The purpose of this report is to update members on the progress of three key consultations undertaken by the Land Use Policy Team in recent months. The consultations focused on:

- A borough wide Houses in Multiple Occupation (HMO) Article 4 Direction.
- A HMO Supplementary Planning Document (SPD)
- A Planning Obligations Supplementary Planning Document (SPD).

2.2. The report also seeks permission to, consult upon the two SPDs for a further 8 week period, this is to allow for the publication of the Consultation Statement and to allow additional time to obtain residents and stakeholders view, to, in consultation with the Director of Neighbourhood and Regulatory Services and the Chair of committee update the two draft SPDs and submit to Full Council for Adoption in May 2026.

3. Background

3.1. In summer 2025 the Land Use Policy Teams began preparing for the process of consulting upon a borough wide HMO Article 4 Direction, a HMO SPD and an update to the 2015 Planning Obligations SPD. This process involved liaison with key HBC officers such as the legal team, highways team, licensing team, the drafting of documents and then formal consultation.

3.2. On 30th June 2025 a report was taken to Neighbourhood Services Committee to inform members of a proposed statutory consultation for potentially introducing a HMO Article 4 Direction within Hartlepool. The report was in response to concerns raised by members of the public and elected members in relation to HMOs across the borough.

3.3. On 20th October 2025 two reports were taken to Neighbourhood Services Committee for permission to consult upon a draft HMO SPD and the updated Planning Obligations SPD. The reports detailed that there is a need to introduce HMO guidance to ensure that applicants are aware of what will be expected should they seek planning permission for a HMO in Hartlepool. It was also set out that the 2015 Planning Obligations SPD required an update due to inflation, to provide more clarity to residents and stakeholders and to set out the parameters and information relating to Discount Market Value Affordable Homes.

3.4. With regard to the Article 4 Direction, a 12 month statutory consultation commenced on 24th September 2025, this consultation is necessary to ensure the Article 4 Direction can be implemented lawfully. In addition to the statutory 12 months consultation, the council undertook a 17 week (24th September 2025 to 23rd January 2026) information seeking consultation to gain residents and stakeholders views.

- 3.5. With regard to the two SPDs covering HMO Standards and Planning Obligations, information seeking consultations were undertaken for 11 weeks (3rd November 2025 to 16th January 2026) in a bid to obtain residents and stakeholders views.
- 3.6. For all of the consultations the HBC platform Your Say was utilised and consultation notifications were sent to those signed up to the platform. Key stakeholders and those within the Land Use Policy consultation database who have requested to be informed about changing HBC policy and guidance were sent e mails with all the key information and survey links contained. The consultations were advertised in Hartbeat magazine, a press release was issued, and documents were available online and in Hartlepool Civic Centre. Officers received a number of phone calls in which the consultations were discussed.
- 3.7. The consultations have now closed, but, as stated in Section 3.4, the statutory HMO Article 4 Direction consultation remains in place until 24th September 2026.

4. Proposals

- 4.1. Officers seek to undertake an additional eight week consultation period in order to publish the consultation statements that were drafted alongside the draft SPDs but not available at the first consultation period. Officers also consider it prudent to undertake additional consultation to ensure residents and stakeholders have sufficient time to comment upon and assist in shaping the SPDs prior to adoption.
- 4.2. The council is proposing to adopt the two SPDs in June 2026 and may bring into force the HMO Article 4 Direction on 1st December 2026 subject to Member agreement. A decision on adopting the SPDs will be taken by Full Council, a decision on how to proceed with the HMO Article 4 Direction will be brought to Neighbourhoods and Regulatory Services Committee and then Full Council in due course.
- 4.3. Officers are in the process of addressing the comments received to draw out key points. A comprehensive assessment and response to all comments will be included within the legally required consultations statements that will be provided alongside future Council reports to formally adopt the SPDs.
- 4.4. The paragraphs below set out information relating to the engagement undertaken to date and a summary of the key issues raised.

Houses in Multiple Occupation Article 4 Direction

4.5. 331 online responses were received; no letters were received to date, consultation is still open.

4.6. The key issues raised were as follows:

- 269 respondents were in support of introducing the HMO Article 4 Direction (81%).
- 62 respondents were not in support of introducing the HMO Article 4 Direction (18%).
- HMOs give rise to anti-social behaviour.
- HMOs take up car parking spaces and put pressure on the network.
- HMOs reduce community cohesion as people come and go.
- HMO Article 4 Direction will allow the council better control.
- HMO Article 4 Direction will give residents a voice and the chance to have their say.
- HMOs are needed for those who can't afford private rent.
- More affordable homes are needed across the borough.

Houses in Multiple Occupation Supplementary Planning Document

4.7. 30 online responses were received along with 1 letter from Historic England.

4.8. The key issues raised were as follows:

- Broad support for the draft SPD to be used as a tool to better manage and maintain HMOs.
- Broad support for avoiding the sandwich effect of HMOs with non HMO residential properties.
- Broad support that there should be no more than 3 HMOs in a row.
- Those disagreeing with limiting to no more than 3 in a row mostly think there should be fewer than this.
- Uncertainty about whether HMOs above commercial units is appropriate.
- General preference for there to be specific parking requirements for HMOs rather than flexibility.
- Concerns raised regarding parking pressures, antisocial behaviour and that there is already perceived to be too many HMOs in the town.

Planning Obligations SPD Supplementary Planning Document

4.9. 14 online responses were received and 2 letters were submitted. Letters submitted were from Sport England and Lichfileds (planning agent) on behalf of Taylor Wimpey.

4.10. The key issues raised were as follows:

- Should prioritise brownfield sites.
- There is a need for updated guidance from the council.
- Priorities for s106 spending –affordable housing, green infrastructure and community facilities.
- Broad support for play facilities.
- Broad support for outdoor sport (tennis courts and bowling greens).
- Broad support for the council’s approach to green infrastructure provision.
- Broad support for contributions towards preserving heritage assets.

5. Other Considerations/Implications

Risk Implications	There are identifiable risks associated with: (a) the Article 4 confirmation and scope (including potential Secretary of State modification/cancellation and Judicial Review risk), (b) the adoption of SPDs (procedural Judicial review risk if statutory steps are missed), and (c) operational resourcing due to an increase in planning applications once the Article 4 takes effect. Mitigations include adherence to Schedule 3 GPDO procedure, robust evidence justifying geography (smallest area necessary), legal review of notices. (d) weight given to the SPDs if they are not adopted by the Government’s deadline of 30th June 2026.
Financial Considerations	There are limited financial implications, including statutory advertising and site notice costs for the Article 4 making/confirmation, potential additional casework costs due to increased planning applications following confirmation, and publication costs for SPD adoption. These will be managed within existing budgets.
Subsidy Control	None
Legal Considerations	The work relating to the HMO Article 4 Direction has and will continue to be in accordance with HMO Article 4(1) of the Town and Country Planning (General Permitted Development) (England) Order 2015 (as amended).

	<p>The work relating to the SPDs has been and will continue to be in accordance with the Planning and Compulsory Purchase Act (2004) (as amended) and The Town and Country Planning (Local Planning) (England) Regulations 2012.</p> <p>The Leveling up and Regeneration Act (2023) allows for the provision of regulations that will alter the way that Local Development Framework Documents (LDF) are produced. On 27th November 2025 the Government issued guidance on what to expect within the regulations when they are brought into force in spring 2026. Of note is that the Government intends to set out that SPDs cannot be adopted after the 30th June 2026. If this deadline is not achieved then it could be argued that the SPDs do not hold weight and cannot be relied upon in decision making.</p>
Single Impact Assessment	<p>During the preparation of the HMO Article 4 Direction and the draft SPDs along with during the consultation process, officers had regard to the protected characteristics within the Equality Act (2010) and the additional protected characteristics that HBC deem appropriate (care leavers, armed forces and poverty and disadvantage). Officers are of the view that the drafts, consultation and thus update reports have no negative impact upon protected characteristics but could in fact have a positive impact upon some protected characteristics by virtue of creating a better environment for all, including those with protected characteristics.</p>
Staff Considerations	None
Asset Management Considerations	None
Environment, Sustainability and Climate Change Considerations	<p>The report provides an update following the closure of the consultations and provides a time frame on when members are likely to be asked to progress the HMO Article 4 Direction and/or adopt the SPDs. This report has no environmental, sustainability and climate change impact however any such will be discussed when the HMO Article 4 Direction and SPDs are discussed at a future committee.</p>
Consultation	<p>Statutory consultation on the HMO Article 4 Direction is ongoing but will cease on 24th September 2026. The information seeking consultation has now closed.</p>

	<p>Consultation on the two SPDs ran from 3rd November to 16th January 2026. A second consultation is proposed from 3rd March for an 8 week period.</p> <p>Engagement with residents and key stakeholders will continue in an informal and ad hoc manner up until the implementation of the HMO Article 4 Direction.</p>
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6. Recommendations

- 6.1. Members note the content of the report.
- 6.2. Members allow officers to consult upon the two SPDs for an eight week period.
- 6.3. Members agree to delegate to the Director of Neighbourhood and Regulatory Services, to consider representations and make modifications to the SPDs, in consultation with the Committee Chair.
- 6.4. That Members agree to permit the modified SPDs to be presented to Full Council for adoption In May/June 2026.

7. Reasons for Recommendations

- 7.1. To ensure members are kept informed of SPD progress, to ensure that the correct permission is in place prior to consultation and to ensure that the SPD can be adopted in June 2026.

8. Background Papers

- 8.1. Neighbourhood Services Committee report 30th June 2025 (Article 4 Direction).
- 8.2. Neighbourhood Services Committee report 20th October 2025 (Two SPDs).

9. Contact Officers

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Land Use Policy Team Leader

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Sign Off:-

Chief Executive	Date: 10/02/2026
Director of Finance, IT and Digital	Date: 06/02/2026
Director of Legal, Governance and HR	Date: 19/02/2026

Hartlepool Borough Council – Single Impact Assessment Form

5.1 Appendix 2

Guidance for completing this form is available in the “Single Impact Assessment: Toolkit for Officers”, available from the Single Impact Assessment page on the intranet at <https://hbcintranet/Pages/Single-Impact-Assessments.aspx>.

Section 1 – Details of the proposed action being considered

1.1 Lead Department:	Neighbourhood and Regulatory Services
1.2 Lead Division:	Land use Policy

1.3 Title of the proposed action:
Urban Forest Master Plan

1.4 Brief description of the proposed action:
<p>The Tree Equity for Hartlepool multi-year project led by Trees for Cities and will begin closing the nature gap by planting at least 10 hectares of trees in nature-deprived areas and connect 1,500 local people to nature through tree planting and community engagement. This project is part-funded by the National Lottery Heritage Fund and The Linbury Trust with a grant of £250,000, who are supporting the first two years of work in a much bigger journey to tackle this low tree equity.</p> <p>Hartlepool suffers from a markedly low tree canopy cover, with the second lowest Tree Equity Score for any local authority in England. Tree Equity for Hartlepool is the first project in the UK to use the Tree Equity Score UK tool comprehensively and town-wide, to form rigorous plans for change for communities where trees are needed most. Beyond tree canopy cover, the tree equity tool highlights other environmental factors such as heat severity and air pollution, taking a holistic view on which areas are in greatest need of green investment. To meet legally binding environmental targets, we need to increase tree cover nationally to 16.5% by 2050.</p>

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5.1 Appendix 2

The Urban Forest Master Plan sets out a strategic framework to identify, prioritise, and plan for the long-term planting, maintenance, and management of Hartlepool’s trees, extending well beyond the initial two-year project period.

The proposed action is to:

- Confirm the key challenges and targets for tree equity;
- Establish Key Performance Indicators (KPIs) to measure progress;
- Endorse the recommended actions to improve tree equity across Hartlepool; and
- Approve the release of the Urban Forest Master Plan for public consultation.

1.5 Who else is involved:

Four HBC officers assessed the scheme and agreed to the findings of this assessment. The project is being led by Treeconomics through grant funding provided by the National Lottery Heritage Fund. There is an internal working group of HBC officers together with Tree for Cities and the Woodland Trust. Consultation events with elected members and external stakeholders has also taken place.

1.6 Who will make the final decision about the proposed action:

Neighbourhoods and Regulatory Services committee and the Housing, Growth and Communities committee

1.7 Which wards will be affected by the proposed action? Tick all that apply

All wards	<input checked="" type="checkbox"/>	Hart	<input type="checkbox"/>	Seaton	<input type="checkbox"/>
Burn Valley	<input type="checkbox"/>	Headland & Harbour	<input type="checkbox"/>	Throston	<input type="checkbox"/>
De Bruce	<input type="checkbox"/>	Manor House	<input type="checkbox"/>	Victoria	<input type="checkbox"/>
Fens & Greatham	<input type="checkbox"/>	Rossmere	<input type="checkbox"/>	N/A - Internal council activities	<input type="checkbox"/>
Foggy Furze	<input type="checkbox"/>	Rural West	<input type="checkbox"/>		

Hartlepool Borough Council – Single Impact Assessment Form

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Appendix 2

1.8 Completed By:		
Name	Job Title	Date Completed
Scott Watson	Arboricultural Officer	09/12/2025

1.9 Version	Author	Summary of Changes	Date
1.0	Scott	Original version	09/12/2025

Section 2 – Explaining the impact of the proposed action

2.1 What data and evidence has informed this impact assessment?

- Department of Health & Social Care (2025). *Local Authority Health Profile – Hartlepool*. London: DHSC.
- Environment Act (2021). *Environment Act 2021*. London: The Stationery Office.
- Hartlepool Members Seminar - Tree Strategy on 01/12/2025 at 1730.
- External Stakeholders Workshop - Tree Strategy on 02/12/2025 at 0900.
- UFMP1 - Forest Research (2025). *The benefits of trees and woodlands for people, climate and environment: an evidence review*. Farnham: Forest Research.
- UFMP2 - Li, Y., Hsu, J.A. and Fernie, G., (2013). Aging and the use of pedestrian facilities in winter—the need for improved design and better technology. *Journal of urban health*, 90(4), pp.602-617.
- UFMP3 – Department for Transport. (2021). *Inclusive Mobility: A Guide to Best Practice on Access to Pedestrian and Transport Infrastructure*. London: Department for Transport.
- UFMP4 - Hajek, A., König, H.H., Buczak-Stec, E., Blessmann, M. and Grupp, K., (2023). *Prevalence and determinants of depressive and anxiety symptoms among transgender people: results of a survey*. In *Healthcare* (Vol. 11, No. 5, p. 705). MDPI.
- UFMP5 - Downward, P., Rasciute, S. and Kumar, H., (2022). *Mental health and satisfaction with partners: a longitudinal analysis in the UK*. *BMC psychology*, 10(1), p.15.
- UFMP6 - Donovan, G.H., Prestemon, J.P., Kaminski, A.R. and Michael, Y.L., (2025). *The association between tree planting and birth outcomes*. *Science of The Total Environment*, 975, p.179229.
- UFMP7 - Singh, S., van de Wouw, M., Tomfohr-Madsen, L., Lebel, C. and Giesbrecht, G.F., (2025). *The role of green and blue spaces in perinatal maternal mental health outcomes during the transition to parenthood*. *Environment International*, p.109572.
- UFMP8 - Rüttenauer, T., Bader, F., Ehler, I. and Best, H., (2025). *Breathing unequal air: environmental disadvantage and residential sorting of immigrant minorities in England and Germany*. *Social Forces*, p.soaf032.
- UFMP9 - Donovan, G.H., Derrien, M., Wendel, K. and Michael, Y.L., (2024). *Old lessons for new science: How sacred-tree metaphors can inform studies of the public-health benefits of the natural environment*. *Heliyon*, 10(15).
- UFMP10 - Make Space for Girls. (2025) *Make Space for Girls Research Report*. Make Space for Girls.

- UFMP11 - Tamura, K., Deng, Y., Rogers, B., Moniruzzaman, M., Jagannathan, R., Hu, L., Miura, K., Roger, V.L. and Mariño-Ramírez, L., (2025). *Neighbourhood deprivation and cardiometabolic outcomes in the UK Biobank: differences by sex and ethnicity*. *Open Heart*, 12(1).
- UFMP12 - Moagi, M.M., van Der Wath, A.E., Jiyane, P.M. and Rikhotso, R.S., (2021). *Mental health challenges of lesbian, gay, bisexual and transgender people: An integrated literature review*. *Health SA Gesondheid*, 26(1).
- UFMP13 - ARUP & University of Westminster. (2025) *Queer Perspectives on Public Space*. London: ARUP.
- UFMP14 - National Children’s Bureau. (2021) *Mental Health Inequalities and Social Deprivation*. London: NCB.
- UFMP15 - Wheeler M, Cooper NR, Andrews L, Hacker Hughes J, Juanchich M, et al. (2020) *Outdoor recreational activity experiences improve psychological wellbeing of military veterans with post-traumatic stress disorder: Positive findings from a pilot study and a randomised controlled trial*. *PLOS ONE* 15(11): e0241763.
- UFMP16 - Office for National Statistics. (2025) *English Indices of Deprivation 2025*. London: ONS.
- UFMP17 - American Forests, Centre for Sustainable Healthcare and Woodland Trust (2025) *Tree Equity Score UK*, Hartlepool.

2.2 If there are gaps in evidence or not enough information to assess the impact, how have you addressed this or how will you address it?	
Gap(s) Identified	How it / they have or will be addressed
Public consultation.	Through public consultation of the Urban Forest Master Plan for 8 weeks during 2026.

Hartlepool Borough Council – Single Impact Assessment Form

5.1 Appendix 2

2.3 Risk Score

Impact	Negative Impact Score	Explanation – what is the impact?
Age		
<input type="checkbox"/> Positive Impact	Likelihood score: N/A e.g. Almost certain 4 Impact score: N/A e.g. Major 3 Overall score: N/A e.g. Red 12	Trees have positive impacts across all age groups, with the aims of reducing health risks for older residents, improving development and wellbeing in children, and supporting active travel and mental health in working age adults. (UFMP1) Accessibility is an issue for elderly people when it comes to times of years when leaf fall is prevalent (Autumn/Winter), the right tree in the right place for the right reason in an underlying theme of the UFMP and new planting schemes. (UFMP2)
Disability		
<input type="checkbox"/> Positive Impact	Likelihood score: N/A Impact score: N/A Overall score: N/A	Trees improve accessibility, reduce environmental health risks, increase participation, and provide physical, mental and social wellbeing benefits. The effects are particularly significant in areas of Hartlepool with low canopy cover where disabled residents already experience poorer health outcomes and reduced access to quality green space. (UFMP1) Pathway widths which are reduced because of street trees can be an obstacle and therefore will be in alignment with the Department for Transport Inclusive Mobility - A Guide to Best Practice on Access to Pedestrian and Transport Infrastructure 2021. (UFMP3)
Gender Reassignment		
<input type="checkbox"/> Positive Impact	Likelihood score: N/A Impact score: N/A Overall score: N/A	High quality urban green space improves mental wellbeing and reduces stress across population groups; trans communities, who face higher mental health burden, are likely to benefit disproportionately from these gains. (UFMP4)

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5.1 Appendix 2

Impact	Negative Impact Score	Explanation – what is the impact?
Marriage and Civil Partnership		
<input type="checkbox"/> Positive Impact	Likelihood score: N/A Impact score: N/A Overall score: N/A	<p>Trees and the Urban Forest Master Plan will have a slight positive impact on marriage and civil partnerships. Research shows that investing in mental health as an asset can improve the well-being of individuals through raising satisfaction with partners which can be amplified across partnerships. (UFMP5)</p> <p>High quality urban green space improves mental wellbeing and reduces stress therefore people who are married and/or are in a civil partnership are likely to benefit disproportionately from these gains. (UFMP1)</p>
Pregnancy and Maternity		
<input type="checkbox"/> Positive Impact	Likelihood score: N/A Impact score: N/A Overall score: N/A	<p>Trees improve air quality, important for pregnant women and infants because poor air quality is associated with low birthweight and respiratory problems. (UFMP6)</p> <p>Shaded, tree lined routes make walking with prams and young children cooler, safer and more comfortable, encouraging physical activity during and after pregnancy. Access to green space is linked to better mental health, reducing stress, anxiety and postnatal depression risk. Tree planting near schools, children’s centres and residential estates also supports healthier early years development. (UFMP7)</p>
Race (Ethnicity)		
<input type="checkbox"/> Positive Impact	Likelihood score: N/A Impact score: N/A Overall score: N/A	<p>Nationally, ethnic minority communities are more likely to live in low canopy, high pollution and high traffic areas. (UFMP8)</p> <p>Trees and green space reduce air pollution, heat stress and cardiovascular risk, all of which disproportionately impact some minority groups. (UFMP1)</p> <p>A visible, inclusive tree strategy that designs planting with diverse communities can build trust and participation.</p>

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5.1 Appendix 2

Impact	Negative Impact Score	Explanation – what is the impact?
Religion or Belief		
<input type="checkbox"/> Positive Impact	Likelihood score: N/A Impact score: N/A Overall score: N/A	<p>Trees hold diverse and profound symbolic meanings across many world religions, often representing life, wisdom, strength, the connection between heaven and earth, and the cycle of life, death, and rebirth. (UFMP9)</p> <p>Trees around places of worship, cemeteries and community hubs can enhance cohesion, reflection and wellbeing across different faiths and beliefs. Green routes make it easier and more pleasant to walk to places of worship or community gatherings, supporting community cohesion. (UFMP1)</p>
Sex		
<input type="checkbox"/> Positive Impact	Likelihood score: N/A Impact score: N/A Overall score: N/A	<p>Both men and women benefit from increased physical activity, better air quality and reduced heat stress enabled by tree planting.</p> <p>Women and girls have been found to place value on perceived safety and attractiveness of streets and parks. Well-designed treescapes with good lighting and clear sightlines can increase their use of public space. (UFMP10)</p> <p>Men, especially in more deprived areas, have higher rates of cardiovascular mortality. improved air quality, shade and opportunities for active travel may support better heart health outcomes. (UFMP11)</p>

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Impact	Negative Impact Score	Explanation – what is the impact?
Sexual Orientation		
<input type="checkbox"/> Positive Impact	Likelihood score: N/A Impact score: N/A Overall score: N/A	<p>LGBTQ+ communities experience higher levels of depression, anxiety and minority stress. Evidence that green space improves mental health implies a disproportionately positive benefit if access is equal. (UFMP12)</p> <p>Greener, better-maintained public spaces can be designed to feel safer and more welcoming, which is particularly important where LGBTQ+ people may fear harassment in public places. Tree related volunteering and community projects offer inclusive social opportunities, helping reduce isolation among LGBTQ+ residents. (UFMP13)</p>
Care Leavers (Local)		
<input type="checkbox"/> Positive Impact	Likelihood score: N/A Impact score: N/A Overall score: N/A	<p>Care leavers have higher rates of mental health problems, loneliness and economic disadvantage. Access to safe, attractive green space and tree lined routes can support low-cost recreation, social connection and stress relief. (UFMP14)</p> <p>Urban forest projects (planting events, ward tree walks, citizen science) offer structured, supportive volunteering and skill building opportunities that can enhance CVs and confidence for young people. Tree planting in and around areas with higher concentrations of social housing or supported accommodation improves the immediate living environment for many care leavers.</p>
Armed Forces (Local)		
<input type="checkbox"/> Positive Impact	Likelihood score: N/A Impact score: N/A Overall score: N/A	<p>Veterans and serving personnel can experience PTSD, anxiety, depression and physical injury. Exposure to nature and trees is linked with reduced stress, improved mood and faster recovery. (UFMP15)</p> <p>Engagement with local armed forces community (e.g. veterans' hubs, reservist units, cadet organisations) ensures the Urban Forest Master Plan is aligned with Armed Forces Covenant commitments locally.</p>

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5.1 Appendix 2

Impact	Negative Impact Score	Explanation – what is the impact?
Poverty and Disadvantage (Local)		
<input type="checkbox"/> Positive Impact	Likelihood score: N/A Impact score: N/A Overall score: N/A	<p>Hartlepool has multiple LSOAs in the most deprived percentiles nationally. These same areas typically have lower tree canopy, higher pollution, more traffic, lower car ownership, poorer housing and worse health outcomes. Tree planting and the Urban Forest Master Plan will directly support cooler streets and homes in heatwaves, cleaner air near busy roads, better walking routes to shops, schools and services and free local recreation for families who cannot travel to distant green spaces. (UFMP16)</p> <p>The key aim of tree equity is targeted planting, investment and engagement towards Hartlepool’s most deprived neighbourhoods, rather than simply greening already leafy areas. (UFMP17)</p>

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Section 3 - Mitigation Action Plan or Justification

Group(s) impacted	Proposed mitigation	How this mitigation will make a difference	By when	Responsible Officer

Justification If you need to justify your proposed action explain this here

Section 4 - Sign Off

Responsible Officer sign off:	
Name	Helen Smith (09/12/2025)
Job title	Land Use Policy Team Leader
Assistant Director / Director sign off:	
Name	
Job title	Director for Neighbourhoods and Regulatory Services

Once the Single Impact Assessment is completed please send to impactassessments@hartlepool.gov.uk.

Section 5 - Review (To be completed after implementation)

5.1 Review completed by:		
Name	Job Title	Date review completed

5.2 Did the impact turned out as expected?

5.3 Were the proposed mitigations the correct ones and were they successful in reducing any negative impacts?

5.4 Were there any unexpected outcomes?

5.5 Following the review please identify next steps here (Select one)

- Additional mitigation required (give details below - 5.6)
- Original proposed course of action needs to be revisited
- No further action required

5.6 Additional mitigation(s) or justification

Group(s) impacted	Proposed mitigation	How this mitigation will make a difference	By when	Responsible Officer

Justification If you need to justify your proposed action explain this here

Submit form with completed review to impactassessments@hartlepool.gov.uk



NEIGHBOURHOODS AND REGULATORY SERVICES COMMITTEE

2 March 2026

Report of: Director for Neighbourhood and Regulatory Services

Subject: LOCAL TRANSPORT PLAN PROGRAMME

Decision Type: Key (NRS107/26)

1. Council Plan Priority

Hartlepool will be a place:
- where people live healthier, safe and independent lives. (People)
- that is connected, sustainable, clean and green. (Place)

2. Purpose of Report

2.1 To inform the Neighbourhoods and Regulatory Services Committee of the current position with the Local Transport Plan Delivery Plan, progress made under the Local Transport Plan delivery programme during 2025/2026, and to set the budget allocations for 2026/2027.

3. Background

3.1 The third Local Transport Plan (LTP) 2011-2026, sets out how a safe and sustainable transport system can be delivered within Hartlepool. It was recognised that funding, particularly in the short to medium term, was significantly reduced from the second LTP period. Whilst this represented a significant challenge to delivering the Council's aspirations for improving the transport network in Hartlepool, it provided opportunities to work more closely with our partner organisations for the overall benefit of the Borough.

- 3.2 The LTP is also aligned with key regional strategies and local development frameworks. The Tees Valley Combined Authority’s Strategic Transport Plan covers the period up to 2030, and has the vision *“To provide a high quality, quick, affordable, reliable, low carbon and safe transport network for people and freight to move within, to and from the Tees Valley.”*
- 3.3 In recent years the TVCA has become a conduit between local authorities and central government on policy implementation, bid development, and funding allocations, with LTP budgets now being provided to the combined authority for onward distribution to individual Councils.
- 3.4 As outlined at 3.1 the current plan is coming to an end, and a budget allocation is included in this year’s programme to develop an updated document in line with national/ regional legislation and ambitions.

4. Proposals

- 4.1 The works identified for delivery during 2025/2026 are shown at **Appendix 1**, along with proposed schemes for 2026/2027 (subject to further committee approvals where necessary).
- 4.2 In 2025/26 the level of demand for safety schemes outweighed the Local Transport Plan budget and therefore a prioritisation matrix is used which was approved by Neighbourhood Services Committee (18th October 2019). This allows schemes to be prioritised on the basis of requirement, with a further report being brought before Committee for scheme approvals.
- 4.3 In addition, Members of Neighbourhoods and Regulatory Services Committee had previously requested a review of the allocations to try and unlock further safety scheme funding. This review has relocated £25,000 into safety schemes for 2026/2027 (subject to member approval).

5. Other Considerations/Implications

RISK IMPLICATIONS	No relevant issues
FINANCIAL CONSIDERATIONS	All works/ schemes will be funded via the LTP allocation from Central Government, with the Integrated Transport Block allocation confirmed as £724,625. Individual scheme type allocations for 2025/26, along with the allocations for 2026/27 are shown in Appendix 2 , with this being the last year of a multi-year settlement up to and including 2026/27.

	A further multi-year settlement is anticipated to be confirmed during the coming months.
SUBSIDY CONTROL	No relevant issues
LEGAL CONSIDERATIONS	No relevant issues
SINGLE IMPACT ASSESSMENT	No relevant issues
STAFF CONSIDERATIONS	The delivery of the LTP allocation will be overseen by the Councils Highway Infrastructure Manager.
ASSET MANAGEMENT CONSIDERATIONS	Additional assets will be added to the Council's asset register as appropriate.
ENVIRONMENT, SUSTAINABILITY AND CLIMATE CHANGE CONSIDERATIONS	Allocations within the LTP are designed to improve sustainability, increase greener transport uptake and contribute to the Council's net zero aspirations.
CONSULTATION	Consultation on individual schemes will be undertaken with Members and the public/ residents/ businesses as appropriate.

6. Recommendations

6.1 That Neighbourhoods and Regulatory Services Committee notes the works/ schemes delivered during 2025/2026, and approves the budget allocations for 2026/2027.

7. Reasons for Recommendations

7.1 To facilitate the continuation of the Local Transport Plan delivery programme.

8. Background Papers

8.1 LTP Strategic Delivery Plan – Neighbourhood Services Committee – 16th March 2015.

8.2 Local Transport Plan Scheme Update – Neighbourhood & Regulatory Services Committee – 10th November 2025.

9. Contact Officers

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Peter Frost
Highway Infrastructure Manager
Tel: (01429) 523200
E-mail: peter.frost@hartlepool.gov.uk

Sign Off:-

Chief Executive	Date: 06/02/2026
Director of Finance, IT and Digital	Date: 05/02/2026
Director of Legal, Governance and HR	Date: 05/02/2026

Scheme Type	2025-2026	2026-2027
Bus Infrastructure (£5,000)	<p>New shelter at Raby Road bus stop (opp. Grainger St).</p> <p>Repairs to HBC owned bus shelters:- Maintenance of HBC owned bus shelters / poles, as required. Low floor infrastructure bus stop upgrades, as required.</p> <p>Miscellaneous:- Carriageway lining works – various locations. New / replacement timetable installation works at various locations. Installation of missing, corroded and damaged bus stop poles. Printing of new / updated bus timetables (various).</p>	<p>Shelter upgrades along bus improvement corridor identified in work with TVCA as part of Bus Service Improvement Partnership. New shelter requests, subject to public consultation where required.</p> <p>Repairs to HBC owned bus shelters:- Maintenance of HBC owned bus shelters / poles, as required. Low floor infrastructure bus stop upgrades, as required.</p> <p>Miscellaneous:- Carriageway lining works – various locations. New / replacement timetable installation works at various locations. Installation of missing, corroded and damaged bus stop poles. Printing of new / updated bus timetables (various).</p>
Cycle tracks / lanes (£50,000)	<p>Design works on LCWIP and LUF schemes, specifically:- Town Centre to Waterfront/ promenade link Town Centre to Summerhill via Burn Valley Town Centre to Headland Town Centre to Wolviston Raby Road King Oswy Drive improvements phase 1</p>	<p>Contributions to the following LCWIP and LUF schemes as required:- Town Centre to Waterfront/ promenade link <i>*Est. start on site April '26</i> Town Centre to Summerhill via Burn Valley <i>*Est. start on site April '26</i> Town Centre to Headland <i>*Est. start on site Summer '26</i> Town Centre to Wolviston King Oswy Drive improvements phase 1 (Construction)</p>

Scheme Type	2025-2026	2026-2027
New cycle parking facilities	Cycle parking at new Highlight leisure centre.	N/a
Signage Improvements (£34,000)	Seaton Front weight restriction signage. Various signage/ road markings as required.	Additional signage to be identified.
Sustainable Travel Improvements (£30,000)	General footpath/ crossing improvements. Minor improvement works to School Crossing Patrol sites/ school gate parking issues.	Support to LCWIP/ LUF programme as required. General footpath/ crossing improvements. Minor improvement works to School Crossing Patrol sites/ school gate parking issues.
Safer Routes To School (£40,000)	West View Road raised junction. Small scale safety schemes (guard rails, footpath works, etc).	Additional schemes to be identified/ confirmed in a further local safety scheme report to Committee.
Local Safety Schemes (£125,000)	Elwick Road safety scheme. Park Road safety scheme. A689 (Esso petrol station) safety scheme. A179/ Worset Lane safety scheme. Traffic regulation orders (various).	Additional schemes to be identified/ confirmed in a further local safety scheme report to Committee. Traffic regulation orders (various).
Road Crossings (dropped kerb schemes) (£10,000)	Throston Grange Lane, Fieldfare Rd, Swalebrooke Ave, Stratford Rd	Additional schemes to be identified.
School Safety Schemes (£25,000)	Murray Street zebra crossing	West Park School traffic calming (subject to consultation)
Verge Hardening programme / car parking bays (£50,000)	Selby Grove, Sandbanks Drive, Fulthorpe Ave, Torbay Grove, Wiltshire Way	Schemes to be identified / confirmed.

Scheme Type	2025-2026	2026-2027
Traffic Signal Improvements (£110,000)	Contribution to upgrades at Marina Way, A689/ Queen's Meadow and Catcote Rd/ Truro Drive	Upgrades at Hart Lane/ Serpentine Rd and A689/ Stockton Rd A179 (Merlin Way) toucan crossing upgrade Contribution to new toucan crossing at Brenda Rd (Golden Flatts)
Car Parking Improvements (£20,000)	Permit zone lining/ signing works. West Side Car Park lining works.	Car park surfacing works to be identified. Permit zone lining/ signing works. Contribution to replacement camera enforcement car. Potential Archer Street car park contribution.
Highway Improvement Schemes (£100,000)	Contribution to potential A19/ A179 improvement scheme set aside. Cats eyes/ road markings upgrades. Highlight leisure centre access road contribution. Small scale neighbourhood schemes (bollards, guard rails, signage, footpaths, etc).	Match funding contributions to various DfT/ TVCA funding bids as required. Cats eyes/ road markings upgrades as required. Contribution to A179/ Palace Row junction improvement scheme. Small scale neighbourhood schemes (bollards, guard rails, signage, footpaths, etc).
Access To Employment (£5,000)	Localised highway works on approach to shopping centre.	Works to be identified.
Street Lighting (£110,000)	Promenade ornamental column replacement. Localised column replacement/ upgrades.	Column replacement/ lighting upgrade schemes along key route network (A179 Hart Bypass). Localised column replacement/ upgrades where required.

Allocation of Integrated Transport Block Capital Funding (2025/26 & Proposed 2026/27)

Theme	Scheme Type	2025-26	2026-27
		£000's	£000's
Bus Infrastructure (BI)	Bus Stop Improvements	10	5
Cycling Schemes (CY)	Cycle tracks/lanes	50	50
	New cycle parking facilities	5	0
Sustainable Travel (ST)	Sustainable Travel Improvements	40	30
Local Safety Schemes (LS)	Safer routes to school	40	40
	Local Safety Schemes	100	125
	Signage Improvements	34	34
Road Crossings (RC)	Dropped Kerbs	15	10
Traffic Management & Traffic Calming (TM)	Traffic Signal Improvements	100	110
	School Safety Schemes	25	25
	Highway Improvement Schemes	120	100
Local Road Schemes (RD)	Verge Hardening Programme	50	50
	Access to Employment	5	5
Miscellaneous (OS)	Car park Improvements	30	20
	LTP 4 Development		10
	Street Lighting Improvements	100	110
Total		724	724



Neighbourhoods and Regulatory Services Committee

2 March 2026

Report of: Director for Neighbourhood and Regulatory Services

Subject: RESIDENTIAL DESIGN CODE SUPPLEMENTARY PLANNING DOCUMENT (SPD)

Decision Type: Key (DNRS 114 / 26)

1. Council Plan Priority

Hartlepool will be a place:
where people live healthier, safe and independent lives. (People)
that is connected, sustainable, clean and green. (Place)

2. Purpose of Report

2.1. The purpose of this report is to seek Members agreement to consult upon the draft Residential Design Code Supplementary Planning Document (the **SPD**), modify as required following the consultation and then present to Full Council for adoption (**Appendix 1**).

3. Background

3.1. In 2024 the council secured £120,000 in funding from the Ministry Housing Communities and Local Government (MHCLG) as part of the Design Code Pathfinder Fund to produce a local design code.

4. Proposals

4.1. The council has developed and are proposing to consult upon the SPD, that will, once adopted, be a material consideration in planning decisions. The National Model Design Code, which forms part of the government's planning practice guidance, defines a design code as a set of simple and concise design requirements that provide specific, detailed parameters for the development of a site or area. The SPD provides design parameters (codes) and guidelines (guidance) that when followed will ensure that new residential development is high quality, positively enhances its location and meets the aspirations of the council and residents.

4.2. The SPD will serve as a tool to aid in the delivery of new high quality residential areas within the borough in accordance with the Hartlepool Local Plan (2018). The codes and guidance within the SPD are intended to inform every stage of the development process, from site analysis to detailed design, and when followed will deliver well designed neighbourhoods that enhance the quality of life for residents while respecting the unique characteristics of the borough.

4.3. The benefits of the SPD are as follows:

- Provide clear and transparent design parameters and guidance for new residential development.
- Provide certainty on the council's aspirations for residential development.
- Speed up the planning application process for proposals which meet the design codes and guidance.
- Bespoke to Hartlepool and it sets different parameters to meet the different characteristics of the borough.

4.4. The SPD must be referenced in all new residential applications made to the council. It will be a material planning consideration that is consistent with existing national and local planning policy. This SPD builds upon and provides more detailed advice or guidance to policies in the local plan, the NPPF and the National Design Guide.

4.5. Work on the SPD started in April 2024 and continued throughout 2025. The process of producing the draft SPD was threefold

4.6. Stage 1: Analysis

This covered a review of existing information and data including extensive research, attending national meetings, seeking advice, borough wide site visits, engagement with key stakeholders etc. The geographical area and development types to be covered by the code were assessed. A baseline was then established as well as an internal working group.

4.7. Stage 2: Draft Vision.

Analysis 1.1.of the borough to establish area types with similar characteristics was undertaken followed by the production of a Coding Plan. Meetings with Parish Councils were carried out and a design champion secured. The first round of public consultation was carried out online and at various public locations in the borough covering Hartlepool Cricket Club, Wynyard Golf Club, Seaton Library, Headland Library, Greatham Village Hall and the Central Hub. Consultation took place for a period of 8 weeks from 16th September to 16th November 2024 following which a draft vision and aims were established.

4.8. Stage 3: Coding

This stage involved preparing the initial Code and Guidance. Internal consultation was carried out and feedback used to update the draft codes and guidance, this process helped shape the draft SPD.

4.9. Following the formal consultation, all responses will be analysed and the draft SPD will be updated accordingly. Any modifications will be brought to the Director of Neighbourhood and Regulatory Services and considered in consultation with the Committee Chair. The updated SPD will be taken to full council for adoption in May/June 2026.

5. Other Considerations/Implications

Risk Implications	If members do not support this consultation process then the SPD will not be adopted in June 2026.
Financial Considerations	There are no financial implications associated with the consultation of the SPD. Significant consultation will be undertaken by free online platforms and any events attended will be funded by the MHCLG funding obtained and not HBC.
Subsidy Control	None
Legal Considerations	The work relating to the SPD has been and will continue to be in accordance with the HBC and MHCLG Memorandum of Understanding signed on

	<p>21st March 2024 and The Town and Country Planning (Local Planning) (England) Regulations 2012.</p> <p>The Leveling up and Regeneration Act (2023) allows for the provision of regulations that will alter the way that Local Development Framework Documents (LDF) are produced. On 27th November 2025 the Government issued guidance on what to expect within the regulations when they are brought into force in spring 2026. Of note is that the Government intends to set out that SPDs cannot be adopted after the 30th June 2026. If this deadline is not achieved then it could be argued that the SPDs do not hold weight and cannot be relied upon in decision making.</p>
Single Impact Assessment	<p>During the preparation SPDs including the consultation undertaken to date, officers had regard to the protected characteristics within the Equality Act (2010) and the additional protected characteristics that HBC deem appropriate (care leavers, armed forces and poverty and disadvantage). Officers are of the view that the draft SPD will not have a negative impact upon protected characteristics but could in fact have a positive impact upon some protected characteristics by virtue of creating a better environment for all, including those with protected characteristics. The SPD 8 week consultation will be fair and open to all and officers are of the view that no one with protected characteristics will be disadvantaged.</p>
Staff Considerations	<p>There are no staff considerations associated with the consultation of the SPD.</p>
Asset Management Considerations	<p>None</p>
Environment, Sustainability and Climate Change Considerations	<p>None</p>
Consultation	<p>Consultation has already taken place as outlined above. A further formal consultation process is planned 3rd March to 28th April 2026.</p>

6. Recommendations

- 6.1. That Members note the content of this report and the draft SPD and agree to the 8-week formal SPD consultation.
- 6.2. Members agree to delegate to the Director of Neighbourhood and Regulatory Services, to consider representations and make modifications to the SPD, in consultation with the Committee Chair.
- 6.3. That Members agree to permit the modified SPD to be presented to full council for adoption In May/ June 2026.

7. Reasons for Recommendations

- 7.1. To ensure members are kept informed of SPD progress, to ensure that the correct permission is in place prior to consultation and to ensure that the SPD can be adopted in June 2026.

8. Background Papers

- 8.1. There are no papers in relation to this report.

9. Contact Officers

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Sign Off:-

Chief Executive	Date: 06/02/2026
Director of Finance, IT and Digital	Date: 06/02/2026
Director of Legal, Governance and HR	Date: 06/02/2026



Hartlepool
Borough Council



Hartlepool Borough Council Residential Design Code & Guidance

Supplementary Planning Document Draft
February 2026

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INTRODUCTION

Overview

The Residential Design Code and Guidance ('This code') sets out a vision for the design of new residential development within Hartlepool.

This code provides design parameters (codes) and guidelines (guidance) that when followed will ensure that new residential development is high quality, positively enhances its location and meets the aspirations of the council and residents.

This code will serve as a tool to aid in the delivery of new high quality residential areas within the borough in accordance with the adopted Local Plan (2018). The codes and guidance within this document are intended to inform every stage of the development process, from site analysis to detailed design, and when followed will deliver well-designed neighbourhoods.

Benefits of this code:

- Provide clear and transparent design parameters and guidance for new residential development.
- Provide certainty on the council aspirations for residential development.
- Speed up the planning application process for proposals which meet the design codes and guidance.
- Bespoke to Hartlepool and sets different parameters to meet the different characteristics of the borough.
- Forms a material consideration in the determination of planning applications.

What is a Design Code?

The National Model Design Code defines a design code as a set of simple and concise design requirements that provide specific, detailed parameters for the development of a site or area.

Who is this code for?

This code is intended to be used by a wide audience.

The key users of this code will be:

- Council officers: to assess residential planning application proposals and inform pre-application discussions. This code will also

enable the provision of consistent advice.

- Applicants: To give applicants for new residential development including designers, architects, developers and members of the public, clear parameters and guidance on what is expected in the design of proposals.
- Planning Committee: to guide Councillors during their decision making at planning committee and ensure consistency in decision making.

Purpose of this design code and guidance

This Design Code will serve as a valuable tool, providing a set of specific and measurable parameters for new residential developments within Hartlepool.

By following the design codes and guidance within this document, new residential proposals will create well-designed neighbourhoods that enhance the quality of life for residents while respecting the unique characteristics of the borough and will create residential areas, which meet the expectations of the council and its residents.

This code and planning applications

This code will be a Supplementary Planning Document (SPD) and as such should be referenced in applications made to the council. It will be a material planning consideration that is consistent with existing national and local planning policy.

This Design Code SPD builds upon and provides more detailed advice or guidance to policies in the adopted Local Plan, the NPPF and the National Design Guide.

As this SPD does not form part of the Development Plan, it cannot introduce new planning policies, therefore any reference to requirements including 'will' or 'must' in the Design Code are to be treated as guidance providing a strong steer to stakeholders setting out what the council expects from new development in the borough.

PLANNING POLICY

National Policy and guidance

The National Planning Policy Framework (NPPF)

The NPPF sets out the Government's planning policies for England and how these are expected to be applied. The overriding message from the NPPF is that planning authorities should plan positively for new development, and approve all proposals wherever possible. It defines the role of planning in achieving sustainable development under three topic headings – economic, social and environmental, each mutually dependent.

There is a presumption in favour of sustainable development, which requires Local Planning Authorities to approach development management decisions positively, utilising twelve 'core principles' that should underpin both plan-making and decision taking, these being; empowering local people to shape their surroundings, pro-actively drive and support economic development, ensure a high standard of design, respect existing roles and character, support a low carbon future, conserve the natural environment, encourage re-use of previously developed land, promote mixed use developments, conserve heritage assets, manage future patterns of growth and take account of and support local strategies relating to health, social and cultural well-being.

The creation of high quality, beautiful and sustainable buildings and places is fundamental to what the planning and development process should achieve.

The NPPF sets out that to provide maximum clarity about design expectations at an early stage, all local planning authorities should prepare design guides or codes consistent with the principles set out in the National Design Guide and National Model Design Code, and which reflect local character and design preferences. Design guides and codes provide a local framework for creating beautiful and distinctive places with a consistent and high quality standard of design.

Their geographic coverage, level of detail and degree of prescription should be tailored to the circumstances and scale of change in each place, and should allow a suitable degree of variety.

The NPPF further states that design guides and codes can be prepared at an area-wide, neighbourhood or site-specific scale, and to carry weight in decision-making should be produced either as part of a plan or as supplementary planning documents. Whoever prepares them, all guides and codes should be based on effective community engagement and reflect local aspirations for the development of their area, taking into account the guidance contained in the National Design Guide and the National Model Design Code.

PLANNING POLICY

This National Design Guide, and the National Model Design Code and Guidance Notes for Design Codes illustrate how well-designed places that are beautiful, healthy, greener, enduring and successful can be achieved in practice. It forms part of the Government’s collection of planning practice guidance and should be read alongside the separate planning practice guidance on design process and tools.

The National Design Guide (NDG) 2021

The National Design Guide sets out the characteristics of well-designed places and demonstrates what good design means in practice. It forms part of the government’s collection of planning practice guidance.

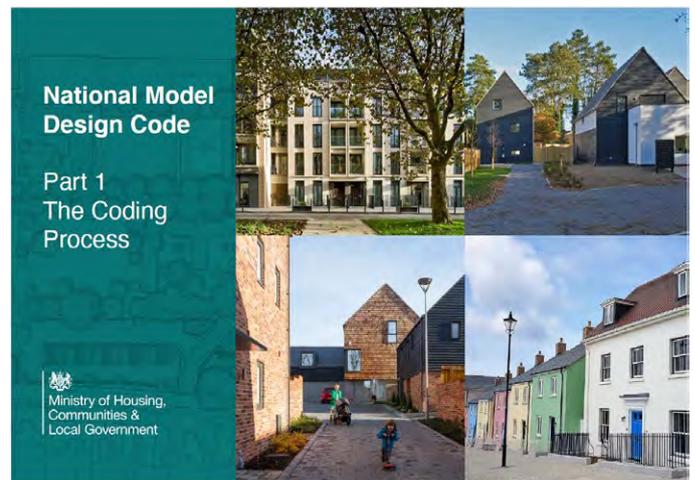
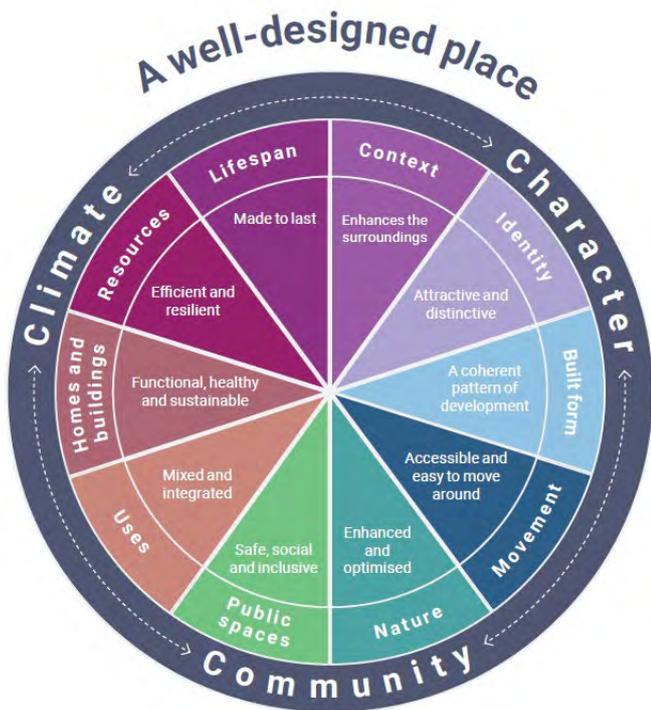
The National Design Guide addresses the question of how we recognise well-designed places, by outlining and illustrating the Government’s priorities for well-designed places in the form of ten characteristics. The NDG sets out that in well-designed places, an integrated design process brings the ten characteristics together in a mutually supporting way.

The National Model Design Code (NMDC) 2021

Provides detailed guidance and a framework for local authorities to create their own specific design codes, aiming to ensure high-quality, beautiful, and sustainable new developments that reflect local character, expanding on the 10 characteristics of good design in the National Design Guide.

The NMDC provides tools, examples (like area types, parking, built form), and methods for community engagement to help create clear, illustrated requirements for developments.

The NMDC sets a baseline standard of quality and practice, which local planning authorities are expected to take into account when developing local design codes and guides.



PLANNING POLICY

Local Policy

The Hartlepool Local Development Framework consists of the Local Plan (2018), Minerals and Waste DPD's, Rural Neighbourhood Plan (2018) and a suite of Supplementary Planning Documents (SPD's).

This Design Code and Guidance will be adopted as a Supplementary Planning Document to support policies within the Hartlepool Local Plan (2018) and replace the existing Residential Design Guide SPD (2019).

Hartlepool Local Plan (2018)

This code relates to several policies within the Local Plan, in particular Policy QP3 and Policy QP4:

Policy QP4: Layout and Design of Development.

The policy seeks to ensure all developments are designed to a high quality and positively enhance their local setting.

Development should:

1. Be of an appropriate layout, scale and form that positively contributes to the borough and reflects and enhances the distinctive features, character and history of the local area.
2. Respect the surrounding buildings, structures and environment.
3. Have adequate, well located and planned public space/s.
4. Be aesthetically pleasing, using a variety of design elements relevant to the location and type of development.
5. Use an appropriate mix of materials and colour.
6. Sustain and/or enhance the historic environment and heritage assets including archaeological remains, and their settings.
7. Have appropriately designed and well located street furniture.
8. Be flexible to changing needs of users and consider the needs of an ageing population.
9. Should not negatively impact upon the relationship with existing and proposed neighbouring land uses and the amenity of occupiers of adjoining or nearby properties by way of general disturbance, overlooking

and loss of privacy, overshadowing and visual intrusion particularly relating to poor outlook.

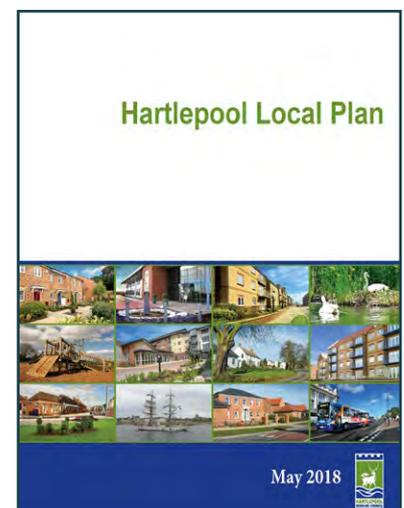
10. Ensure that the provision of private amenity space should be commensurate to the size of the development.
11. Use well-designed and placed obstacles, such as rocks or vegetation, to prevent vehicles parking on areas not designated for vehicles such as green spaces and pavements.

Policy QP3: Location, accessibility, highway safety and parking.

The council will seek to ensure that development is safe and accessible along with being in a sustainable location or has the potential to be well connected with opportunities for sustainable travel.

When considering the design of development developers will be expected to have regard to the following matters:

1. The proximity of the development to nearby shops and services and how shops and services can be accessed along with how access can be improved particularly via the green infrastructure network.
2. Ensuring all residents and visitors can move with ease and safety and that features such as doors, entrance ways and parking are conveniently located.
3. Servicing arrangements and highway safety provisions are in line with the relevant local guidance and the requirements to maintain



PLANNING POLICY

such provisions over the lifetime of the development.

4. Creating multi-purpose environments, managing the conflict between highways users specifically by distinguishing between paths and roads, whilst recognising the need to limit and/or create sympathetic highway safety measures.
5. The provision of adequate, safe, secure and conveniently located car and cycle parking, having regard to the possible movement of residents and visitors.
6. Ensure new car parking areas and proposals that include car parking should, where viable, use permeable paving solutions and/or introduce a SuDS scheme to mitigate for any increase in surface water run-off.
7. Recognise the wider benefits that safety improvements at level crossings can bring about.

Hartlepool Rural Neighbourhood Plan (2018)

The Hartlepool Rural Neighbourhood Plan was made in 2018, the plan covers the rural parishes within Hartlepool that lie to the west of and adjoining the main built up area of Hartlepool, these include:

- Brierton,
- Claxton,
- Dalton Piercy,
- Elwick,
- Greatham,
- Hart, and
- Newton Bewley.

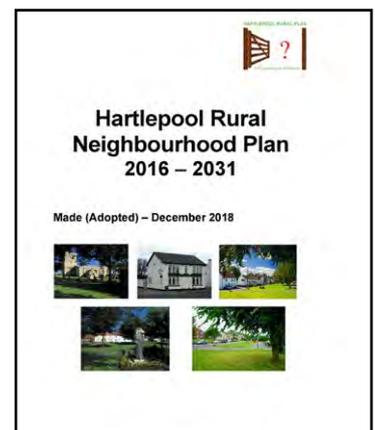
The Neighbourhood Plan sets out that all development should be well designed and take account of the design principles set out in Policy GEN 2.

Policy GEN 2: Design Principles.

The design of new development should demonstrate, where appropriate:

1. How relevant village design statements and conservation area appraisals have been taken into account;
2. How the design of new housing scores against the Hartlepool Rural Plan Working Group's Checklist as set out in appendix 4;
3. How the design helps to create a sense of place and reinforces the character of the village or rural area by being individual, respecting the local vernacular building character, safeguarding and enhancing the heritage assets of the area, landscape and biodiversity features;
4. How the design helps to reinforce the existing streetscape or green public spaces by facing onto them;
5. How the design preserves and enhances significant views and vistas;
6. How the design demonstrates that it can be accessed safely from the highway and incorporates sufficient parking spaces;
7. How the design uses sustainable surface water management solutions in new developments to reduce all water disposal in public sewers and manage the release of surface water into fluvial water and;
8. How the design ensures that homes are flexible to meet the changing needs of future generations.

In addition to those policies listed above a full list of applicable policies within the Local Plan and Rural Neighbourhood Plan are provided in Appendix 1.



PUBLIC CONSULTATION

The National Design Guide and National Model Design Code set out that a Design Code will be 'based upon effective community engagement and reflect local aspirations.' The council's Land Use Policy Team undertook an eight-week public consultation between September and November 2024 to gain the views of residents on what they valued about where they live and how future residential developments should be designed.

The consultation consisted of a series of public drop in events throughout the borough, engagement with a local youth group and an on-line survey and mapping tool.

The findings of the consultation event are summarised below and have informed the vision, aims, design codes and guidance within this document.

Character of Place

Residents valued distinctive architecture, detailing, and the overall identity of their neighbourhoods. Features such as bay windows, decorative trims, and painted accents were mentioned as contributing to a sense of place and making areas easier to navigate and remember. Homes that looked different from each other were preferred over uniformity.

Younger participants echoed this, highlighting how streets with colourful features (like painted doors or lintels) felt more inviting and easier to orient around. Even small visual differences between homes were appreciated.

There was also concern about new housing areas lacking this identity, often described as repetitive or "engineered," with little sense of individuality.

Homes and Buildings

There was a strong desire for homes that feel spacious and functional. While a preference for traditional house types was observed, however, this should not be at the cost of visual variety, with varied building form being viewed as the most attractive feature for new buildings.

For homes specifically, residents appreciated large rooms, high ceilings, practical layouts (especially rectangular room shapes), and usable landings. These features allow greater flexibility and comfort for modern living.

Participants emphasised that homes should be built with attention to both internal and external character—such as alcoves, fireplaces, and traditional detailing. There was also a call for homes to offer ample built-in storage, usable third bedrooms, and multiple bathrooms to suit family needs.

A clear dislike emerged for houses perceived as bland or cheaply finished, particularly in newer developments where materials like render were said to age poorly. The concept of "a home, not just a house" resonated throughout responses, emphasising warmth, light, and character over uniformity.



September 11, 2024

What do you value about where you live - Hartlepool Design Code

Planning Policy are collecting the views and ideas of Hartlepool residents on the what people ...

[Visit hub](#)

On-line survey

PUBLIC CONSULTATION

Layout and Density

There was a preference for homes not to be located too closely together. Layouts should allow natural light into both houses and gardens, with fences and building orientation designed accordingly.

Residents emphasised the importance of privacy, quiet, and internal space in new developments. There was support for designs that allow for community interaction without sacrificing personal space, such as homes that don't open directly onto the street but have a small set-back.

Participants also wanted a variety of home sizes and styles to avoid visual monotony and to suit a range of life stages and needs.

Streets and Movement

Navigability, walkability, and green street design were key themes. Navigable street layouts were identified as being important, with cul-de-sacs noted as being disorienting. However, narrow alleyways or cut-throughs were not seen as a good alternative due to concerns over safety and appearance.

Pavement obstructions caused by parked cars and overgrown shrubs were frequently cited as making walking difficult. In contrast, greenery, especially street trees, were positively viewed when well-maintained and placed to avoid blocking street lighting.

Participants wanted streets to feel open and safe, ideally with front gardens, green verges, low boundaries, and good lighting. Passive surveillance was viewed as important for creating a sense of security.

Parking

Residents generally preferred off-street parking, particularly where vehicles can be seen from the home. On-street parking was considered visually disruptive and impractical, particularly when it impeded pedestrian movement.

There was some support for car-free or low-car streets, provided parking was still conveniently located nearby. People also valued streets that could be adapted for events, such as temporary closures for neighbourhood gatherings.

The importance of practical street design was raised, including space for bins, buses, and delivery vehicles. Parking at the back of homes was unpopular due to visibility and safety concerns.



Drop in events

PUBLIC CONSULTATION

Open Space and Nature

Green infrastructure was consistently and widely valued by a majority of respondents. Access to open spaces, street trees, and community gardens were consistently referenced as desirable features for communities. Some respondents expressed a desire for more ambitious green initiatives such as fruit trees, orchards, and space for residents to grow food.

Privacy was also cited as being important while advocating for open and welcoming frontages. There was a strong belief that personal and shared green space contributes positively to well-being and mental health.

Open green links through estates and local parks were also seen as essential for encouraging walking, relaxing, and building community.

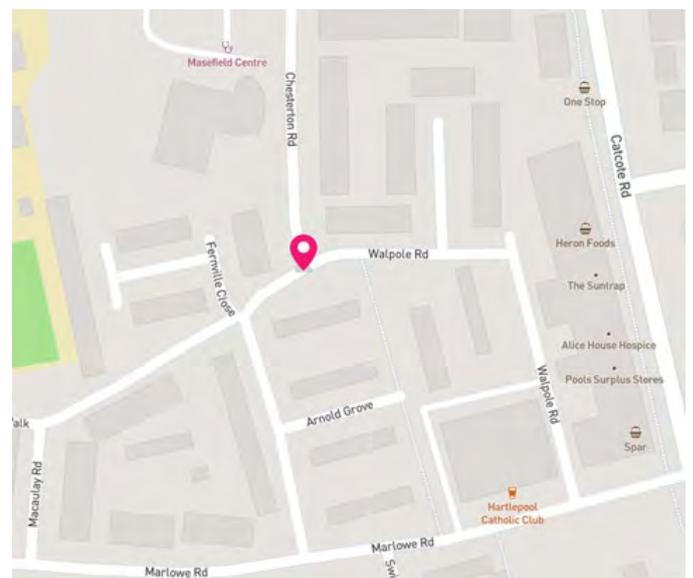
Community and Facilities

People want neighbourhoods that support everyday life. This included consistently available nearby shops, GPs, dentists, schools, and sustainable transport links. Several concerns were raised about the over-development of peripheral areas and a corresponding decline in the town centre.

Respondents emphasised the importance of infrastructure matching the rate of housing growth, as well as including gathering spaces into developments to support community interaction. Older areas were mentioned as having potential for renewal, with reinvestment opportunities being mentioned.



Community drop-in event



Example response to mapping tool

VISION

Following the findings of the public consultation and consultation with key internal stakeholders the following vision and aims have been produced for the Design Code and Guidance.

Vision:

To create attractive, distinctive and sustainable places to live within Hartlepool. New housing areas will provide well connected and healthy neighbourhoods with streets and green spaces that are social spaces and allow communities to thrive.

Aims:

1. Improve the quality of residential streets through encouraging them as social spaces and reducing the dominance of vehicles on the street scene.
2. Ensure new residential developments are successfully integrated into existing communities and encourage active travel through well designed movement networks.
3. Greening of residential areas through increasing the levels of landscaping, tree planting and green spaces.
4. Ensure new housing has its own identity and reflects any identified positive features of the area.
5. Promoting the use of renewable energy sources and energy efficiency in new homes.





SOLD

TO LET

STRUCTURE OF THIS CODE

This Code is broken down into two sections, Area Wide and Area Types.

Section 1: Area wide

The first section provides Area Wide codes and guidance which are applicable to all new residential developments throughout the borough.

Section 2: Area Types

The second section forms design codes and guidance based on Area Types. The Area Types are areas within the Borough which will see key residential growth. The three Area Types identified are:

1. Outer Neighbourhood.
2. Wynyard.
3. Villages (Core and Edge).

A character analysis of the borough has been undertaken to define the boundaries of the Area Types and identify key characteristics. This analysis along with the findings of the public consultation and assessment of planning applications has informed the Area Type codes and guidance.

Coding Plan

A Coding Plan has been produced (see opposite), which identifies the three Area Types.

The Coding Plan defines the extent of where the three Area Types codes must be applied to. It does not allocated areas for residential development. Where a site is located outside of a Area Type see Code AT1.

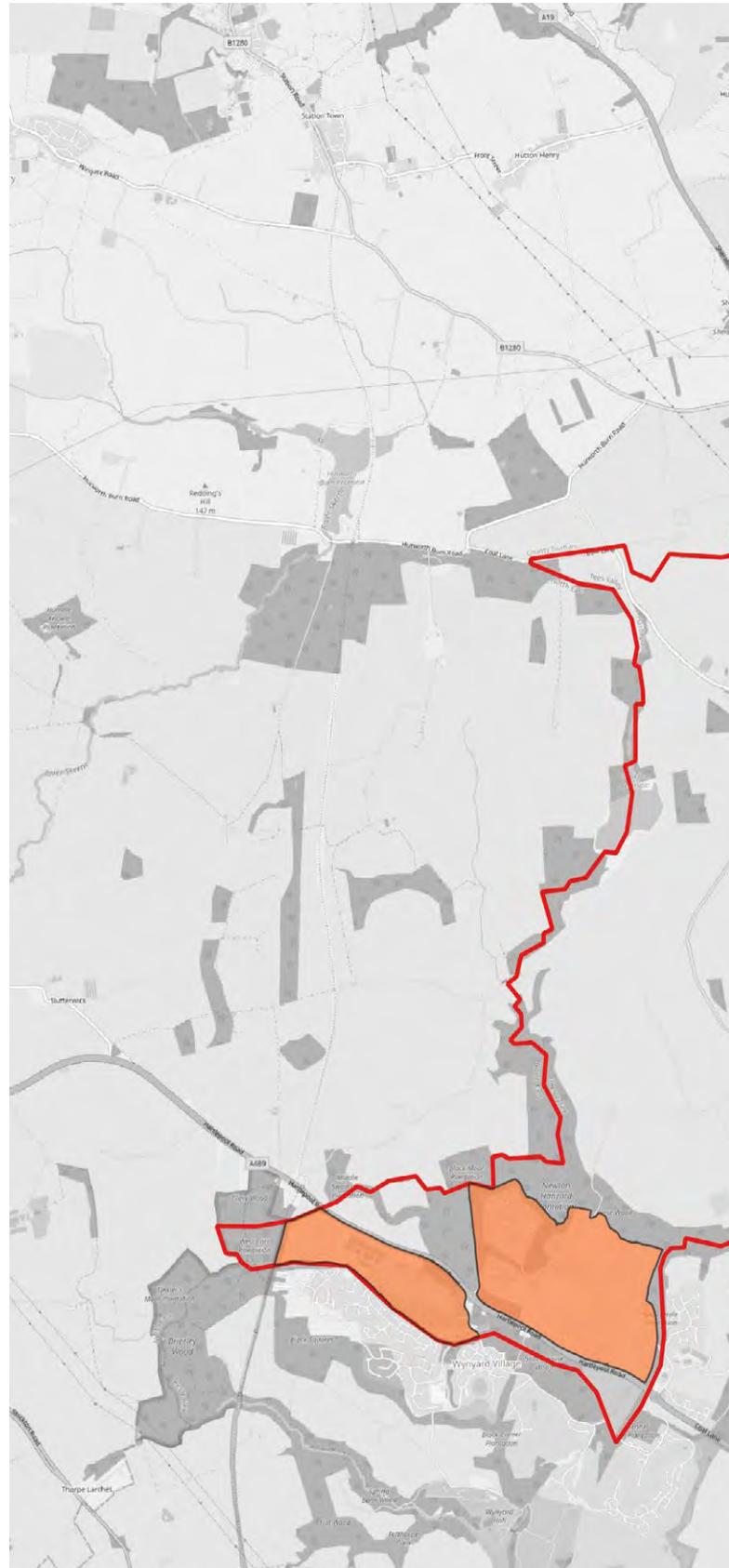
Should applicants be unsure which codes apply to a particular development, it is recommended that the council planning officers be contacted for pre-application advice.

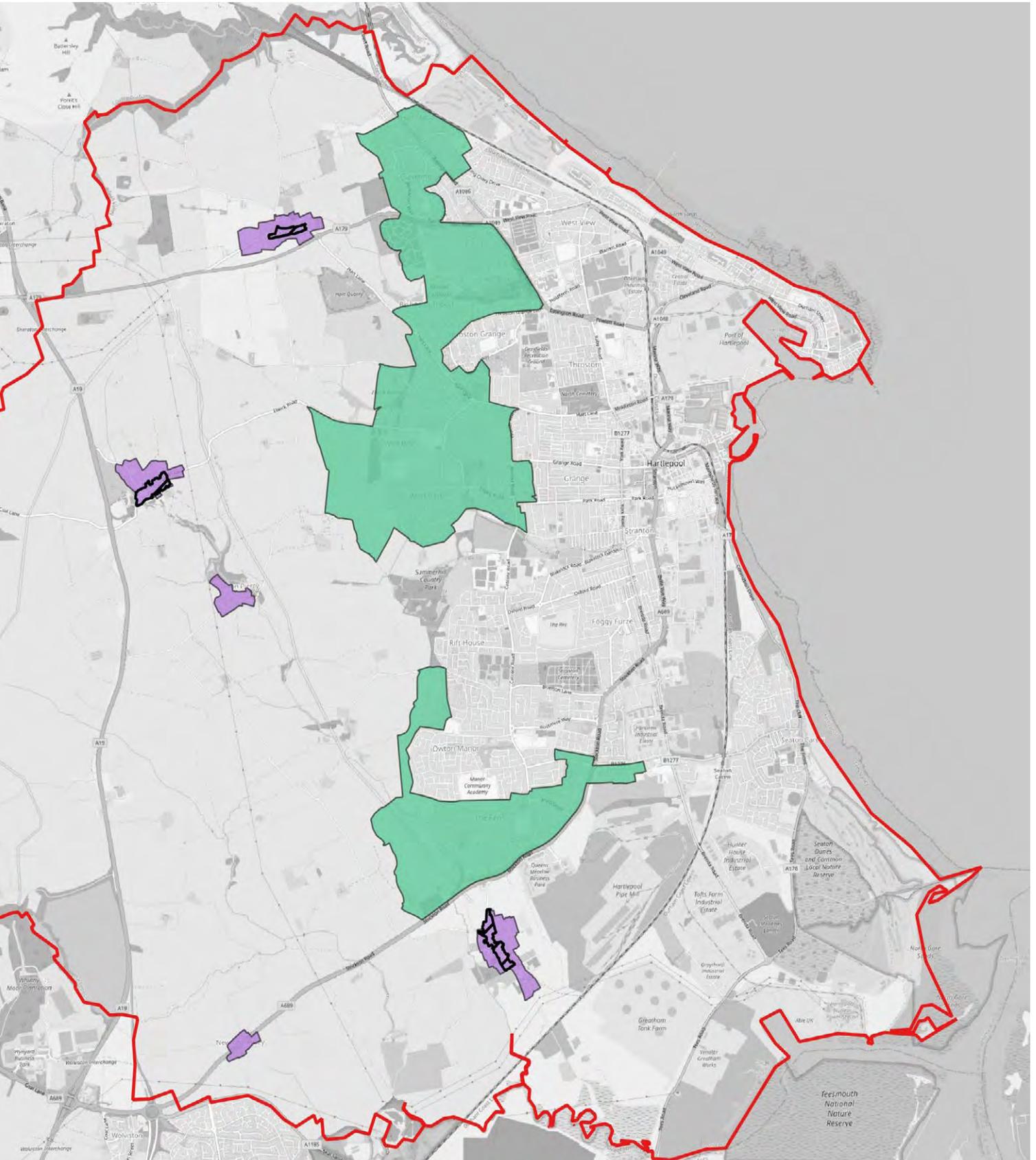
 Outer Neighbourhood Area Type (ON)

 Wynyard Area Type (WY)

 Village Area Type (VL)

 Village Core





STRUCTURE OF THIS CODE

How to use this design code

The document provides a combination of design parameters (codes) and guidance.

Codes are requirements for all new residential development, which must be followed.

Guidance is a specific set of guidelines, which may be applicable to residential development proposals and should be followed.

Justification with evidence will be required for proposals, which cannot meet or deviate from the codes and guidance.

To aid in distinguishing between what is a code and what is guidance each code is provided with a unique reference number and is in a coloured box.

Demonstrating Compliance

Applications for new residential dwellings must show how they have complied with the applicable design codes and guidance.

Where a departure from this code is proposed within an application, a robust justification must be provided, along with any associated evidence to enable the council to appropriately determine whether the departure is acceptable.

Section 1: Authority Wide Codes and Guidance

SITE ANALYSIS

A key element of ensuring that proposals appropriately address their site and surrounding context is to undertake a thorough baseline analysis. Each site proposed for development must undertake their own site analysis.

Code SA1: Site Analysis

Submissions for new residential development must provide a site analysis which encompasses the following information, where applicable:

- Topography.
- Utilities.
- Conservation designations.
- Existing natural features e.g. hedges, trees.
- Neighbouring land uses.
- Separation distances to existing properties.
- Ecological designations.
- Land use designations.
- Location of closest facilities and services.
- Key views.
- Existing connections- vehicular, pedestrian and cycle.
- Public Rights of Way.
- Desire lines.
- Public transport routes and closest bus stops to the site.
- Flood risk and drainage.
- Landscape character.
- Archaeology.
- Pollution/contamination.

Please note this is not an exhaustive list and additional information may be required depending on the nature of the site.



Example of site analysis plans from the National Design Guide (2021)

MOVEMENT

You Said:

- » Within new housing areas, having safe and easy walking routes along with the ability to walk and cycle to facilities are the most desired connections.

Housing areas should benefit from easy and safe pedestrian, cycle and vehicular access. They must be safe to move around, for all residents, regardless of age or physical ability. Areas that are not easy to move around can become dominated by vehicular movement and can, for some residents, have a negative impact upon confidence and lead to social isolation.

Creating well-connected and healthy neighbourhoods

It is important that any new residential development successfully integrates into established communities through providing connections to existing movement networks and creating a network of routes for all modes of transport.

Code MN1: Integrated Network

- All new development must successfully integrate into existing movement networks and street patterns.

Pedestrian and cycle connections

The provision of a high quality, attractive and safe network of footpaths and cycleways is an important element of creating sustainable communities, promoting active travel within new residential developments and encouraging the use of streets as social spaces.

Walking and cycling should be the first choice for short local journeys, particularly those of 5 miles or less.

Code MN2: Pedestrian and Cycle Connections

- New pedestrian and cycle connections must be provided and create direct links to existing and/or proposed:
 - » Green infrastructure corridors.
 - » Public Rights of Way.
 - » Permissive routes.
 - » Pedestrian and cycle routes.
 - » Key locations identified within site analysis e.g. local centres, employment areas, schools and transport hubs.
 - » Green spaces within 800m of the site.
- Where pedestrian and cycle routes are provided they must be overlooked by properties and not include features that can create hiding places.
- Lighting must be provided for well-used footpaths and avoid light spillage.
- Where a shared pedestrian and cycle route is provided segregation between cyclists and pedestrians must be clearly marked.
- Proposals for dedicated cycle provision must be in accordance with the specifications set out within Design Guide & Specification – residential and industrial estates development (May 2021).
- Pedestrian routes must be a minimum of 2m in width.
- Shared pedestrian and cycle routes must be a minimum of 3m in width.

Connections Guidance:

- Pedestrian and cycle only routes are encouraged in locations where vehicle access is not available, for instance to connect cul-de-sac streets.
- Ensure any future connections required to expand development are understood and factored into the proposed network and include:
 - » Way-markings or sign posts.
 - » Countryside furniture, where appropriate.
 - » Use of natural materials e.g. boulders to prevent vehicle access.

MOVEMENT



Example of treatment of movement connections in proposals

MOVEMENT

Natural Surveillance

New residential areas must provide safe active travel routes through ensuring they are overlooked from the surrounding housing.

Code MN3: Natural Surveillance

- Development must be orientated to provide frontage or active side elevations onto existing or proposed pedestrian and cycle routes.
- Activation must be provided by ground floor main windows of habitable rooms.

Treatment of Public Rights of Way (PRoW) and Permissive Routes

The Guidance for Public Rights of Way SPD (2025) (Draft) provides details in relation to the treatment of PRoW and their design within Hartlepool.

The below codes set out the requirements for their treatment in relation to new residential development.

Code MN4: Treatment of PRoW and Permissive Routes

- Where PRoW and permissive routes are located within a proposed residential development site, the route must be retained, either in its current alignment or diverted
- PRoW existing or diverted must be located within a green route or open space.
- Connections must be provided into the existing PRoW network and proposed/existing active travel routes to create an accessible neighbourhood.

Treatment of PRoW and Permissive Routes Guidance

- Where it is proposed to divert an existing PRoW the route should not form part of an adopted route or Private Drive.

Design of PRoW

There are three types of PRoW designs.

- Urban path: a PRoW which is within the developable area of the site and not located within green space.
- Urban edge path: a PRoW within the development site but located within an area of green space.
- Rural: a PRoW within the countryside.

Code MN5: PRoW Urban path

- The design of routes within the developable area must:
 - » Provide connections from the developable area onto the PRoW.
 - » Be a minimum width of 2m for a footpath or 3m for a shared footpath/cycleway.
 - » Be constructed of a tarmac surface.
 - » Be located within a green route with a minimum of 1m of landscaping/ green space either side of the route.
 - » Provide resting places within the associated landscaping/green space (see Code N6), where route is over 200m in length.
 - » Not include any barriers.
 - » Be subject to active frontage from surrounding properties.
 - » Not run alongside proposed or existing highways.
 - » Provide countryside furniture, where required.
 - » Use natural materials e.g. boulders to prevent vehicle access.



Urban path example

MOVEMENT

Code MN6: PRow on Urban Edge

- The design of routes located within open space of residential development must
 - » Provide connections into the wider countryside network, the green infrastructure network and between the developable area and the PRow.
 - » Be a minimum width of 2m for a footpath or 3m for a shared footpath/cycleway.
 - » Be constructed of a bound surface or aggregate. Tarmac will not be considered an appropriate surface.
 - » Have a minimum of 2m landscaping / open space either side of the path.
 - » Be a minimum of 5m from the developable area, with the exception of the point of access.
 - » Provide resting places within the associated landscaping/green space (see Code N6), where the route is over 200m in length.
 - » Not include any barriers.
 - » Ensure any planting is designed to retain visibility and a sense of safety on the route.
 - » Include way-markings or sign posts.
 - » Provide countryside furniture, where required.
 - » Use natural materials e.g. boulders to prevent vehicle access.



Urban edge path example

Code MN7: PRow Rural

- The design of routes within the open countryside must:
 - » Provide connections into the wider PRow network.
 - » Be appropriately graded and well drained, where possible.
 - » Be a minimum width of 2m for a footpath or 3m for a shared footpath/cycleway.
 - » Be a minimum of 4m in width for a bridleway.
 - » Be constructed of aggregate, if surfacing is required, otherwise natural surface e.g. grass.
 - » Provide countryside furniture in accordance with British Standard BS5709:2018 for Gaps, Gates and Stiles.

PRow Guidance

- Where it is proposed to divert an existing PRow the route should not form part of an adopted route or Private Drive.
- The treatment of a PRow and road junction should consider including the following elements:
 - » Raised tables.
 - » Dropped kerbs.
 - » Clear markings and signage.



Rural path example

MOVEMENT

You Said:

- » Green links through housing areas and local parks are essential for encouraging walking, relaxing and community building..

Green Routes

Green routes provide off road connections between areas of green infrastructure.

Green Routes Guidance

- Where multiple areas of green space are being provided within proposals, green routes should be created to connect the spaces and form an off-road walking network.
- Green routes can utilise the Root Protection Areas requirements for retained hedges or tree belts
- They can be in the form of:
 - » Formal footpaths with surfacing or,
 - » Informal walking routes such as mown paths within areas of landscaping or grassed areas.
- Should have at least 1m either side of the path as grassed verge or landscaping.



Green route along woodland belt

MOVEMENT

Public Transport

Access to public transport is key to providing people with choice on journeys beyond their immediate neighbourhood. The bus network remains the most important mode of public transport within Hartlepool.

Code MN8: Public Transport

- New residential development must be accessible to bus services.
- All new housing must be within 400m walking distance of a bus stop.

Public Transport Guidance

- This may not always be possible in the more remote parts of the borough but in larger developments (over 200 units), developers may be required to contribute towards a bus service.

Car Free Residential Developments

The aim is to remove vehicles from residential streets thereby promoting the use of streets as social spaces whilst also encouraging sustainable transport methods such as cycling or walking. Car parking spaces are not provided for new residential dwellings within the plot or within the street.

Code MN9: Car Free Developments

- Where car free developments are proposed they must:
 - » Be located in sustainable locations with access to public transport.
 - » Provide connections into existing walking and cycling routes.
 - » Provide new pedestrian and cycling routes in accordance with Code MN2.
 - » Provide access for emergency services.
 - » Include delivery bays and/ or hubs.
 - » Incorporate communal cycle parking.
 - » Incorporate well-designed central bin collection points.
 - » Provide landscaping, planting and trees within streets.
 - » Provide informal play facilities (see code N7).
- Provide parking in a central location, where required, and must:
 - » Be at reduced provision levels to those set out in Highways Design Guide & Specification – Residential and Industrial Estates Development (amended May 2021)
 - » Be in the form of a courtyard (see Area Type Codes) and within 400m of the proposal.



Car free housing - Vaux, Sunderland

Car Free Development Guidance

- The surface material chosen for car free streets must signify that they are vehicle free and should consist of a paved surface. The use of tarmac will not be encouraged.
- Consideration should be given to incorporating dedicated bays to car share clubs.

BUILT FORM

Hartlepool has a rich and varied built form, from historic villages in the rural area to the Headland with its unique coastal location and the urban area with Victorian and Edwardian terraced houses and villas. Most recent residential developments have been focused on the urban edge and Wynyard, creating large scale urban extensions.

When buildings and spaces are designed correctly, in most instances, they blend seamlessly with surrounding land uses, reflect local character and create a sense of place. Well-designed buildings and spaces should be aesthetically pleasing and assist in uplifting mood or heightening senses in a bid to improve the overall well-being of residents and visitors.

Code BF1: Character Analysis

- Due to the variations in character within the borough it is important that each proposal appropriately reflects any positive features of it's surroundings. A character analysis must be undertaken and must be referenced in the design of the proposed built form. The character analysis must include:
 - » Predominant house types e.g. apartments, bungalows, terraced, semi-detached, detached.
 - » Predominant building heights.
 - » Density.
 - » Massing.
 - » Distinctive buildings which positively influence the character of the wider area. Window proportions, rhythm and treatments.
 - » Entrance treatments e.g. canopies, porches.
 - » Features e.g. chimneys.
 - » Building materials and colours.
 - » Landscaping.
- Please note this is not an exhaustive list and additional information may be required depending on the nature of the site.
- Where an area has limited, or no positive architectural features which contribute to character, design precedents must be provided.



Example of character analysis - National Model Design Code (2021)

BUILT FORM

You Said:

- » Homes should look different from each other. Distinctive architecture, detailing, and the overall identity of the neighbourhood is important.

Legibility

New residential areas must be easy to navigate for residents and visitors. To enable this there needs to be a clear distinction of places within proposals through the use of street hierarchy, changes in the built form and materiality, and the use of landscaping and open space.

The inclusion of corner turning buildings, landmark buildings and gateways must aid in wayfinding and legibility.

Code BF2: Corner Buildings

- Corner turning buildings must:
 - » Be provided where both the front and side elevations of a property present onto the public realm.
 - » Be dual frontage properties, i.e. provide ground floor activation and detailing on the two sides which face onto the public realm.
- To aid in legibility and way finding corner turning house types must be differentiated throughout proposals. E.g. on major



Corner turning building -Seaton Meadows

developments where more than one corner turning building is needed, a change in house type or materiality must be provided.

- Corner plots where rear gardens border the public realm must provide a boundary treatment of either:
 - » Brick wall.
 - » Brick wall and timber panelling combination.
- A 1.8m timber fence will not be considered an appropriate response on corner plots.

Code BF3: Landmark Buildings

- Landmark buildings must be easily identifiable, contrast from the surrounding built form and have a sense of prominence.
- Landmark buildings must have:
 - » Increased scale.
 - » Change in materials.
 - » Different elevational treatment.
- Landmark buildings must be provided at key locations including:
 - » Site entrance, both vehicular and pedestrian if separate.
 - » Nodes.
 - » Prominent corners.
 - » Key view terminus points.



Landmark building - Lucy Cavendish

BUILT FORM

Frontage and Building Line

Successful streets are characterised by active edges and natural surveillance where the street space is overlooked from homes. This creates a sense of ownership and safety over the street. In residential areas it is important that elevations facing the street include entrance doors and windows from habitable rooms.

For new residential development to sit comfortably within its surroundings, it is important that the proposed building line respects the existing built form.

Code BF4: Active Frontages

- To ensure the creation of safe and attractive streets, which encourage active travel and the use of streets as social spaces, new residential development must present active primary elevations on to the street with:
 - » Front doors located onto the street.
 - » Windows from habitable rooms must face onto the building line to provide natural surveillance over the street and public spaces.

Code BF5: Building Line

- New residential developments must follow the established building line where it exists.
- Proposals within the Area Types must follow the building line guidance set out within the relevant Area Type section.

Street Scene

Careful consideration needs to be given to the house types proposed and how they will be read within the street.

Street Scene Guidance

- House types proposed should create a coherent street scene taking into consideration the scale of the properties and the elevational detailing.

Phased Development

Where large scale residential development is proposed, which will be provided over several phases, it is important to ensure that the built form between the phases relates well to one another and that it is not clear where the boundary of the phase is.

Phased Development Guidance

- Ensure that built form of different phases of development sits comfortably and relates well with each other. This should be achieved through the use of similar:
 - » Building materials.
 - » Detailing e.g. window treatment.
 - » Proportion or pattern of fenestration.
 - » Scale of the buildings.

Conservation Areas

There are 8 conservation areas within Hartlepool. The codes and guidance within this SPD do not provide a detailed analysis of the conservation areas, these are provided within Conservation Area Management Plans (CAMPs) or Visual Assessments.

Code BF6: Heritage and Conservation

- Any proposals for new residential development within a conservation area must take into consideration the relevant CAMPs and/ or Visual Assessments, where applicable.

HOME & BUILDINGS

You Said:

- » Ensuring privacy, quiet, and internal space are important in new developments.
- » Attention should be paid to both internal and external character.

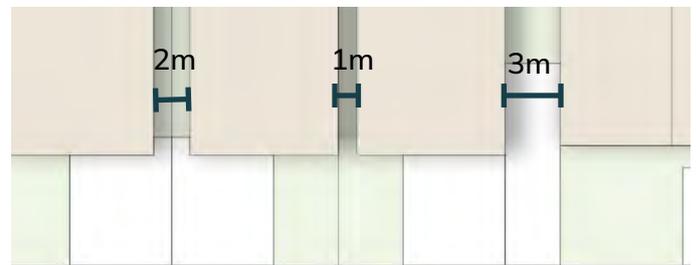
Separation distances

Residents must be able to enjoy their home and must enjoy a certain level of natural light, must benefit from privacy and must not be unreasonably overlooked by neighbours or passers-by. To ensure this, residential separation distances must be applied to all new developments.

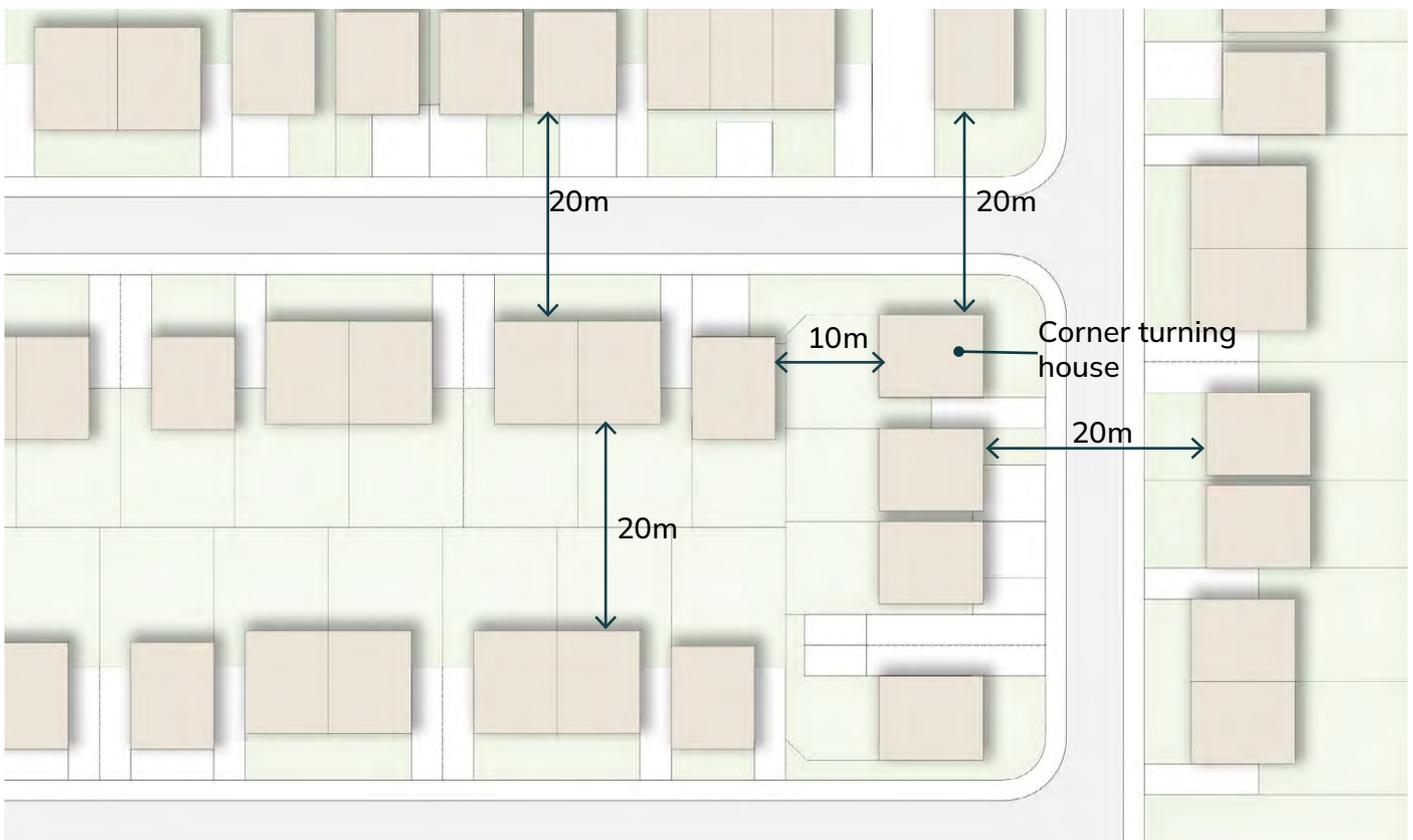
A reduced distance may be considered where it can be demonstrated that this will assist in creating a variety of streets or an exemplar development.

Code HB1: Separation Distances

- The following standards are required, as a minimum, of all new residential developments.
 - » 20m between principle elevations.
 - » 10m between gable and principle elevations.
 - » 2m between blank gable elevations.
 - » 1m between blank gable elevations will be considered where the preceding distance between gables is 3m or over.



Separation between blank gables plan



Separation distances plan

HOME & BUILDINGS

Internal Space Standards

Residents should have sufficient space within their homes to undertake typical day-to-day activities. Providing adequate space within dwellings allows households to socialise with family members and guests, improves storage capacity, improves space for solitary activities, provides greater flexibility in arranging rooms to meet different/changing needs, allows more opportunity for working from home, provides more space for managing waste and recycling, and improves daylight and ventilation.

The Nationally Described Space Standard (NDSS) deals with internal space within new dwellings and is suitable for application across all tenures. It sets out requirements for the Gross Internal (floor) Area of new dwellings at a defined level of occupancy as

well as floor areas and dimensions for key parts of the home, notably bedrooms, storage and floor to ceiling height.

The Gross Internal Area of a dwelling is defined as the total floor space measured between the internal faces of perimeter walls that enclose the dwelling. This includes partitions, structural elements, cupboards, ducts, flights of stairs and voids above stairs.

Code HB2: Nationally Described Space Standards

- All new dwellings must, as a minimum, meet the Nationally Described Space Standards (2015), as set out below.

Number of bedrooms	Number of bed spaces (persons)	1 storey dwellings	2 storey dwellings	3 storey dwellings	Built-in storage
1b	1p	39 (37)*			1.0
	2p	50	58		1.5
2b	3p	61	70		2.0
	4p	70	79		
3b	4p	74	84	90	2.5
	5p	86	93	99	
	6p	95	102	108	
4b	5p	90	97	103	3.0
	6p	99	106	112	
	7p	108	115	121	
	8p	117	124	130	
5b	6p	103	110	116	3.5
	7p	112	119	125	
	8p	121	128	134	
6p	7p	161	123	129	4.0
	8p	125	132	138	

Minimum gross internal floor areas and storage (m²)

* Where a 1b1p has a shower room instead of a bathroom, the floor area may be reduced from 39m² to 37m², as shown bracketed.

HOME & BUILDINGS

You Said:

- » Homes should have their own growing space, even balconies.

Private Amenity Space

Residents must have the space to enjoy their private amenity space and undertake day-to-day activities, like drying clothes, eating out and playing. Private amenity space must visually add to the quality of the home and the area.

Code HB3: Private Amenity Space

- All dwellings to be provided with outdoor private amenity space commensurate to the size of the dwelling.
- For dwelling houses gardens must:
 - » Provide sufficient space for the anticipated day-to-day needs of the residents.
 - » Be usable e.g. where topographical changes occur, terracing must be created.
 - » Be orientated to benefit from good levels of daylight.
 - » Include vegetation and/or landscaping to improve the overall visual quality of the dwelling and the area.
 - » Enable space for food growing opportunities.
- For flatted development amenity space must:
 - » Be provided for all occupants within the curtilage of the building. This may be in the form of outdoor private space for ground floor accommodation, balconies for accommodation above ground floor level, or communal amenity space.
 - » Private amenity space must be a minimum of 5m² for 1-2 person apartments with an additional 1m² per additional resident and a minimum depth of 1.5m.
 - » Enable space for food growing opportunities e.g. container gardens or communal growing gardens.

Refuse Storage

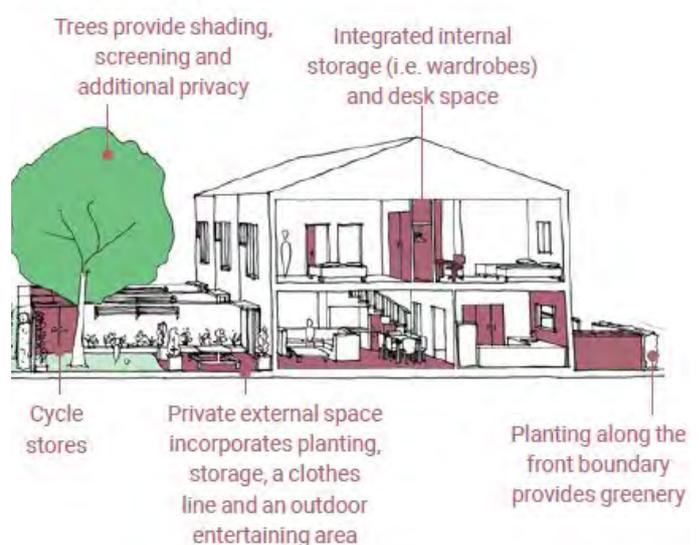
The provision of accessible bin storage must be provided for each dwelling, the provision of dedicated bin stores must avoid issues with waste receptacles cluttering the street scene or impacting on the character of the built form.

Code HB4: Refuse Storage

- Suitable refuse space of at least 1.5 square meters must be provided for each unit.
- In accordance with current building regulation bin stores must be located within 45m of a suitable collection point on the street network.

Code HB5: Bin Collection Points

- Bin collection points must be provided on Private Drive street types.
- Be located a maximum of 25m from the adopted highway.
- Have a hard standing surface.
- Be of a size to accommodate one bin per unit of the Private Drive.



Private amenity space - National Design Guide (2021)

HOME & BUILDINGS

Refuse Storage Guidance

- When located to the rear or side of the building bin storage should be marked by a hard standing area or a purpose-built store.
- When located to the front of properties these should be:
 - » Co-located with cycle parking.
 - » Screened from public view with an appropriate boundary treatment, which compliments the materials of the primary building, or
 - » Incorporated into the design of the building.
- Communal waste bins should be in accessible locations and provide screening from the street scene.

Cycle Storage

The promotion of more sustainable alternatives to the private car is a consistent theme within both national and local transport policy. For this reason, secure and conveniently located cycle parking facilities must be provided for all new developments.

Code HB6: Cycle Storage

- Every unit must be provided with cycle parking space.
 - » Dwellings – 2 spaces per dwellings, in curtilage.
 - » Apartments communal parking – 1 per 4 units for up to 100 units.
- Cycle storage must be in accessible locations, secure and dry.
- Where cycle storage is proposed to the front of homes this must be accommodated alongside refuse stores and not dominate the street scene. Materials used must complement the primary building material.

Utilities

Placement of meters and their housing requires careful consideration to ensure they do not impose unduly on the overall facade of properties, whilst still being accessible for reading.

Code HB7: Utilities

- Meters must be housed to the side elevation of properties.
- Where this is not feasible, for instance on terraced properties, the meter must be designed in a manner to not be over imposing on the facade. i.e the colour of the housing box used.



Meter box impacting on elevation

NATURE & PUBLIC SPACE

You Said:

- » Access to open spaces, street trees, and community gardens are desirable features for communities.

To add to the overall character and quality of an area and to assist in improving physical and mental health, all residents must have access to open spaces and other green infrastructure within the vicinity of where they reside. In providing open spaces close to home and access to a green network, residents will have greater opportunities for play, sport, recreation, relaxation and local food production, which in turn will promote active healthy lifestyles close to home.

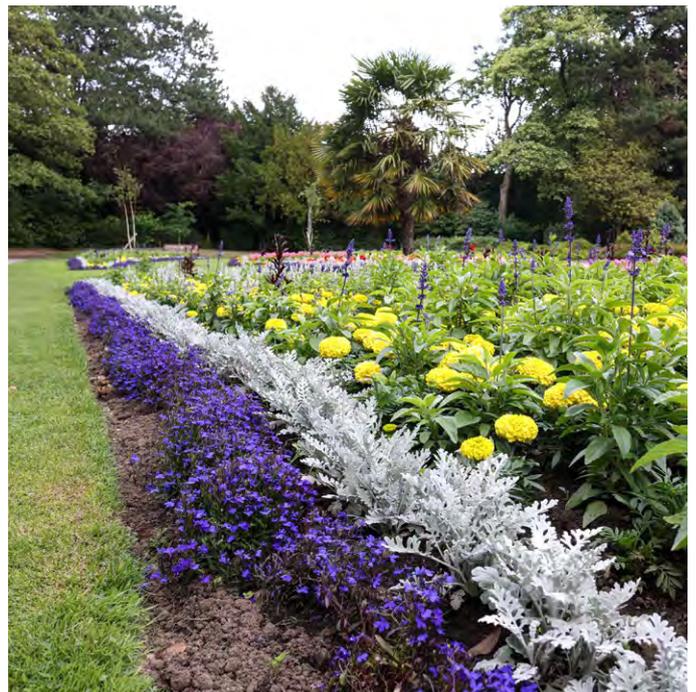
The Green Infrastructure SPD has an aim of 'providing a network of interconnected GI and spaces rich in habitat and home to a diverse range in wildlife'. New residential developments must contribute to the delivery of this aim through the provision of new green infrastructure from large open spaces to tree lined streets.

Code N1: Green Space

- Green spaces must to be at the heart of new residential developments, ensuring that everyone has access to green spaces where recreational and leisure activities can take place.
- Any proposed green space must be within 800m of all proposed properties.
- Green space must be in easily accessible locations and usable by all.
- Green space must create nature rich spaces close to where people live.
- The scale of any proposed green space and its features will be dependant on the level of residential development being proposed and the needs of the community.
- Formal play space must be provided when a need is identified and in accordance with Code N8.



Summer Hill, Hartlepool



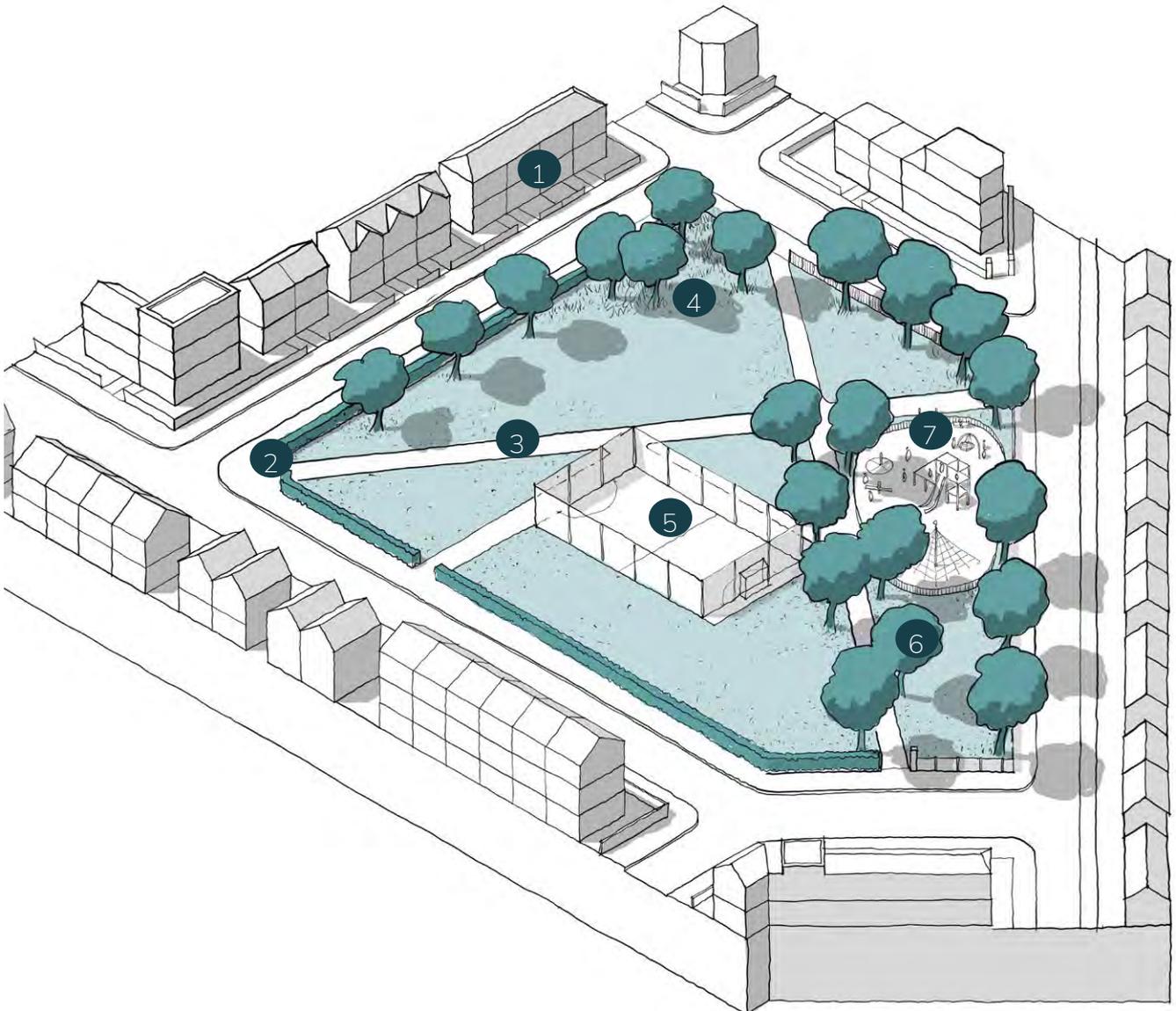
Ward Jackson Park, Hartlepool

NATURE & PUBLIC SPACE

Green Space Design:

The way in which spaces are designed is crucial to their success. The design considerations vary with the type of space, a formal park being very different to a large natural green space.

- 1 Ground floor activation
- 2 Convenient entrances
- 3 Attractive, clear & direct routes
- 4 Nature rich
- 5 Sports facilities in locations to reduce disturbance
- 6 Tree planting
- 7 Multi-functional spaces



Example of well designed multi-functional green space

NATURE & PUBLIC SPACE

Code N2: Green Space Design

- Proposals for green space within new residential development must:
 - » Provide active ground floor windows onto green space.
 - » Avoid presenting rear boundaries onto open space.
 - » Ensure entrances and access points into green spaces are conveniently located on desire lines for walking and cycling.
 - » Provide attractive, clear and direct pedestrian and cycle routes in accordance with Code MN2 and constructed of tarmac.
 - » Be multifunctional spaces.
 - » Incorporate drainage solutions (see Code N11).
 - » Include areas that are nature-rich and provide opportunities to improve biodiversity through planting.
 - » Include tree planting.
 - » Place landscaping to discourage anti-social behaviour.
 - » Protect spaces from unauthorised vehicular access, by using sensitively placed obstacles, such as rocks or vegetation.
 - » Provide a space which facilitates informal play and socialising.
 - » Ensure sports facilities are in locations that limits disturbance to properties and avoids conflict with other users.
 - » Ensure barriers around sport pitches are permeable and made of high quality materials.
 - » Provide lighting for well-used footpaths and games areas, their type and placement must avoid light spillage.
 - » Provide appropriate management that is secured under a s106 agreement or other such secure arrangement.

Code N3: Creating a Network

- Proposals for green spaces must be connected into the wider Green Infrastructure network.
- Where more than one green space is provided on site, connections must be provided between them. Connection must be for pedestrian, cycle and where feasible, wildlife movement.
- Where green spaces sit on the edge of the site and border the rural edge/countryside, plans must be submitted outlining how access and connection into the countryside are provided.

Code N4: Community Growing

- Green spaces of over 1 hectare must provide community growing facilities such as communal allotments or orchards.



Communal food growing - Hartlepool

NATURE & PUBLIC SPACE

Landscape Pockets

Landscape pockets are small areas of landscaping to be provided within certain street types (see Area Types) and offer the opportunity to provide visually attractive areas of landscaping, increase the provision of street trees and can incorporate resting places with street furniture.

Code N5: Landscape Pockets

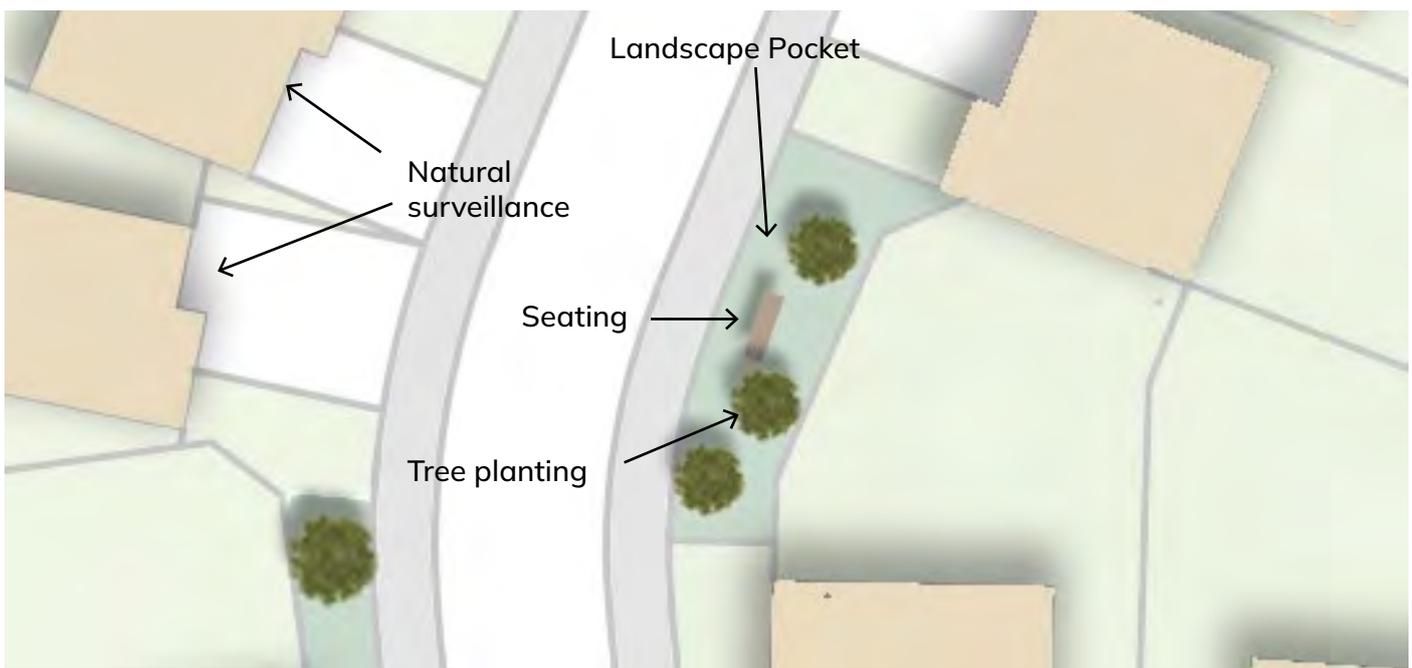
- Provide landscape pockets in accordance with the street typology of the Area Type.
- Must create high quality areas of landscaping which:
 - » Are a minimum of 50m².
 - » Incorporate street trees and planting.
 - » Include resting places (see Code N6).

Code N6: Resting Places

- To encourage active travel by all residents, resting places must be provided approximately every 200m.
- Where a proposed street length does not meet the 200m requirement at least one resting place must be provided within a site.
- Provide seating with a back and arm rest and ensure shading is created through tree planting.
- Seating must be constructed of a low maintenance and robust material.
- Can be incorporated into landscaped verge and landscape pockets depending on the street typology (see relevant Area Type Codes).
- Placed in areas subject to natural surveillance.
- Must not obstruct movement on the footway.

Resting places

Resting places invite people to sit, dwell and enjoy their surroundings. They are an integral part of well-designed active places through encouraging people to walk and cycle more and for longer distances. Resting places are particularly important for older people and people with mobility issues.



Example of landscape pocket with resting place

NATURE & PUBLIC SPACE

You Said:

- » Having trees planted within the street was the most attractive feature, followed by the provision of open space near homes and strips of landscaping along the street.

Play Areas

Encouraging healthy and active lives is a priority for the council. Including areas for informal and formal play will be key to achieving this. Play spaces are also important in encouraging social interaction and creating cohesive communities.

Informal Play

To encourage the use of streets by families, improve health and well-being and create more child friendly places, the inclusion of informal doorstep play or 'play on the way' must be provided within new residential developments.

Code N7: Informal Play

- Informal 'play on the way' opportunities must:
 - » Be incorporated through utilising verges, landscape pockets, green routes and green spaces.
 - » Use natural materials. e.g. tree stumps and rocks.



Play on the way - Farmstead Road, Lewisham

Code N8: Formal Play Areas

- Where formal play areas are required they must:
 - » Be easy to find and access using safe and direct active travel routes.
 - » Predominantly use natural materials for equipment.
 - » Be subject to natural surveillance either from active frontages from surrounding residences or other functions to prevent anti-social behaviour and to encourage a sense of safety.
 - » Be enclosed. The boundary treatment must relate to the Area Type, be permeable in nature and up to 1.2m in height.
 - » Provide seating areas with shade.
 - » Be accessible and suitable for all.

Play Guidance

- Where new play facilities are being provided within a site, discussions will be required between the applicant and the council to ensure the most appropriate form of play is being provided.



Play area with natural materials - Wynyard, Hartlepool

NATURE & PUBLIC SPACE

Trees and Hedges

Incorporating trees and other landscape features in streetscapes provides habitat, shading, cooling, air quality improvements and carbon sequestration, as well as being a vital component of attractive places.

Research has proven that trees provide significant benefits for developers and property owners by increasing property values and selling prices. Hartlepool currently has a tree canopy cover of 9.4%, placing it among the lowest local authority areas nationally and substantially below the national average of 15.8%. The Environment Act 2021 establishes a statutory target for increasing tree canopy cover to 16.5% across England, reinforcing the requirement for development to safeguard existing trees and hedgerows and to incorporate meaningful new tree planting within layout proposals.

Code N9: Trees & Hedges

- Existing category A and B trees within development sites must be retained.
- Where there is a requirement to remove an existing tree, exceptional circumstances must be demonstrated. The tree/s must be replaced on-site.
- There must be no net loss of potential canopy cover within the curtilage of a development.
- Existing hedges must be retained and utilised within development proposals.
- Replacement trees must be planted no later than the next available planting season upon completion of the relevant phase of development or before the occupation of a property, whichever comes first.
- Trees must be suitably positioned to allow sufficient space for mature growth. Projected crown size must be used to ensure that conflict does not occur with property, infrastructure, street lighting and highway sight lines.
- Existing hedges and their associated easement must be utilised for green routes within proposals.
- Tree planting must be appropriate for a changing climate and suitable for the future, not just the present. The introduction of non-

native species into planting schemes where suitable must be explored to ensure a resilient and biodiverse tree population of the future.



Tree planting within landscaping - Hartlepool



Tree planting within landscape pocket - Goldsmith Street



Existing trees and development - Place Fields

NATURE & PUBLIC SPACE

Street Trees

Incorporating trees within streets will aid in the greening of new residential areas of Hartlepool and bring health, biodiversity, economical and ecological benefits.

Code N10: Street Trees

- Street trees must be provided in several locations depending on the street type.
- Trees must be:
 - » Within verges of Neighbourhood Streets to create avenues.
 - » Within landscaping pockets within Residential Streets and Lanes.
 - » Between parking bays where landscaping is required to break up more than 4 consecutive spaces.
 - » Within the carriageway, in build outs, consideration must be given to ensuring visibility.
- Underground rooting systems must be used to accommodate street trees where space is restricted and incorporate water management.
- Tree species selection must be relevant, specific and suitable for its intended use following existing research and literature designed to inform specifiers such as - Trees & Design Action Group – Tree Species selection for Green Infrastructure, a guide for specifiers and The Essential Tree Selection Guide from RBG Kew.

Guidance

- Coordinating tree planting with utilities providers and service ducts early in the lifetime of a scheme can ensure that trees do not interfere with underground services.
- Tree planting should be carefully selected and positioned to allow space for the mature tree without causing obstruction or interfering with property, infrastructure, street lighting or junction sightlines.

Trees in Gardens Guidance

- Tree planting within front gardens is encouraged however this should not be the sole provision of street trees due to the potential for their removal by future occupiers.



Street trees - Ashmere, Kent

NATURE & PUBLIC SPACE

Sustainable Drainage Systems (SuDS)

SuDS are designed to manage storm water locally (as close to sources as possible), and mimic natural drainage to reduce the effect on the quality and quantity of surface water run off from developments. When designed and managed appropriately SuDS can positively contribute to place making and provide multiple benefits including visual amenity, recreational, and ecological.

The codes and guidance below are in relation to the design of the SuDS and should be considered alongside any technical specifications. Consultation with the Lead Local Flood Authority (LLFA) at the earliest opportunity is advised.

Code N11: SuDS

- The design of SuDS must be in accordance with:
 - » National Standards (2025) or any future versions.
 - » Industry recognised guidance including C753 The SuDS Manual.
- SuDS within new residential proposals must:
 - » Be integrated into on-site green infrastructure provision in accordance with the principles and standards of Natural England's Green Infrastructure Framework.
 - » Not negatively impact upon the level of usable green space for leisure and amenity.
 - » Support Biodiversity Net Gain.
 - » Provide recreational routes on their perimeter.
 - » Be natural in appearance, forming shapes which mimic natural ponds.
 - » Incorporate features such as rocks and pebbles.
 - » Include appropriate planting.
 - » Minimise any visual impact of large pre-cast concrete outfalls with key-clamp railings.
 - » Complement the landscape character of the proposal.

SuDS Guidance

- SuDS designed into highway provision are encouraged and can be provided within verges in the following forms:
 - » Swales.
 - » Rain gardens.
 - » Tree pits.



Suds within open space - Brooklands, Milton Keynes



Suds within street - St Chads, Tilbury

RESOURCES

Renewable Energy

The council is committed to taking a leading role and working with everyone across the borough to act now on the causes and impacts of climate change.

Residential developments have a key role to play in helping to address climate change and tackle fuel poverty through the use of sustainable design and construction.

The government has set a target to reduce greenhouse gas emissions to net zero by 2050 with an interim carbon budget requiring a 78% reduction in emissions by 2037. Incorporating renewable, decentralised or low carbon energy in new residential developments is an important component of meeting these targets.

Renewable Energy Guidance

- To reduce the carbon footprint of new homes the following renewable technologies should be incorporated:
 - » Solar panels.
 - » Heat pumps.
 - » Rainwater harvesting

Fabric First

Much of the focus of improving the energy efficiency of new buildings is on the building fabric itself and energy technologies. Fabric First is an approach to design which maximises the efficiency of the components and materials that make up the fabric of the building.

Fabric First Guidance

- Fabric First should be considered alongside the use of renewable technologies.

Solar Gain

Solar gain refers to the increase in temperature in a space, object or structure that is a result of energy from the sun. Energy from the sun is free; therefore solar gain can reduce heating costs as well as having significant environmental benefits.

The principles of solar gain use the design and positioning of buildings to maximise the amount of natural heat and light that is obtained. There should however be a balance with the need for energy efficiency and the provision of natural surveillance and other elements of this code.

Solar Gain Guidance

- The main elevation should face within 30 degrees of due south.
- The main long axis of the building i.e. the ridge line should run east-west.
- Taller buildings should, where possible, be located to the north of the site to avoid restricting sunlight.
- The largest part of the roof's surface should be south facing or SSE/SSW for solar panel placement.
- The incorporation of either an overhang on the southern elevation should be used to prevent overheating.
- Glazing should be maximised on the south to maximise energy from the sun and minimised on the north elevation.
- Use buildings, trees and landscaping to provide protection from prevailing winds.



Section 2: Area Type's

AREA TYPES

An analysis of the general characteristics of the borough has been undertaken to define 3 distinct Area Types.

- Outer Neighbourhood (ON)
- Wynyard (WY)
- Village (VL)

Within each Area Type there are variations within the street network and built form however there are overall similarities in characteristics of the areas.

These characteristics along with the views expressed during the public consultation exercise, views of key stakeholders, and aspirations for the areas have been used to inform the following codes and guidance for each Area Type.

Proposals Outside of Area Types

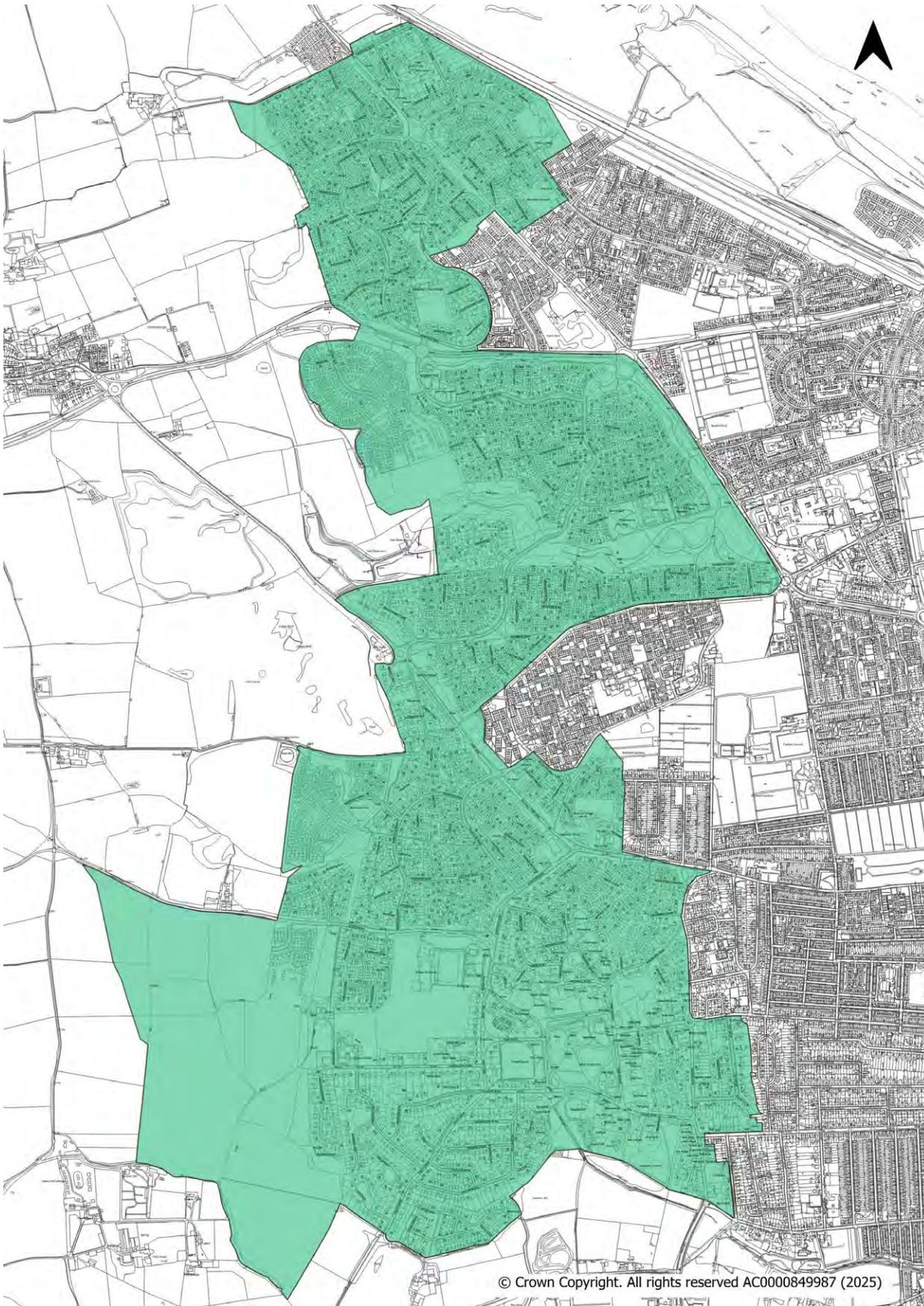
The area wide section does not provide codes for the street network, parking and built form detail, as these are covered by the Area Types. For proposals located outside of the Area Types the detailed design for street network, parking and built form detail will be based upon the following code:

Code AT1: Proposals Outside of Area Types

- Where a proposal is located outside of an area type the following should be applied for the Street Network, Parking and Built Form detail:
 - » Located within the existing urban area - applications will be determined on a case-by-case basis.
 - » Located outside of existing urban area and adjacent an Area Type -Proposals must use the codes within that Area Type adjacent to the site.
 - » Located outside of the existing urban area - applications will be determined on a case-by-case basis.

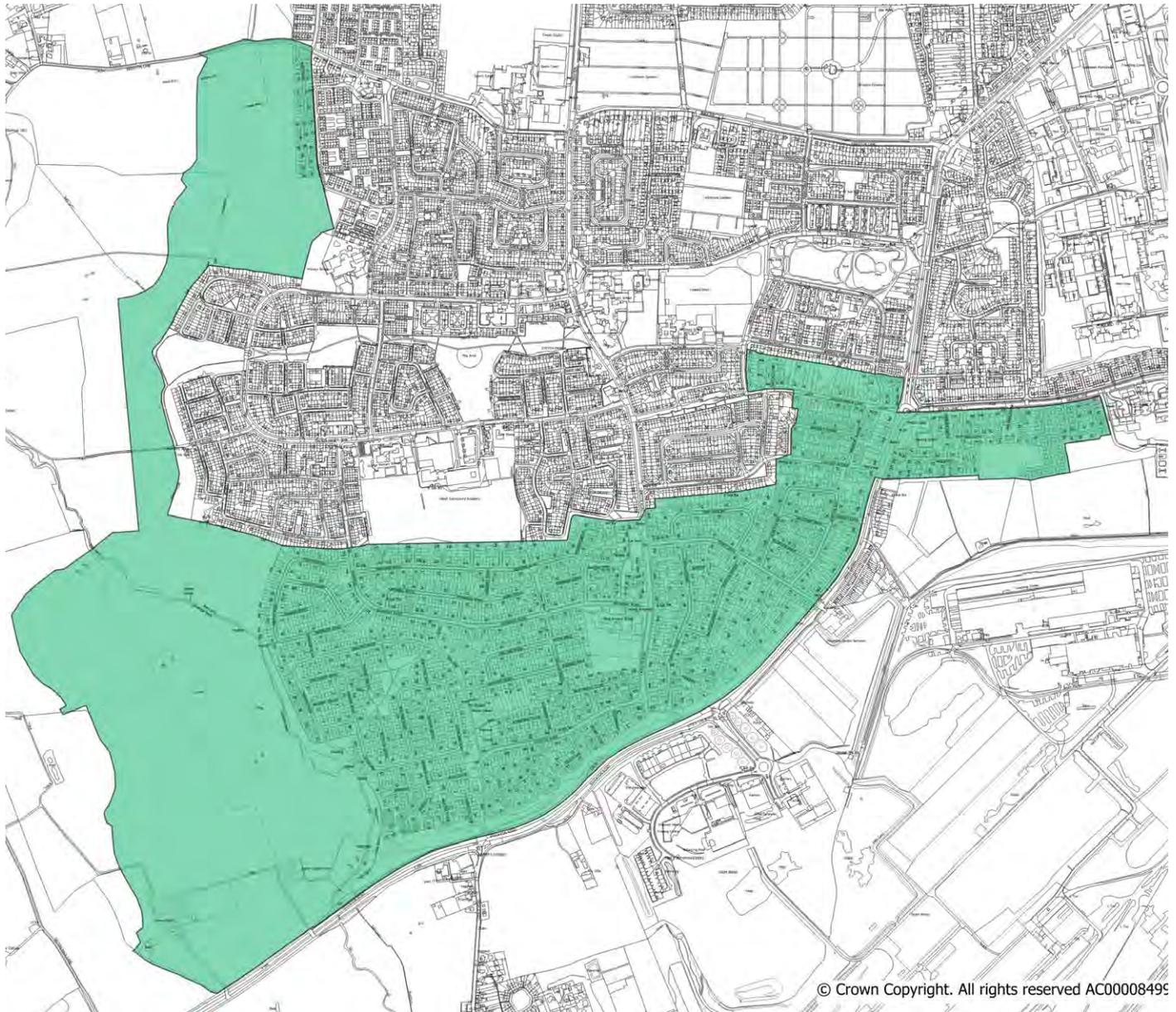
Outer Neighbourhood Area Type

OUTER NEIGHBOURHOOD CODING PLAN



OUTER NEIGHBOURHOOD CODING PLAN

ON



OUTER NEIGHBOURHOOD AREA TYPE (ON)

The Outer Neighbourhood Area Type is characterised by suburban housing on the edges of Hartlepool and the Local Plan housing allocations on the western edge of the urban area. The expansion of Hartlepool over time within this location has resulted in variation between the existing neighbourhoods.

The analysis of the areas has highlighted key characteristics which are present within the neighbourhoods and will be used to inform the codes and guidance for future development within the area.

Outer Neighbourhood Summary of Analysis.

Streets and movement

- Street hierarchies are evident in most areas.
- Wider avenue streets with landscaped verges forming the primary street. Public transport is possible through carriageway width although inappropriate parking can impede this.
- Streets with lower traffics have a greater sense of enclosure.
- Streets have a predominance of front of plot parking where this is not accompanied by front gardens or landscaping the street scene is dominated by parking.
- Inappropriate parking within the street can negatively impact upon the street scene.
- Streets are predominately cul-de-sac's reducing pedestrian permeability and impacting upon way finding.



- Pedestrian connections between recent developments and existing communities is not always provided.

Built Form

- Properties are predominately detached and semi-detached in nature within instances of short runs of terraces in areas surrounding local centres or key facilities e.g. schools.
- Properties are mainly two storey in nature with increases in scale to 2.5 to 3 storey at key locations such as entrances or around local centres.
- Properties are usually provided with a set back of between 3 and 6m from the street to accommodate front of plot parking and front gardens.
- The building line is typically consistent with variation provided on lower density streets toward the rural edge or green spaces.
- Many of the areas form previous large scale urban extensions such as Bishop Cuthbert.

Materials

- Red brick.
- Buff brick.
- Render - white or cream.
- Tile hangings.



OUTER NEIGHBOURHOOD AREA TYPE (ON)

Architectural Detailing

- There is a mix of architectural styles and features which are typical of their era of construction.
- Common features include:
 - » Mini gables.
 - » Gabled front elevations.
 - » Head and cill detailing.
 - » Hipped roofs.
 - » Chimneys.
 - » Dormer windows.
 - » Bay windows.
 - » Canopies.
 - » Porches.

Green Space / Landscaping

- Large open spaces, with formal planting and play spaces.
- Verges provided on Neighbourhood Streets with limited planting.
- Connections provided into surrounding countryside and PRow network.
- Areas of incidental open space with planting.

Facilities

Local centres and schools have been provided to meet the needs of the community. Pedestrian connections are provided, however the location of the facilities are not always within the centre of the neighbourhood which they serve.



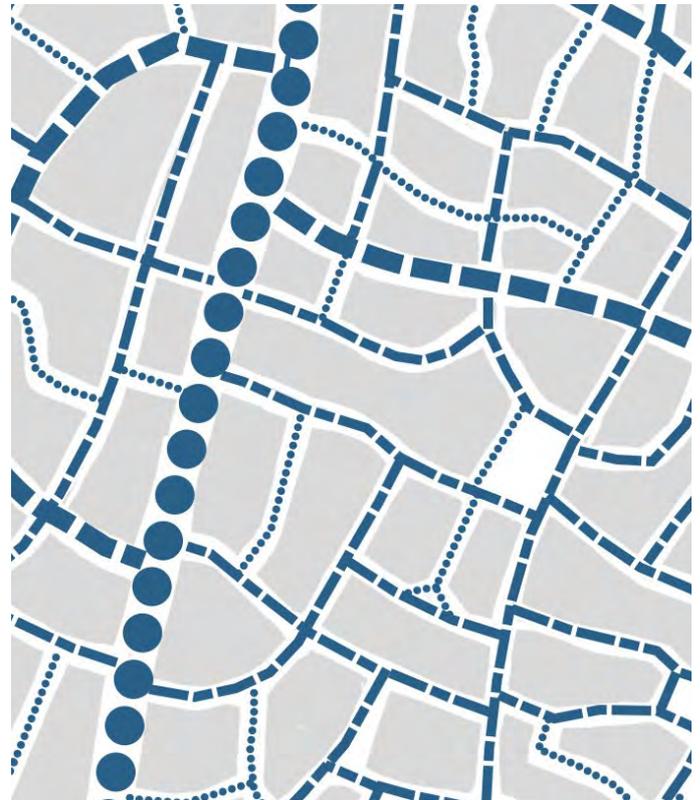
STREET NETWORK (ON)

The design of the street network plays an important role in determining how it is used. Different streets play different roles in a place depending on the movement upon them, the built form and uses around them and the design of the street space itself, including any natural features.

A key characteristic of the built form within the Outer Neighbourhood Area Type is the provision of a hierarchy of streets, from wide Neighbourhood Streets providing the main connections through the development and to key facilities, to shared surface 'Lanes,' which are more informal streets with a greater sense of enclosure.

You Said:

» The most positive feature of residential streets is the availability of planting and public green spaces along with wide streets and pavements.



Example street network

- ● ● Main Distributor
- ■ ■ Neighbourhood Street
- — — Residential Street
- Lanes



Street planting - Marleigh Park Phase 2, Cambridge



Street planting - Liberty Quarter, Kent

STREET NETWORK (ON)

Neighbourhood Street

The aim of the Neighbourhood Street is to provide the main pedestrian, cycle, public transport and vehicle connections through a residential area, these routes can include connections between development parcels and local centres, schools and open spaces. Neighbourhood Streets must connect into the existing movement network surrounding the area and provide through routes.

Code ON1: Neighbourhood Street Verge

- Minimum of 2m verge must be provided on one side of carriageway. This can alternate between sides but must be provided for the entirety of the street.
- The verge must be provided for at least 70% of the total street length taking into consideration driveway crossings. e.g. for a Neighbourhood Street 100m in length the verge must be provided for at least 70m (See example layout opposite).
- Verge must incorporate street tree planting (see Code N10).
- Resting places must be provided every 200m (approx), see Code N6.
- Where visitor parking is provided, their location within the verge corridor will only be considered acceptable as long as 70% of the total street length is verge.
- Barriers must not be provided alongside verges or footways. This enables permeability and reduces visual clutter.

Code ON2: Neighbourhood Street Carriageway

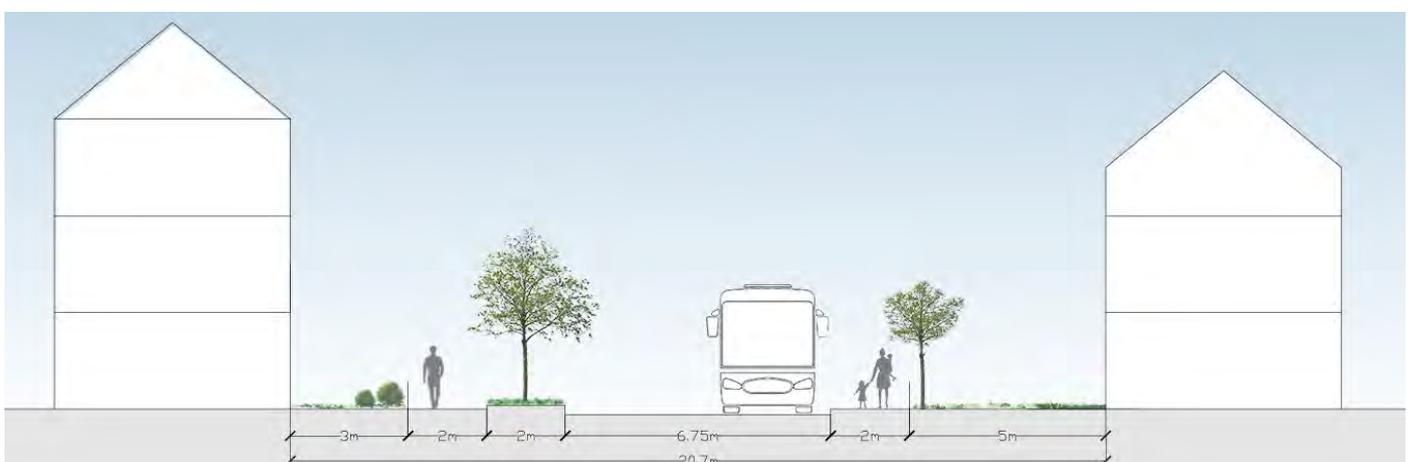
- Provide a carriageway width of 5.5m.
- Where public transport is proposed the carriageway must measure a minimum of 6.75m.
- Cul-de-sacs are not permitted.

Code ON3: Neighbourhood Street Footway and Cycleway

- A minimum footway width of 2m must be provided on both sides of the carriageway.
- Cycling provision to be as either a 3m shared pedestrian/cycleway or a segregated 2m cycleway.
- Cycle links must connect into nearby existing off-road cycle routes, where available.

Code ON4: Neighbourhood Street Setback

- Whilst allowing for the separation distances set out in HB1, a set back of between 2m and 6m must be provided from the pavement.
- Projections such as bay windows, porches and balconies are permitted.
- Variation: the front face of buildings must not vary more than 3m from the building line.



Neighbourhood Street cross-section

STREET NETWORK (ON)

Neighbourhood Street Guidance

- Drainage features such as rain gardens are encouraged in verges (see Code N11).
- The route of Neighbourhood Streets should follow desire lines identified in the context analysis of the site.
- Crossings should be located on or close to desire lines so that pedestrians find them convenient and pleasant to use.



Example of Neighbourhood Street with verge



Verge within Neighbourhood Street - Liberty Quarter, Kent

STREET NETWORK (ON)

Residential Street

Residential Streets mainly carry local traffic and provide connections through development parcels. These streets typically see lower traffic flow and support active travel and encourage the use of streets as social spaces.

Code ON5: Residential Street Carriageway

- Provide a carriageway width of 5.5m.
- Cul-de-sacs are not permitted.
- A minimum footway width of 2m must be provided on both side of carriageway.

Code ON6: Residential Street Landscaping & Planting

- To ensure the creation of green streets throughout new residential developments and the provision of street trees, which are not within private ownership, landscaping pockets of a minimum of 50m² must be provided within Residential Streets. See Code N5.
- These pockets must form 'incidental open space' and not form part of the curtilage of any dwelling.
- Resting places to be provided every 200m (approx) see Code N6.

Code ON7: Residential Street Setback

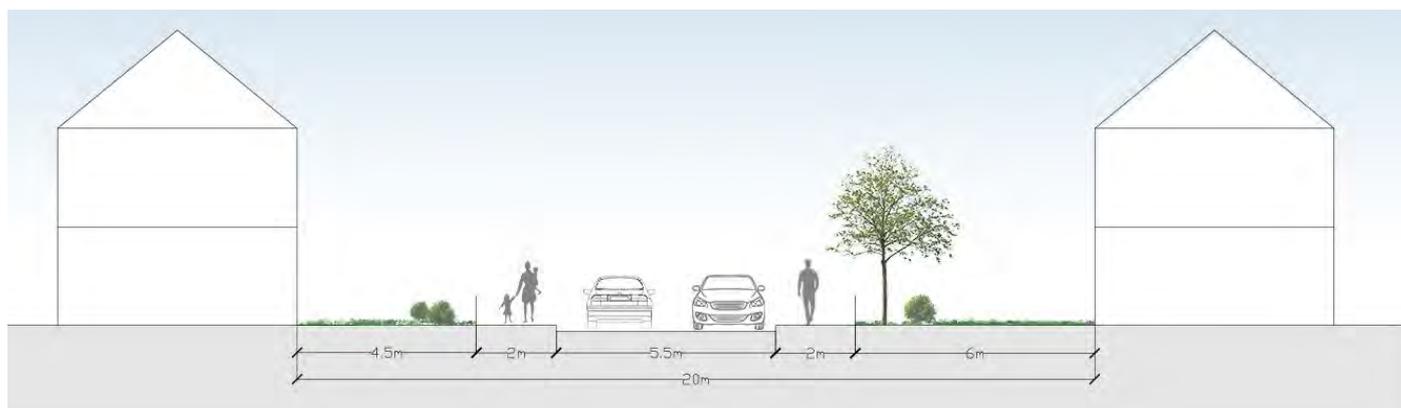
- Whilst allowing for the separation distances set out in HB1, a set back of between 3m and 6m must be provided from the pavement.
- Projections such as bay windows, porches and balconies are permitted.
- Variation: the front face of buildings must not vary more than 3m from the building line.

Residential Street Guidance

- Cycling should be provided for within the carriageway or as a 3m shared surface pedestrian / cycleway.
- Crossings should be located on or close to desire lines so that pedestrians find them convenient and pleasant to use.



Residential Street example layout



Residential Street cross-section

STREET NETWORK (ON)

Lanes

Lanes will consist of shared surface streets. The removal of a defined carriageway supports active travel and encourages the use of streets as social spaces.

Code ON8: Lanes

- Serve a Maximum of 25 units.
- Pedestrian movement has priority.
- Cul-de-sacs permitted.
- Provide a minimum carriageway width of 6m + 0.5m hardened maintenance margin.
- Include a 1.8m service corridor within the carriageway.
- Provide a change in surface material to distinguish the street type and signal to users that a change in behaviour is required.
- Pedestrian only connections must be provided between Lanes to ensure permeability. These can include modal filters (see modal filter diagram on pg51).

Code ON9: Lanes Landscaping and Planting

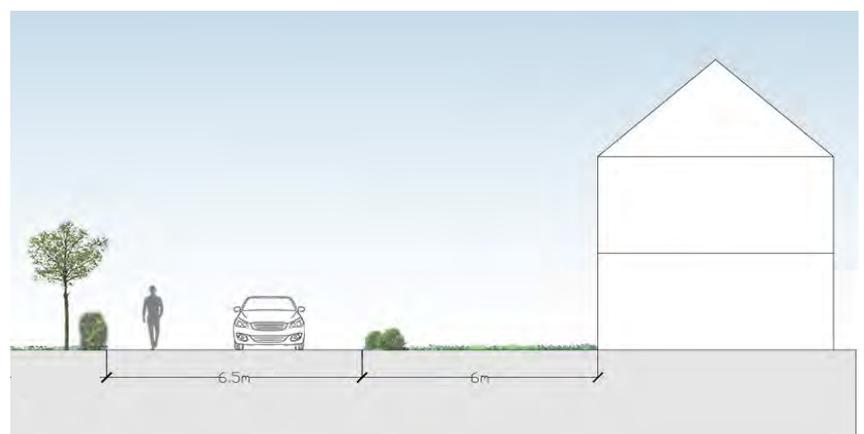
- To ensure the provision of street trees that are not within private ownership, landscaping pockets of a minimum of 50m² must be provided within Lanes. See Code N5.
- These pockets must form 'incidental open space' and not form part of the curtilage of any dwelling.

Code ON10: Lanes Setback

- Whilst allowing for the separation distances set out in HB1, a minimum set back of 2m to be provided from the carriageway.
- Projections such as bay windows, porches and balconies are permitted.
- Variation: the front face of buildings must not vary more than 3m from the building line.



Lanes example layout



Lanes cross-section

STREET NETWORK (ON)

Lanes Guidance

- Cycling should take place within the carriageway.
- The design of the street should ensure that parking is controlled and only taking place in designated areas.
- Tree planting should be utilised within the street to break up front of plot parking. This can be achieved through:
 - » Landscaping pockets or
 - » In landscaped areas between parking bays,
- Tree planting can be used as a means of traffic calming through diverting the carriageway.
- Note: coloured tarmac will be subject to a commuted sum for maintenance.



Modal filter - Ashmere Phase 1, Kent



Lanes connectivity - modal filters

STREET NETWORK (ON)

Private Drives

Provide connections for up to five properties and are in private ownership. The streets are encouraged as private social spaces for the properties they serve and have no through traffic.

Private Drive Guidance:

- Providing a change in surface material is encouraged to distinguish the street type whilst also enhancing the character of the street.

Code ON11: Private Drives

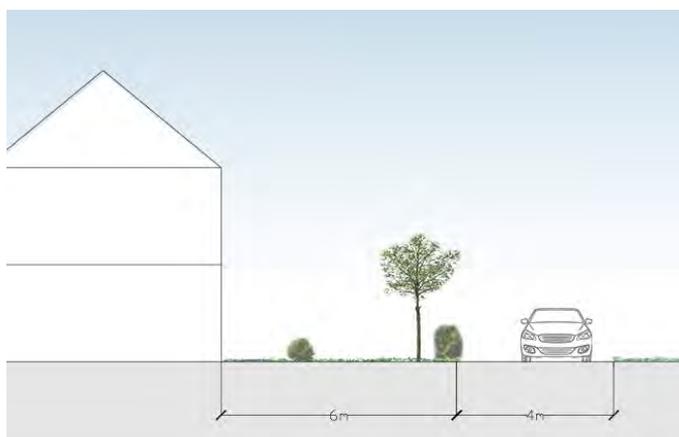
- Must serve no more than 5 properties.
- A maximum of 25m in length.
- Minimum of 3.7m in width.
- Bin collection points to be located a maximum of 25m from the adopted highway (See Code HB5).

Code ON12: Private Drive Landscaping

- Each property on a Private Drive must be provide with a landscaped front garden following the house type plot ratio in Code ON16.
- Tree planting must be provided within front gardens for at least 2 properties per Private Drive.

Code ON13: Private Drive Setback

- Whilst allowing for the separation distances set out in HB1, a minimum set back of 3m from the carriageway.
- Projections such as bay windows, porches and balconies are permitted.
- Variation: the front face of buildings must not vary more than 6m from the building line.



Private Drive cross-section

PARKING (ON)

You Said:

- » The design of parking in new housing could be better a
- » Inappropriate parking should be discouraged

Parking Provision

A balance is to be made within residential areas on the provision of car parking and the creation of attractive streets.

Parking within new residential developments must sit comfortably within the street scene and not detract from the overall visual aesthetic of the residential area. To ensure this is achieved within new residential developments the following codes must be adhered to.

Code ON14: Parking Provision Standards

- Development proposals must provide as a minimum the parking standards as set out below.

Unit Size	Number of spaces
1 bedroom	2
2 bedroom	2
3 bedroom	2
4 bedroom	3
5 bedroom+	3
Flatted development	1.5

- The above parking standards adhere to the Highways Design Guide & Specification – Residential and Industrial Estates Development (amended May 2021).

Parking Methods

There are a range of parking solutions which are considered appropriate for the Outer Neighbourhood, these include:

- In curtilage to the front of properties.
- In curtilage to the side of properties.
- Garage.
- On street allocated.
- On street unallocated.
- Courtyard.

The parking dimensions specified within the following codes align with the Highways Design Guide & Specification – Residential and Industrial Estates Development (amended May 2021).

Code ON15: Parking Methods

- A maximum of 60% of parking proposed can be to the front of properties. This includes in curtilage (driveway) and allocated on-street parking bays.

Parking to plot ratio

Car parking has a negative impact on the street scene when there is also a lack of landscaping and greenery within the street.

Code ON16: Parking to Plot Ratio

- The following minimum requirements of front gardens must be adhered to:
 - » Detached: A minimum of 35 % of the front of plot must be provided as landscaped garden.
 - » Semi detached: A minimum of 25% of the front of plot must be provided as landscaped garden.

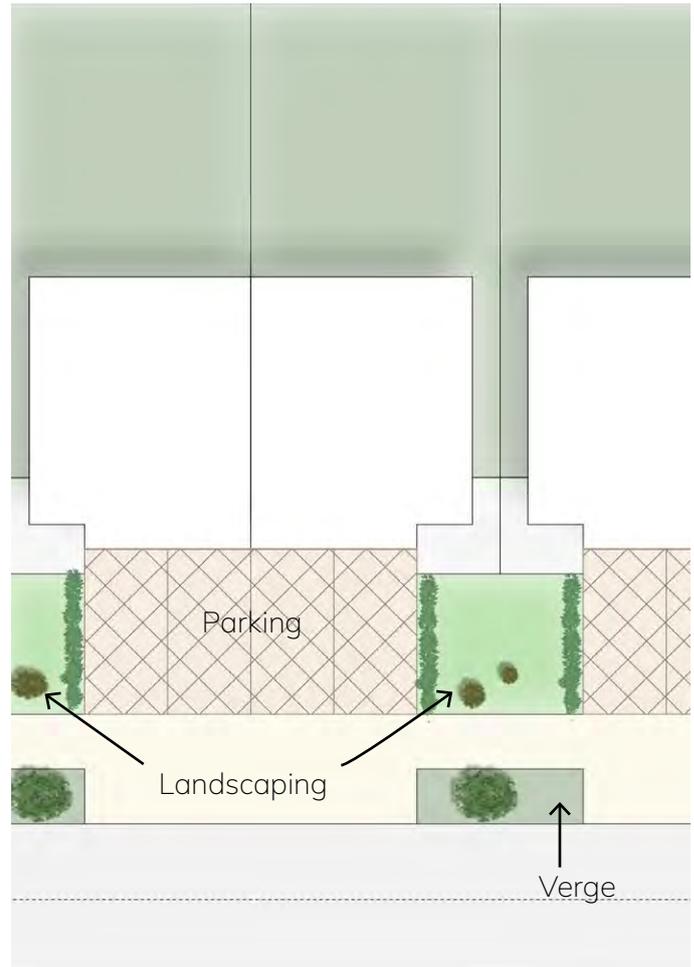
PARKING (ON)

In Curtilage - Front of Property

Providing parking in plot to the front or side of properties was considered the most popular during consultation. To ensure that parking does not dominate the street scene and the creation of green streets the following code and guidance are provided:

Code ON17: In Curtilage - Front of Property

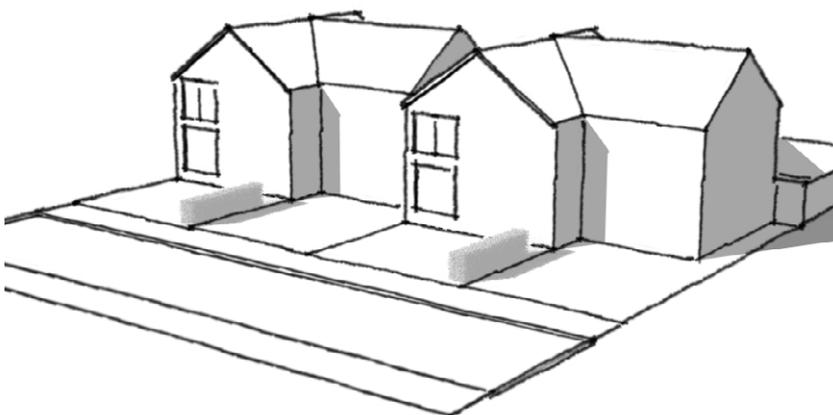
- No more than 4 consecutive front of plot spaces.
- Where more than 4 spaces are required an area of soft landscaping with tree and / or hedge planting measuring a minimum of 2m in length x 5m in width must be provided after every 4th space. This includes front gardens.
- Single space dimensions: 6m in length x 3m in width.
- Double space dimensions: 6m in length x 5.5m in width.
- Additional 0.6m to enable the movement of people and refuse facilities.
- A maximum of 60% of parking proposed can be to the front of properties (see Code ON15)



Front of plot parking and landscaping

Front of Property Guidance

- The inclusion of hedge planting to the side of parking bays is encouraged to act as screening.



Front of plot parking



Screening through planting

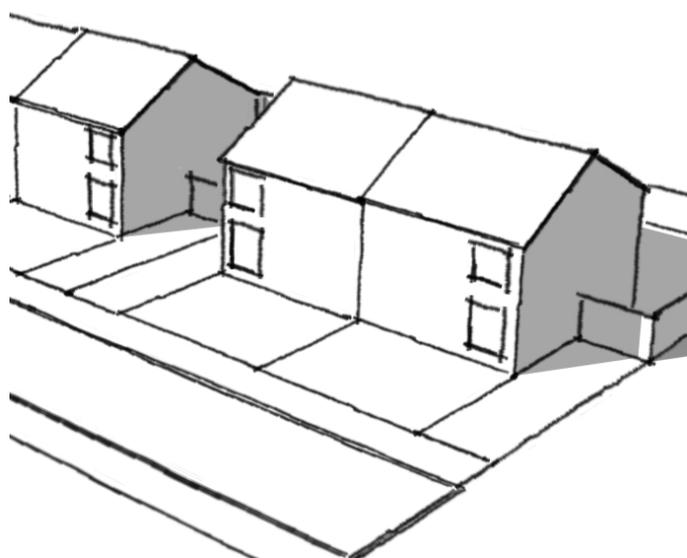
PARKING (ON)

In Curtilage - Side of Property

Providing parking to the side of properties enables the provision of front gardens and tree planting and will also reduce the dominance of parking on the street scene.

Code ON18: In Curtilage - Side of Property

- Single space dimensions: 6m in length x 3m in width.
- Double space dimensions: 11m in length x 3m in width.
- Additional 0.6m to enable the movement of people and refuse facilities, where applicable.



Side of plot parking

Garages

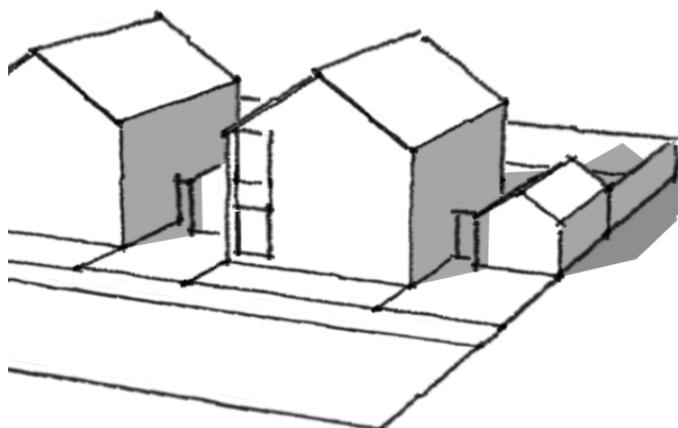
Public consultation indicated that garages are seen as a desirable form of parking in new residential developments. To ensure that they do not impact upon the street scene and encourage active frontages the following code and guidance are provided:

Code ON19: Garages

- To count toward parking provision garages must have minimum internal dimensions of 6m x 3m.
- Detached garages must be a minimum of 2m behind the building line of the associated residential dwelling.
- No more than 2 consecutive garages. This applies to both integrated and detached.

Garages Guidance:

- Integrated garages can impact upon the street scene through increasing the levels of inactive elevations. It is therefore important to ensure that the level of integrated garages does not dominate the street scene.
- Integrated garages should be recessed with the residential aspect of the dwelling being the primary focus of elevations.



Detached garage

PARKING (ON)

Rear Parking

Parking to the rear of buildings enables the removal of cars from the street scene and the provision of landscaping however concerns are raised over their security and safety where natural surveillance is not possible. Rear parking is not considered an appropriate parking solution and should only be used when the following code can be met in full.

Code ON20: Rear Parking

- Where parking is located to the rear within a proposal it must:
 - » Be subject to a level of ground floor activation from the surrounding built form.
 - » Incorporate an element of permeability to boundary treatments e.g. brick wall and open boarded timber combination.
 - » Provide a change in surface material to distinguish between public and private space.
 - » Include landscaping with tree or hedge planting at least 2m in width and 4.8m in length after every 4th space.
 - » Ensure rear access to properties is maintained.

Rear Parking Guidance

- Rear parking will only be considered in circumstances where a level of ground floor activation is provided from neighbouring properties. i.e. where a property on an adjacent street provides frontage.
- Rear parking should be used sparingly within proposals.

Courtyard Parking

The provision of courtyard parking to the front of properties can be a useful parking method in providing allocated on-street parking in a positive setting.

It is important to ensure that courtyards are designed appropriately and must be in the form of small courtyards with landscaping, feature paving and street furniture.

Code ON21: Courtyard Parking

- Proposals for courtyard parking must incorporate the following:
 - » Safe pedestrians and cycle movement.
 - » Natural surveillance from adjoining houses, or by buildings entered from the parking area.
 - » Landscaping as either; a central space with parking around or, as landscaped areas measuring 1m in width after every 4th space and include tree / hedge planting.
 - » Central landscaped areas should include planting and be designed as social spaces.
 - » Provide a careful balance between the desire of car owners to park as near to their homes as possible and the need to maintain the character of the overall setting.
 - » Prevent inappropriate parking through careful placement of street furniture / planting.



Courtyard Parking - Hatlepool

PARKING (ON)

On-Street Parking

Providing parking within the street can take the form of either allocated or unallocated space.

Code ON22: On Street Allocated & Unallocated

- Proposals for on-street parking must:
 - » Be 2.4m in width x 6m in length, per space.
 - » Provide a maximum of 4 consecutive spaces.
 - » Provide an area of soft landscaping with tree planting measuring a minimum of 2.4m in width x 6m in length after every 4th space. Where more than 4 spaces are provided.
 - » Clearly define the space as private for allocated bays.

Terraced properties parking

Where terraced properties are provided with front of plot parking there is a negative impact on the street scene and a reduction in the provision of front gardens.

Code ON23: Terraced Properties parking

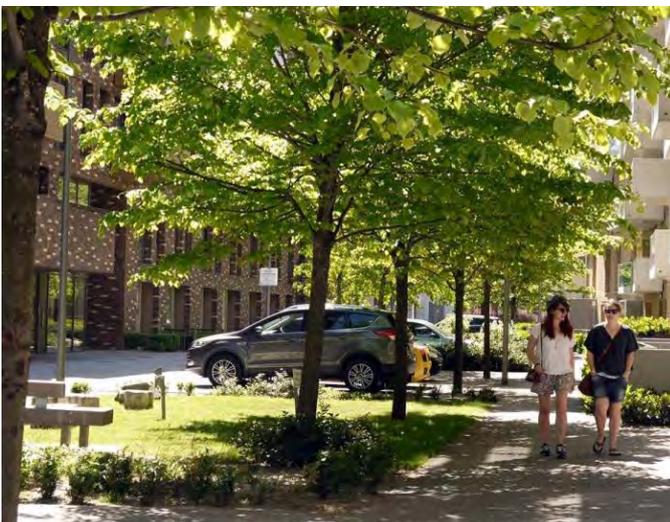
- Parking for short runs of terraced properties (up to 4 units) must consist of:
 - » End properties provided with side of plot parking and front gardens.
 - » Central unit's provided with front of plot parking (see image below).
- Parking for more than 4 terraced properties must:
 - » Take the form of either courtyard parking and / or on street parking.

Parking Materials

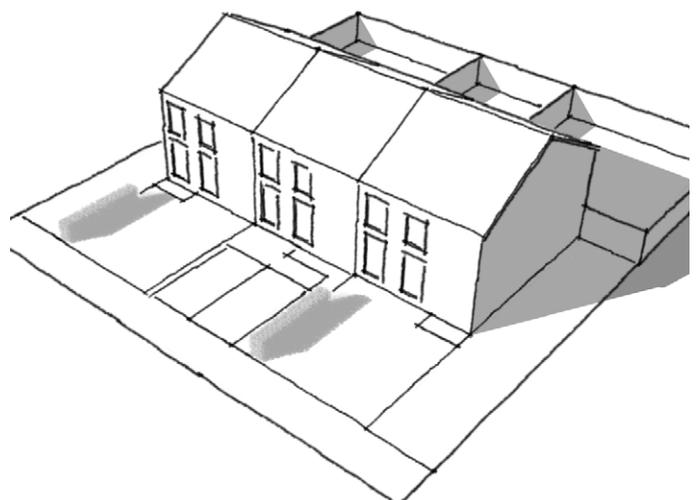
The type of materials used for parking can impact upon the street scene and should be used to define to change from public to private space

Materials Guidance

- Parking within the curtilage of the dwellings should consist of the following:
 - » Permeable paving.
 - » Tarmac.
 - » Blocks.
- Proposals should provide a different surface material to that proposed on footways and roads to clearly distinguished between public and private space.



On-street parking with landscaping - St Andrews



Terraced parking (short runs)

BUILT FORM DETAIL (ON)

Urban Form

Development should take the form of perimeter block development with housing facing onto streets/public realm and gardens facing inward creating a clear distinction between public and private space.

Perimeter blocks can be provided in either formal or informal styles.

- Formal perimeter blocks consist of regular rectangular blocks with consistent building lines and street geometry.
- Informal perimeter blocks are irregular in shape with variations in building lines and curved street geometry.

Urban Form Guidance:

- Formal perimeter blocks should be used for development on Neighbourhood Streets, and near local centres or community facilities, where provided.
- A mix of formal and informal perimeter blocks should be used for development on Residential Streets, Lanes and Private Drives.
- Where development blocks are adjacent to areas of public open space or are located on the rural edge informal perimeter blocks are encouraged to create a soft interface between urban and rural.

You Said:

- » Cul-De-Sacs are disorientating
- » Being able to navigate streets is important
- » Narrow alleyways raise concerns over safety.

Cul-De-Sacs

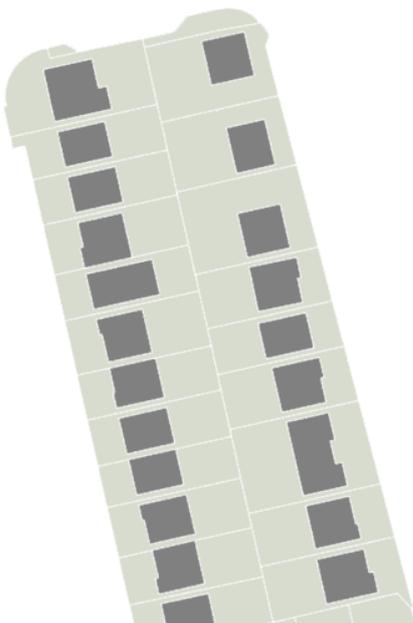
Proposals which are based upon an interconnected series of Cul-De-Sacs can reduce the overall permeability of areas, increase walking distances and cause disorientation for users.

Code ON24: Cul-De-Sacs

- Only permitted on Lanes and Private Drives.
- Pedestrian permeability must be maintained e.g using modal filters (see pg53).

Cul-De-Sac Guidance:

- Cul-De-Sacs within new residential development should only be considered where permeability and wayfinding are maintained for users.



Formal perimeter block



Informal perimeter block

BUILT FORM DETAIL (ON)

Building Line

The shape of the building line will contribute to the character of the area, The Outer Neighbourhood Area Type is characterised by a mix of curved and straight building lines depending on the era of construction and the position within the development.

Frontage Guidance:

- A mix of curved and straight building lines should be created.
- Curved building lines should be on Residential Streets and Lanes with straight building lines being focused on Neighbourhood Streets.
- The building line for Private Drives will be dependant on their location within the development. Close to the site entrance and Neighbourhood Streets should be more formal in nature. Curved building lines should be provided for Private Drives on the rural edge or adjacent to green spaces.
- Houses with side parking can be placed closer to the highway however this should not be at the detriment of front gardens, tree planting and residential amenity.

Building Heights

Building heights are predominantly 2 storey within the Outer Neighbourhood Character Area. Changes in building heights can add interest and variety to street scenes. This can be achieved by changes in roof pitch or through changes in number of storeys provided.



Changes in scale -Alkerden Gateway, Kent

Building Heights Guidance:

- The following storey heights should be achieved:
 - » Neighbourhood Street - up to 3 storey.
 - » Residential Street - 2 storey up to 3 storey at key locations.
 - » Lanes - 2 storey up to 2.5 storey at key locations.
 - » Private Drives - 2 storey up to 3 storey at key locations.
 - » Rural edge - 2 storey.
- Key locations include:
 - » Entrances.
 - » Corner locations.
 - » View terminus points.
 - » Focal points.
 - » Neighbouring community facilities.

Housing Mix

The analysis of the Outer Neighbourhood Area Type highlighted the predominance of detached and semi-detached properties. Instances of terraced properties and apartments are also found within the Area Type, these are focused on primary streets or adjacent to local centres/facilities where increases in density are considered appropriate.

Housing Mix Guidance:

- Any new residential development within the Outer Neighbourhoods should comprise:
- Predominately of detached and semi-detached properties.
- Include terraced properties (short runs of up to 4 units) and apartments. These will be considered appropriate on Neighbourhood and Residential Streets, neighbouring key facilities such as a local centre/school and at site entrances.
- Where access alleys are required for the rear of terraced properties they should:
 - » Not negatively impact upon the rear garden sizes of adjoining properties.
 - » Be clearly defined as private space.
 - » Provide gates at their entrances.

BUILT FORM DETAIL (ON)

Gateways / Entrance Features

Proposals for new residential development within the Outer Neighbourhood Area Type should include gateways which create a clear point of arrival and strengthens the sense of place. This is of particular importance for large scale developments over 200 units.

Code ON25: Gateways

- Where gateways are proposed they must include:
 - » Landmark buildings (see Code BF3).
 - » Landscaping and tree planting.
 - » Entrance features such as feature walls or public artwork.

Materials & Elevational Detailing

It is not the intention of this code to be prescriptive in terms of setting materiality or elevational detailing of proposals or stifle innovative designs.

Materials and Elevational Detailing Guidance

- In accordance with Code BF1, any proposals should have identified the key features of the existing built form, which have a positive influence on the character of the Outer Neighbourhood Area Type.

- Proposals should create their own unique character which reflects any features identified through character analysis.
- Key elevational details identified as part of the analysis for this code include the following details:
 - » Mini gables.
 - » Gabled front elevations.
 - » Head and cill detailing.
 - » Hipped roofs.
 - » Chimneys.
 - » Dormer windows.
 - » Bay windows.
 - » Canopies.
 - » Porches.
- Large scale proposals over 200 units should consider using character areas to distinguish between the different areas within proposals.
- The below images are provided as examples of potential elevational treatment and materials in the Outer Neighbourhood Area Type.



BUILT FORM DETAIL (ON)

Boundary Treatment

Boundary treatments must be used to clearly define changes from public to private space as well as enhancing the character and appearance of the area.

Code ON26: Boundary Treatment

- Boundary treatment to the front of plots must not exceed 1.2m. Ensuring that a level of natural surveillance from the associated property can be provided over the street.
- When proposing boundary treatments to the front of plots, consideration must be given to visibility of vehicles entering and existing the plot.
- Plots that adjoin areas of public open space must clearly distinguish between public and private space through the provision of a boundary treatment of either:
 - » Railings.
 - » Knee rail.
 - » Hedging.

Boundary Treatment Guidance:

- Changes in the type of boundary treatment used on different street typologies is encouraged.
- Boundary treatments should not reduce the overall permeability of the development or reduce the level of openness of the street (see image below).
- Appropriate boundary treatments for Outer Neighbourhood include:
 - » Metal railings.
 - » Hedge.
 - » Low brick wall (brick to compliment the primary material of the residential building).
 - » Brick and metal railing combination.
 - » Knee rail.
- For the established street types the following boundary treatments should be used, where required:
 - » Neighbourhood Street - metal railing, low brick wall, brick and metal railing combination.
 - » Residential Street - metal railing, low brick wall, hedge.
 - » Lanes - metal railing, low brick wall, hedge.
 - » Private Drive - hedge.
 - » Rural edge - non encouraged, hedge if required to define private space.

URBAN EDGE (UE)

Within the Outer Neighbourhood Area Type development will adjoin the urban-rural edge of the borough. It is therefore important to ensure that future residential developments provide an appropriate response, which enables developments to graduate into the rural landscape and respond to potential views.

Code UE1: Street Types

- Street typologies must include Lanes and Private Drives.

Code UE2: Parking

- The appropriate parking solutions for the Urban Edge include:
 - » Front of plot see Code ON17 with a front garden following the ratio's set within Code ON16.
 - » Side of plot Code see Code ON18.
 - » Garages see Code ON19.

Code UE3: Connections

- To encourage active travel and promote health and well-being, connections by modes of active travel must be provided into the countryside, where available.
- Pedestrian and cycle connections must connect into existing Public Rights of Way or Permissive Routes in accordance with Codes M4 - M7.

Code UE4: Frontage

- Properties must provide frontage onto the

Urban Edge.

- Where side elevations are proposed these must be corner turning properties with dual frontages (see Area Wide Code BF2).
- Presenting rear elevations and boundary fencing onto the Urban Edge will not be considered an appropriate response.

Code UE5: Amenity Space

- Landscaped front gardens with tree planting must be provided for all properties fronting onto the urban edge.

Urban Edge Guidance:

- Where Private Drives are proposed on the Urban Edge these should not reduce the overall permeability of proposals. Where the predominant use of Private Drives prevents the movement of users, pedestrian connections to an adoptable standard should be placed within a verge or adjacent green space.
- Changes in surface material of streets should be provided to lessen vehicle speeds.
- Informal development blocks are encouraged on the urban edge to create a soft interface between urban and rural.
- To optimise the integration of the built form into the countryside boundary treatments are not encouraged. Where a clear distinction between public and private space is required a low-level hedge should be provided.



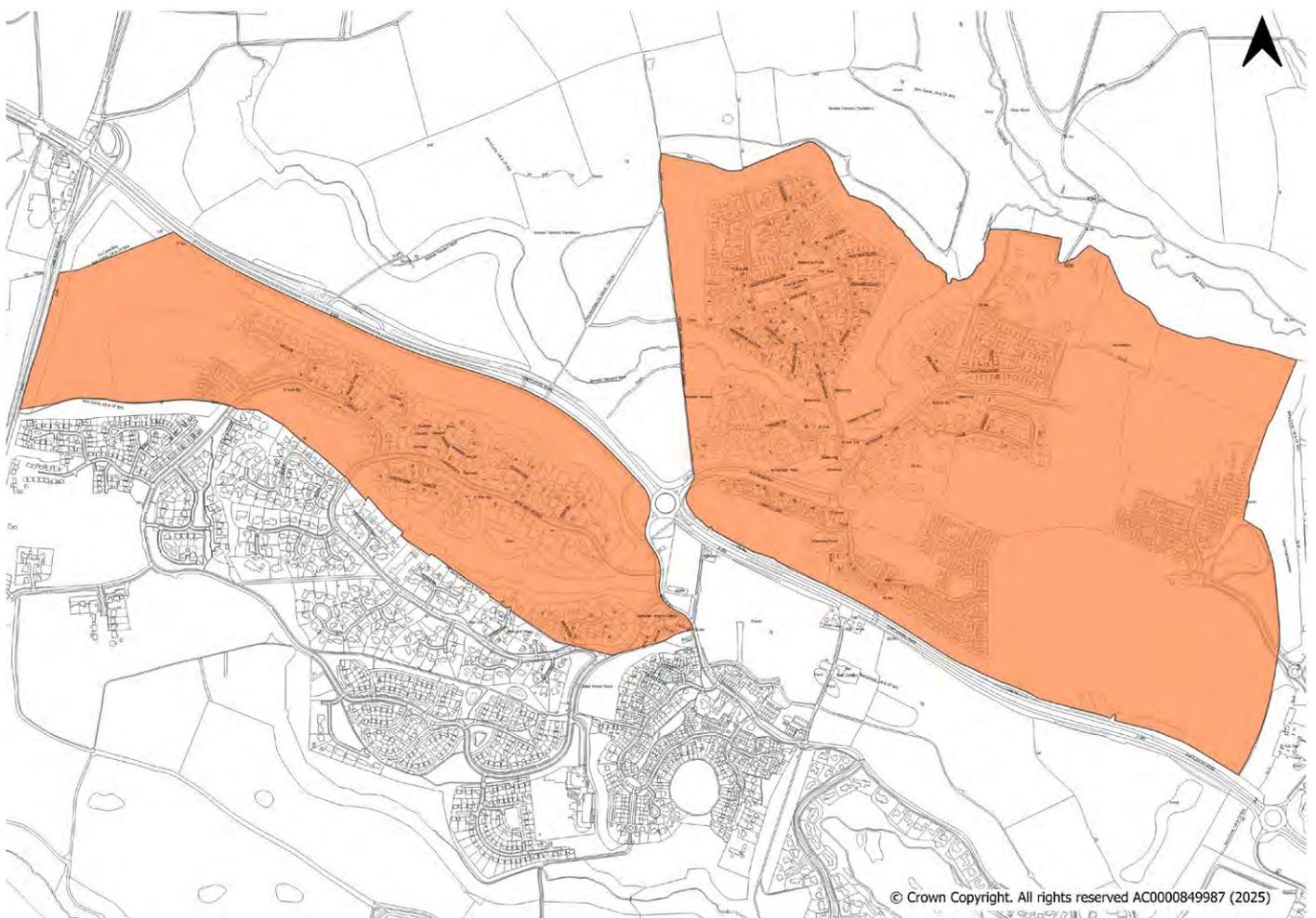
Treatment onto Urban Edge - CP2a, Newhall

OUTER NEIGHBOURHOOD MATRIX

Street type	Neighbourhood Street	Residential Street	Lanes	Private Drives
Verge	2m on one side of carriageway	Non Mandatory	Non Mandatory	Non Mandatory
Landscaping	Planting and trees within verge.	Landscaping pockets min.50m ²	Landscaping pockets min.50m ²	Front gardens
Street Trees	Yes in verge	Yes in landscaping pockets	Yes in landscaping pockets	Front gardens, minimum of 2 trees per street.
Minimum Carriageway Width	5.5 - 6.75m	5.5m	6.5m	3.7m minimum.
Minimum Footway Width	2m (both sides of carriageway)	2m (both sides of carriageway)	Non defined in carriageway	N/A
Resting Places	Every 200m (approx)	Every 200m (approx)	In landscape pockets	No
Cycleway	Yes. 2m segregated or 3m shared.	Non defined in carriageway or 3m shared.	Non defined in carriageway	No
Block Style	Formal	Mix	Mix	Mix
Building Line	Straight	Curved	Curved	Mix
Setback	2 - 6m	3 - 6m	2m minimum	2m minimum
Variation	3m	3m	3m	6m
Cul-de-sacs Permitted?	No	No	Yes	Yes
Permitted on Urban Edge?	No	No	Yes	Yes
House Types	Detached Semi- detached Apartments Terraces	Detached Semi- detached Terraces	Detached Semi- detached	Detached Semi- detached
Boundary Treatment	Metal railing Low brick wall Brick wall and railing combination None	Metal railing Low brick wall Hedge None	Metal railing Low brick wall Hedge None	Hedge None
Building Heights	Up to 3 storey	2 storey up to 3 at key locations	2 storey up to 2.5 at key locations	2 storey up to 3 storey at key locations

Wynyard Area Type

WYNYARD CODING PLAN



WYNYARD AREA TYPE (WY)

The Wynyard Area Type covers the Hartlepool borough area of Wynyard. Wynyard is allocated as a housing growth area within the Local Plan (2019) and is located on the south western edge of the borough. Wynyard is characterised by modern family homes with a woodland backdrop.

The adopted Wynyard Masterplan (2019) provides a framework for the development of the Wynyard area. Subsequent planning applications have superseded many of the elements of the masterplan. The codes and guidance set out within this SPD are intended to supplement the Place Making Design Principles within the adopted Masterplan.

Wynyard Summary of Analysis.

Streets and Movement

- A wide boulevard with a grassed verge either side of the carriageway characterises the main street both north and south of the A689.
- Residential streets leading from the main boulevard into development parcels provide hierarchy of streets.
- Internal streets are predominately cul-de-sacs.
- Street trees are provided in some instances, e.g. The Pentagon and Siskin Park, the remainder of areas rely on trees within front gardens
- Streets with lower traffic levels have a greater sense of enclosure.
- Tighter building lines and set backs in more

recent development.

- Streets are curved in nature.
- Combination of front of plot and side of plot parking.
- Gated communities with reduced permeability.
- Network of walking routes providing connections provided into surrounding countryside and PRoW network.

Built Form

- Properties are predominately large detached houses, more recent developments have introduced semi-detached properties and apartments.
- Properties are between 2 and 2.5 storey in nature with instances of 1 and 3 storey properties.
- Properties are provided with a set back of between 3 and 6m from the street.
- Building lines are consistent in more recent developments with limited variation. The provision of curved streets creates an informal feel.
- Properties provided with front gardens and on plot parking.

Materials

- Red brick.
- Buff brick.
- Multi brick.
- Render - white or cream.
- Tile hangings.



The Pentagon - Street with verge and street trees



Large detached homes

WYNYARD AREA TYPE (WY)

Architectural Detailing

- There is a mix of modern and traditional architectural styles.
- Custom built housing provides an eclectic mix of styles and detailing in some areas.
- Clearly defined different phases of development through changes in architectural styles.
- Some instances of repetitive house types with limited variation in places.
- Common features include:
 - » Mini gables.
 - » Gabled front elevations.
 - » Head and cill detailing.
 - » Chimneys.
 - » Brick detailing.
 - » Window surrounds.
 - » Glazed panels.
 - » Dormer windows.
 - » Bay windows.
 - » Elongated contemporary windows.
 - » Canopies.
 - » Porches.
 - » Balconies.

Boundary Treatment

- Metal railings.
- Brick pillars.
- Low brick wall.
- Open front gardens.

Green Space / Landscaping

- Large green spaces with formal planting and play spaces.
- Incremental areas of landscaping and landscaped front gardens create a 'green' feel to areas.
- Verges provided on Boulevard and Neighbourhood Streets with instances of tree planting.

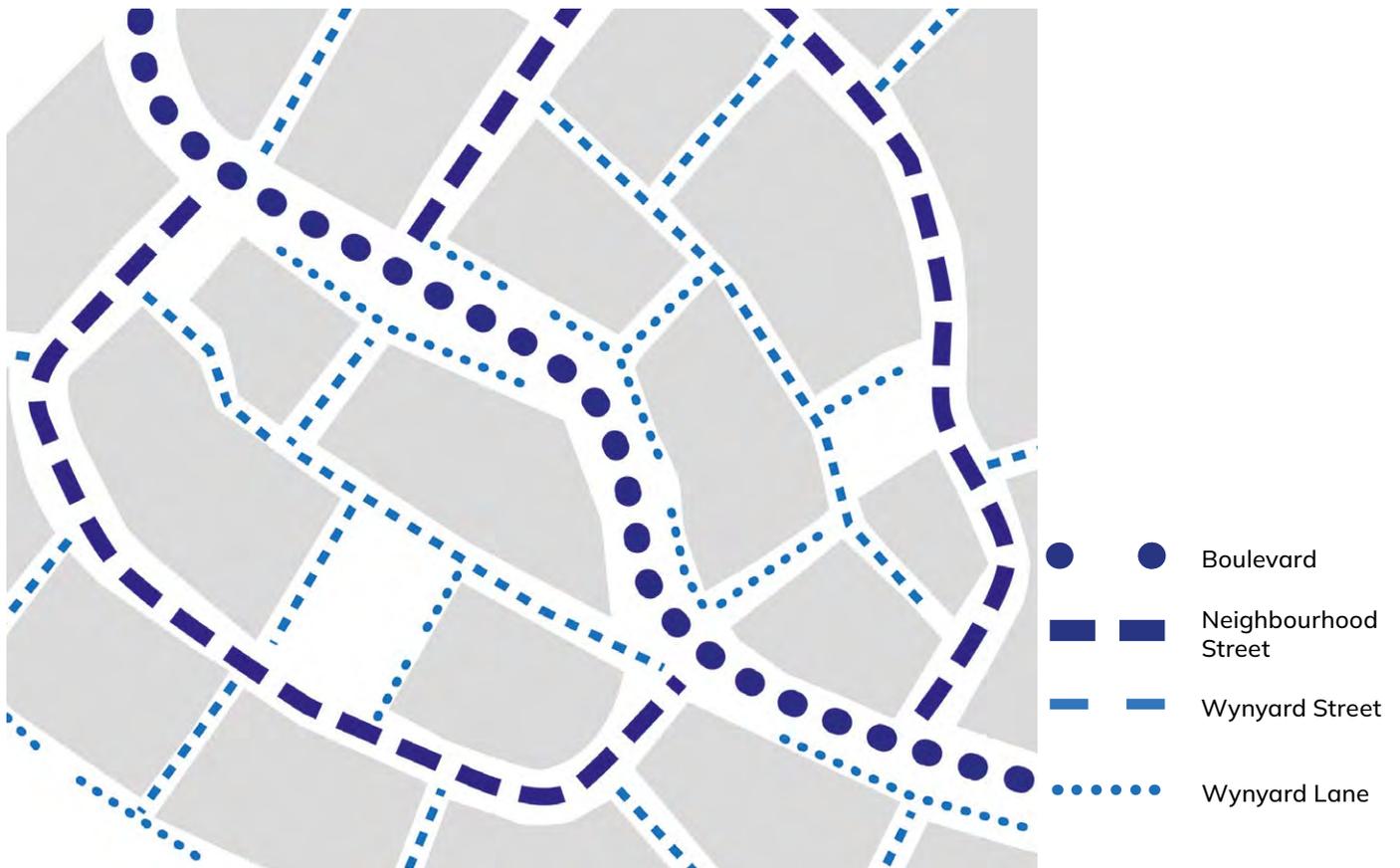
Facilities

Facilities are currently limited to the south of Wynyard within Stockton Borough Council. A local centre will be provided to the north of the A689.



STREET NETWORK (WY)

The street network within Wynyard has been established through the existing and forthcoming residential areas however, there are inconsistencies between the street types between the different development areas. The following codes have been produced to ensure a coherent street network is provided going forward and to focus on the creation of streets, which are well connected, attractive and usable as social spaces.



Example street network



Boulevard -Seaham Garden Village, Durham



Sared surface street- Alkerden Village, Kent

STREET NETWORK (WY)

Boulevard

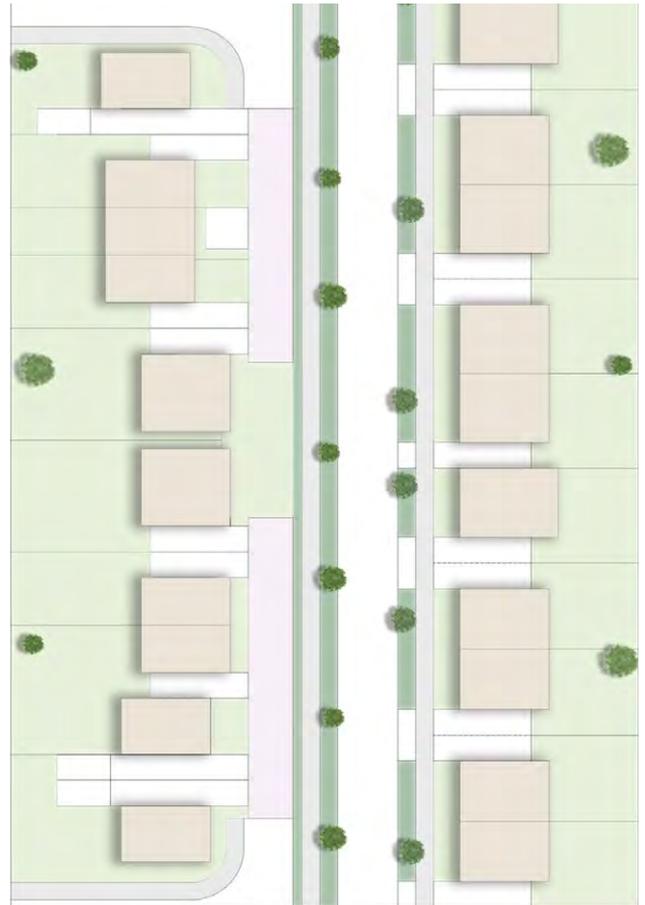
The Boulevard provides the main pedestrian, cycle, public transport and vehicle connections through the Wynyard area, these streets will be the backbone of the street network connecting onto the A689, between development parcels and to facilities including local centres, schools and open spaces. Within the Wynyard Masterplan these are identified as Primary Arterial Streets.

Code WY1: Boulevard Verge

- Minimum of 2m verge must be provided on both sides of the carriageway.
- Verge must incorporate street tree planting (see Code N10).
- Resting places must be provided every 200m (approx) see Code N6.
- Where visitor parking is provided their location within the verge area will be considered acceptable as long as 70% of the total street length is verge.
- Barriers must not be provided alongside verges or footways to enable permeability and reduce visual clutter.

Code WY2: Boulevard Carriageway

- Provide a carriageway width of 6.75m to enable public transport provision.
- Cul-de-sacs are not permitted.



Example of Boulevard with verge



Boulevard cross-section

STREET NETWORK (WY)

Code WY3: Boulevard Footway & cycleway

- Footway must be provided on both sides of the carriageway with a minimum width of 2m.
- Footways to provide direct connections to:
 - » Development parcels.
 - » Existing active travel routes.
 - » Future active travel routes in accordance with the Wynyard Park Woodland Footpath Strategy.
 - » Key facilities identified in the site analysis including the local centre and school (see Code SA1).
- Cycling provision to be included as either a 3m shared pedestrian/cycleway or a segregated 2m cycleway.
- Cycle links to connect into existing off-road cycle routes.

Code WY4: Boulevard frontage

- Frontage must be provided onto the Boulevard, presenting rear boundaries will not be considered acceptable.

Code WY5: Boulevard Setback

- Whilst allowing for the separation distances set out in HB1, a set back of between 2m and 6m must be provided from the pavement.
- Projections such as bay windows, porches and balconies are permitted.
- Variation: the front face of buildings must not vary more than 2m from the building line.

Boulevard Guidance

- Crossings should be located on or close to desire lines so that pedestrians find them convenient and pleasant to use.
- Drainage features such as swales are encouraged in verges (see Code N11).

STREET NETWORK (WY)

Wynyard Neighbourhood Street

The aim of the Neighbourhood Street is to provide the main pedestrian, cycle, public transport and vehicle connections through a residential area, these routes can include connections between development parcels and local centres, schools and open spaces. Neighbourhood Streets will connect into the existing movement network surrounding the area and provide through routes.

Code WY6: Wynyard Neighbourhood Street Verge

- A 2m verge must be provided on both sides of the carriageway.
- The verge must be provided for at least 70% of the total street length taking into consideration driveway crossings. e.g. for a Neighbourhood Street 100m in length the verge must be provided for at least 70m (see opposite).
- Verge must incorporate street tree planting (see Code N10).
- Resting places must be provided every 200m (approx), see Code N6.
- Where visitor parking is provided their location within the verge is considered acceptable as long as 70% of the total street length is verge.
- Barriers must not be provided alongside verges or footways to enable permeability and reduce visual clutter.



Example of Wynyard Neighbourhood Street



Neighbourhood Street cross-section

STREET NETWORK (WY)

Code WY7: Wynyard Neighbourhood Street Carriageway

- Provide a carriageway width of 5.5m.
- Cul-de-sacs are not permitted.

Code WY8: Wynyard Neighbourhood Street Footway & cycleway

- A minimum footway width of 2m must be provided on both sides of the carriageway.
- Cycle links to connect into nearby existing off-road cycle routes, where available.

Code WY9: Wynyard Neighbourhood Street Setback

- Whilst allowing for the separation distances set out in HB1, a set back of between 2m and 6m must be provided from the pavement.
- Projections such as bay windows, porches and balconies are permitted.
- Variation: the front face of buildings must not vary more than 2m from the building line.

Wynyard Neighbourhood Street Guidance

- The route of Neighbourhood Streets should follow desire lines identified in the context analysis of the site.
- Crossings should be located on or close to desire lines so that pedestrians find them convenient and pleasant to use.
- Drainage features such as swales are encouraged in verges (see Code N11)



Planting within street - The Nightingales, Andover



Existing Neighbourhood Street - Wynyard

STREET NETWORK (WY)

Wynyard Street

Wynyard Streets mainly carry local traffic and provide connections through development parcels. These streets typically see lower traffic flow and support active travel and encourage the use of streets as social spaces.

Code WY10: Wynyard Street Carriageway

- Provide a carriageway width of 5.5m.
- Cul-de-sacs are not permitted.
- A minimum footway width of 2m must be provided on both sides of the carriageway.

Code WY11: Wynyard Street Landscaping & Planting

- To ensure the creation of green streets throughout new residential developments and the provision of street trees, which are not within private ownership, landscaping pockets of a minimum of 50m² must be provided within Residential Streets. See Code N5.
- These pockets must form 'incidental open space' and not form part of the curtilage of any dwelling.
- Resting places to be provided every 200m (approx) see Code N6.

Code WY12: Wynyard Street Setback

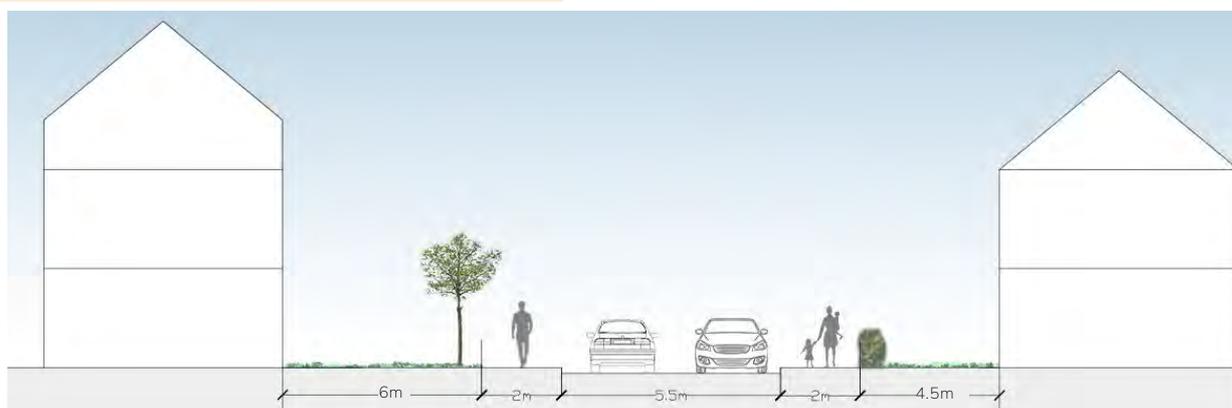
- Whilst allowing for the separation distances set out in HB1, a minimum set back of 2m must be provided from the pavement.
- Projections such as bay windows, porches and balconies are permitted.
- Variation: the front face of buildings must not vary more than 3m from the building line.

Wynyard Street Guidance

- Crossings should be located on or close to desire lines so that pedestrians find them convenient and pleasant to use.



Example of Wynyard Street



Wynyard Street cross-section

STREET NETWORK (WY)

Wynyard Lanes

Lanes will consist of a shared surface street. The removal of a defined carriageway supports active travel and encourages the use of streets as social spaces.

Code WY13: Wynyard Lanes Carriageway

- Serve a Maximum of 25 units.
- Pedestrian movement has priority.
- Cul-de-sacs permitted.
- Provide a minimum carriageway width of 6m + 0.5m hardened maintenance margin.
- Include a 1.8m service corridor within the carriageway.
- Provide a change in surface material to distinguish the street type and signal to users that a change in behaviour is required.
- Pedestrian only connections must be provided between Wynyard Lanes to ensure permeability. These can include modal filters.

Code WY14: Wynyard Lanes Landscaping and Planting

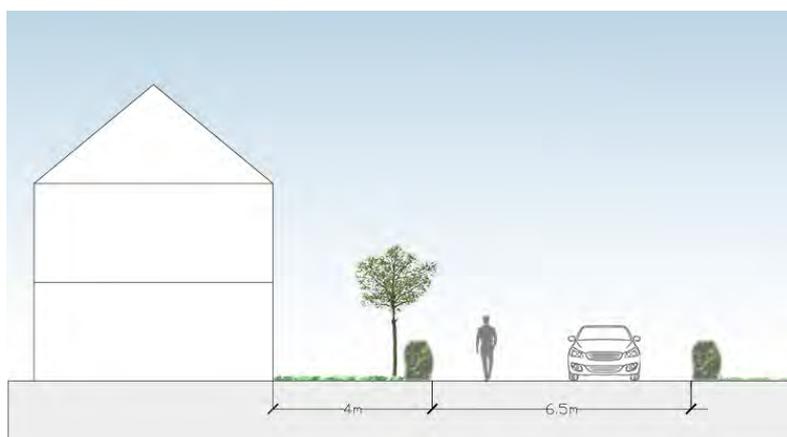
- To ensure the provision of street trees that are not within private ownership, landscaping pockets of a minimum of 50m² must be provided within Lanes. See Code N5.
- These pockets must form 'incidental open space' and not form part of the curtilage of any dwelling.

Code WY15: Wynard Lanes Setback

- Whilst allowing for the separation distances set out in HB1, a minimum set back of 2m must be provided from the carriageway.
- Projections such as bay windows, porches and balconies are permitted.
- Variation: the front face of buildings must not vary more than 3m from the building line.

Wynyard Lanes Guidance

- Cycling on Lanes should be able to take place within the carriageway.
- The design of the street should ensure that parking is clear, legible and only taking place in designated areas.
- Visitor parking should be clearly marked.
- Note, coloured tarmac will be subject to a commuted sum for maintenance.
- Tree planting should be utilised within the street to break up front of plot parking. This can be achieved through:
 - » Landscaping pockets or
 - » In landscaped areas between parking bays.
- Tree planting can be used as a means of traffic calming through diverting the carriageway.



Wynyard Lanes cross-section

STREET NETWORK (WY)

Wynyard Private Drives

Private Drives provide connections for a small group of properties and are in private ownership. The streets are encouraged as private social spaces for the properties they serve and have no through traffic.

Code WY16: Wynyard Private Drives

- Must serve no more than 5 properties.
- A maximum of 25m in length.
- Minimum of 3.7m in width.
- Bin collection points to be located a maximum of 25m from the adopted highway.
- Cul-de-sacs permitted

Code WY17: Wynyard Private Drive Landscaping

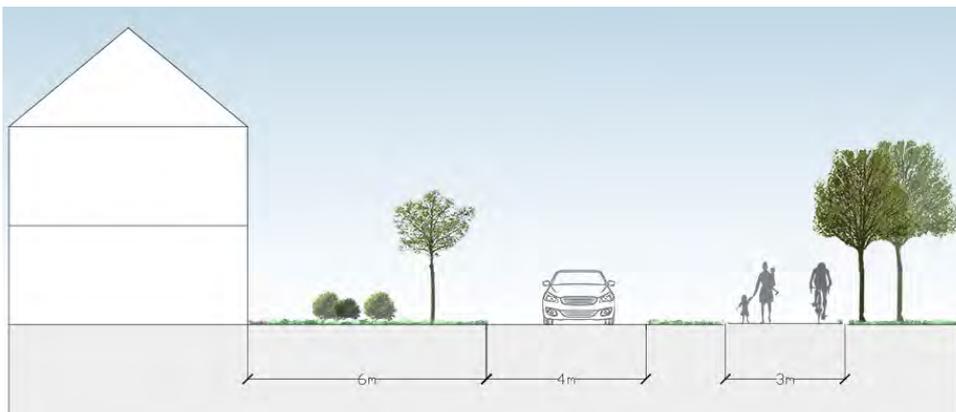
- Each property on a Wynyard Private Drive must be provide with a landscaped front garden following the house type plot ratio in Code WY21.
- Tree planting must be provided within front gardens for at least 2 properties per Private Drive.

Code WY18: Wynyard Private Drive Setback

- Whilst allowing for the separation distances set out in HB1, a minimum set back of 2m must be provided from the pavement.
- Projections such as bay windows, porches and balconies are permitted.
- Variation: the front face of buildings must not vary more than 6m from the building line.

Wynyard Private Drive Guidance:

- Providing a change in surface material is encouraged to distinguish the street type whilst also enhancing the character of the street.
- Cul-de-Sacs within new residential development will only be considered where permeability and way finding are maintained for users.



Wynyard Private Drive onto Green Wedge cross-section

PARKING (WY)

You Said:

- » The design of parking in new housing could be better and inappropriate parking should be discouraged.

Parking Provision

A balance is to be made within residential areas on the provision of car parking and the creation of attractive streets.

Parking within new residential developments must sit comfortably within the street scene and not detract from the overall visual aesthetic of the residential area. To ensure this is achieved within new residential developments the following codes must be adhered to.

Code WY19: Parking Provision Standards

- Development proposals must provide as a minimum the parking standards as set out below.

Unit Size	Number of spaces
1 bedroom	2
2 bedroom	2
3 bedroom	2
4 bedroom	3
5 bedroom+	3
Flatted development	1.5

- The above parking standards adhere to the Highways Design Guide & Specification – Residential and Industrial Estates Development (amended May 2021).

Parking Methods

There are a range of parking solutions which are considered appropriate for Wynyard, these include:

- In curtilage to the front of properties.
- In curtilage to the side of properties.
- Garage.
- On street allocated.
- On street unallocated.
- Courtyard.

The parking dimensions specified within the following codes align with the Highways Design Guide & Specification – Residential and Industrial Estates Development (amended May 2021).

Code WY20: Parking Methods

- A maximum of 60% of parking proposed can be to the front of properties. This includes in curtilage (driveway) and allocated on-street parking bays.

Parking to plot ratio

Car parking has a negative impact on the street scene when there is also a lack of landscaping and greenery within the street.

Code WY21: Parking to Plot Ratio

- The following minimum requirements must be adhered to:
 - » Detached: A minimum of 35 % of the front of the plot must be provided as landscaped garden.
 - » Semi detached: A minimum of 25% of the front of the plot must be provided as landscaped garden.

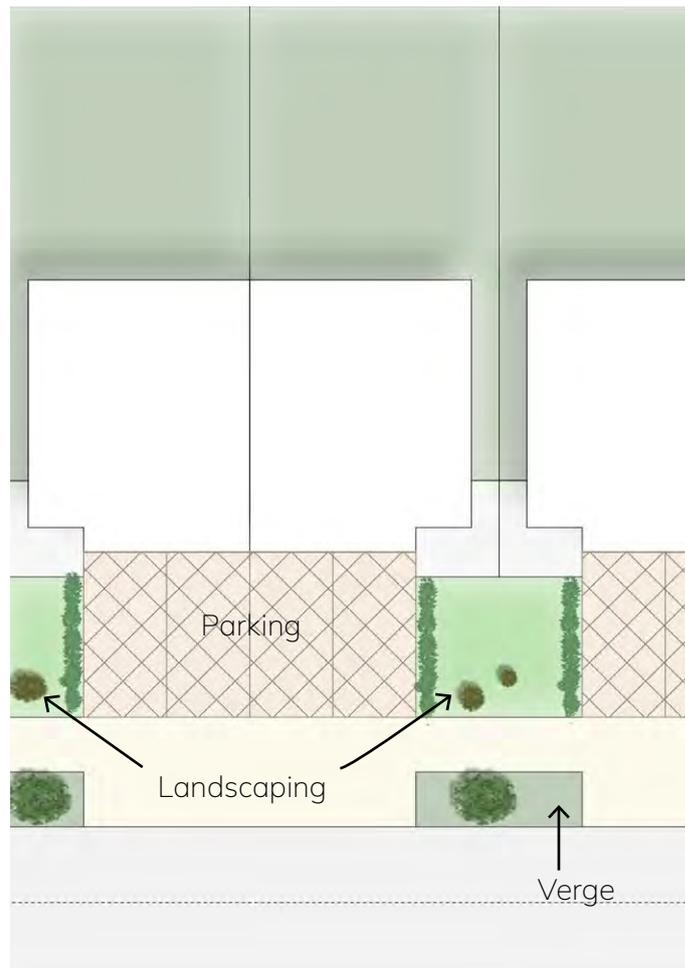
PARKING (WY)

In curtilage - Front of Property

Providing parking in plot to the front or side of properties was considered the most popular during consultation. To ensure that parking does not dominate the street scene and the creation of green streets, the following code and guidance are provided:

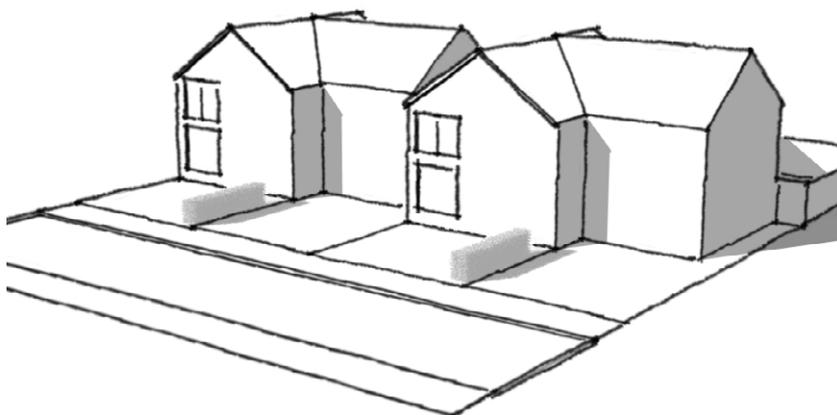
Code WY22: In Curtilage - Front of Property

- No more than 4 consecutive front of plot spaces.
- Where more than 4 spaces are required an area of soft landscaping with tree and / or hedge planting measuring a minimum of 2m in length x 5m in width must be provided after every 4th space. This includes front gardens.
- Single space dimensions: 6m in length x 3m in width.
- Double space dimensions: 6m in length x 5.5m in width.
- Additional 0.6m to enable the movement of people and refuse facilities.
- A maximum of 60% of parking proposed can be to the front of properties (see Code WY20)



Front of Property Parking Guidance

- The inclusion of hedge planting to the side of parking bays is encouraged to act as screening.



Front of plot parking



Screening through planting,

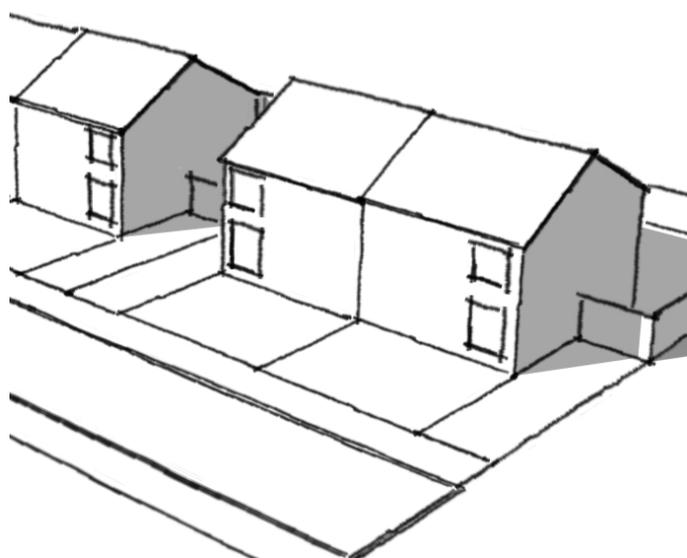
PARKING (WY)

In Curtilage - Side of Property

Providing parking to the side of properties enables the provision of front gardens and tree planting and will also reduce the dominance of parking on the street scene.

Code WY23: In Curtilage - Side of Property

- Single space dimensions: 6m in length x 3m in width.
- Double space dimensions: 11m in length x 3m in width.
- Additional 0.6m to enable the movement of people and refuse facilities, where applicable.



Side of plot parking

Garages

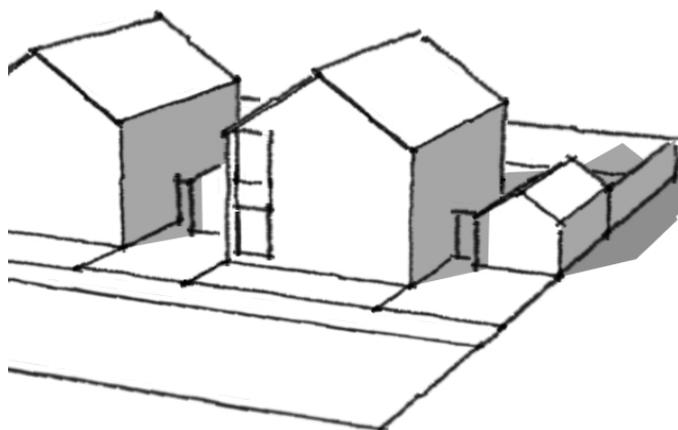
Public consultation indicated that garages are seen as a desirable form of parking in new residential developments. To ensure that they do not impact upon the street scene and encourage active frontages the following code and guidance are provided:

Code WY24: Garages

- To count toward parking provision garages must have minimum internal dimensions of 6m x 3m.
- Detached garages must be a minimum of 2m behind the building line of the associated residential dwelling.
- No more than 2 consecutive garages. This applies to both integrated and detached.

Garages Guidance:

- Integrated garages can impact upon the street scene through increasing the levels of inactive elevations. It is therefore important to ensure that the level of integrated garages does not dominate the street scene.
- Integrated garages should be recessed with the residential aspect of the dwelling being the primary focus of elevations.



Detached garage

PARKING (WY)

Courtyard parking

The provision of courtyard parking can be a useful parking method in providing allocated on street parking in a positive setting.

It is important to ensure that courtyards are designed appropriately and must be in the form of small courtyards with landscaping, feature paving and street furniture.

Code WY25: Courtyard Parking

- Proposals for courtyard parking must incorporate the following:
 - » Safe pedestrians and cycle movement.
 - » Natural surveillance from adjoining houses, or by buildings entered from the parking area.
 - » Landscaping as either; a central space with parking around or, as landscaped areas measuring 1m in width after every 4th space and include tree / hedge planting.
 - » Central landscaped areas should include planting and be designed as social spaces.
 - » Provide a careful balance between the desire of car owners to park as near to their homes as possible and the need to maintain the character of the overall setting.
 - » Prevent inappropriate parking through careful placement of street furniture / planting.



Courtyard Parking - Hatlepool

On Street Parking

Providing parking within the street can take the form of either allocated or unallocated space.

Code WY26: On Street Allocated & Unallocated

- Proposals for on-street parking must:
 - » Be 2.4m in width x 6m in length, per space.
 - » Provide a maximum of 4 consecutive spaces.
 - » Provide an area of soft landscaping with tree planting measuring a minimum of 2.4m in width x 6m in length after every 4th space. Where more than 4 spaces are provided.
 - » Clearly define the space as private for allocated bays.



On-street parking with landscaping - St Andrews

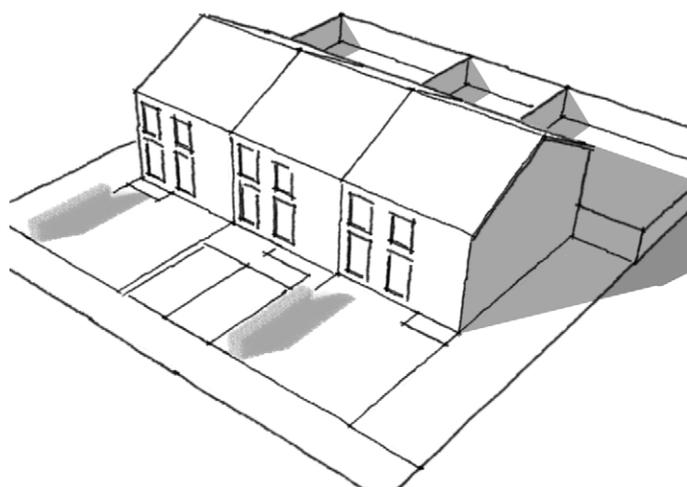
PARKING (WY)

Terraced properties parking

Where terraced properties are provided with front of plot parking there is a negative impact on the street scene and a reduction in the provision of front gardens.

Code WY27: Terraced Properties Parking

- Parking for short runs of terraced properties (up to 4 units) must consist of:
 - » End properties provided with side of plot parking and front gardens.
 - » Central unit's provided with front of plot parking (see image below).
- Parking for more than 4 terraced properties must:
 - » Take the form of either courtyard parking and / or on street parking.
- Rear parking will not be considered appropriate.



Terraced parking (short runs)

Parking Materials

The type of materials used for parking can impact upon the street scene and should be used to define to change from public to private space

Materials Guidance

- Parking within the curtilage of the dwellings should consist of the following:
 - » Permeable paving.
 - » Tarmac.
 - » Blocks.
- Proposals should provide a different surface material to that proposed on footways and roads to clearly distinguished between public and private space.

BUILT FORM DETAIL (WY)

Urban form

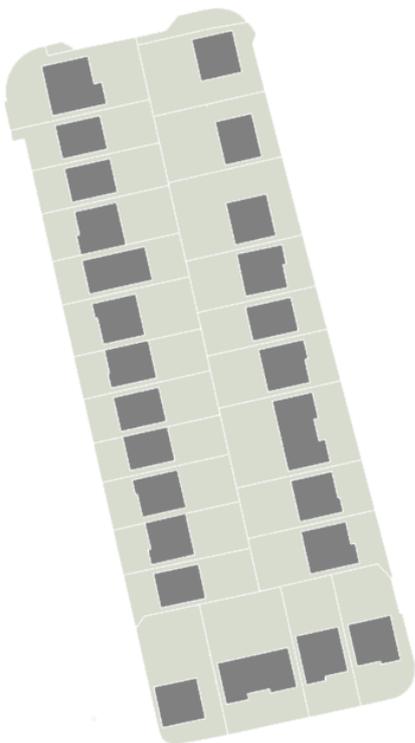
Development should take the form of perimeter block development with housing facing onto streets/public realm and gardens facing inward creating a clear distinction between public and private space.

Perimeter blocks can be provided in either formal or informal styles.

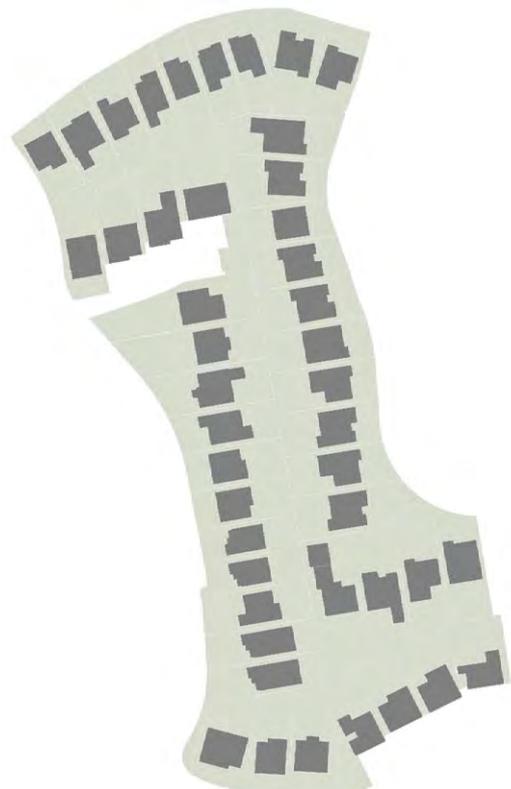
- Formal perimeter blocks consist of regular rectangular blocks with consistent building lines and street geometry.
- Informal perimeter blocks are irregular in shape with variations in building lines and curved street geometry.

Urban Form Guidance:

- Formal perimeter blocks should be used for development on Boulevard's, Wynyard Neighbourhood Streets, and near local centres or community facilities, where provided.
- A mix of formal and informal perimeter blocks should be used for development on Wynyard Streets.
- Informal perimeter blocks should be used on Wynyard Lanes and Wynyard Private Drives.
- Where development blocks are adjacent to areas of public open space, green wedge or are located on the edge of Wynyard, informal perimeter blocks are encouraged to create a soft interface between urban and rural.



Formal perimeter block



Informal perimeter block

BUILT FORM DETAIL (WY)

Cul-De-Sacs

Proposals which are based upon an interconnected series of Cul-De-Sacs can reduce the overall permeability of areas, increase walking distances and cause disorientation for users.

Code WY28: Cul-De-Sacs

- Only permitted on Wynyard Streets, Wynyard Lanes and Wynyard Private Drives.
- Pedestrian permeability must be maintained, e.g. using modal filters (see pg53).

Cul-De-Sac Guidance:

- Cul-De-Sacs within new residential development will only be considered where permeability and way finding are maintained for users.

Building Heights

Within Wynyard there is a variety of building heights from 1.5 - 3 storey. The predominant building height is 2 to 2.5 storey. The changes in building heights throughout Wynyard adds interest and variety to the street scenes. Changes in roof pitch also increase the scale of properties within the area and creates varied roof lines.

Building Heights Guidance:

- The following storey heights should be achieved:
 - » Boulevard - up to 3 storey.
 - » Neighbourhood Street - up to 3 storey.
 - » Wynyard Street - up to 2.5 storey.
 - » Wynyard Lanes - up to 2.5 storey.
 - » Wynyard Private Drives - 2 storey up to 3 storey at key locations.
 - » In locations on the edge of Wynyard up to 2 storey.
- Key locations include:
 - » Entrances.
 - » Corner locations.
 - » View terminus points.
 - » Focal points.
 - » Neighbouring areas for community facilities.

You Said:

- » Cul-de-sacs are disorientating
- » Being able to navigate streets is important.

Housing Mix

The analysis of the Wynyard Area Type highlighted the predominance of detached and semi-detached properties.

Housing Mix Guidance:

- Any new residential development within Wynyard should comprise:
 - » Predominately detached and semi-detached properties.
 - » Terraced properties (short runs up to 4 units) and apartments. These will be considered appropriate on Boulevard's, Neighbourhood and Wynyard Streets, neighbouring key facilities such as a local centre/school, at site entrances and within the retirement village.
- Where access alleys are required for the rear of terraced properties they should:
 - » Not negatively impact upon the rear garden sizes of adjoining properties.
 - » Be clearly defined as private space.
 - » Provide gates at their entrances.

Gateways

Proposals for new residential development within Wynyard should include gateways when accessed directly from Boulevard Streets or the A689.

Code WY29: Gateways

- Where gateways are proposed they must include:
 - » Landmark buildings, see Code BF3.
 - » Landscaping and tree planting.
 - » Entrance features such as feature walls or public artwork.

BUILT FORM DETAIL (WY)

Materials & Elevational Detailing

It is not the intention of this code to be prescriptive in terms of setting materiality or elevational detailing of proposals or stifle innovative designs.

Materials and Elevational Detailing Guidance

- In accordance with Code BF1, any proposals should have identified the key features of the existing built form, which have a positive influence on the character of the Wynyard Area Type. Proposals should clearly set out how the identified key features and materials are reflected within the architectural detailing of proposals.
- Key elevational details identified as part of the analysis for this code include the following details:
 - » Mini gables.
 - » Gabled front elevations.
 - » Head and cill detailing.
 - » Chimneys.
 - » Brick detailing.
 - » Window surrounds.
 - » Glazed panels.
 - » Dormer windows.
 - » Bay windows.
 - » Elongated contemporary windows.
 - » Canopies.
 - » Porches.
 - » Balconies.
- A variety of building materials are present within the Wynyard area. Any proposals should compliment the existing material pallet.
- The following images are provided as examples of potential elevational treatment and materials in the Wynyard Area Type.



BUILT FORM DETAIL (WY)

Boundary Treatment

Boundary treatments must be used to clearly define changes from public to private space as well as enhancing the character and appearance of the area.

Code WY30: Boundary Treatment

- Boundary treatments to the front of plots must not exceed 1.2m to ensure that the level of natural surveillance from the associated property can be provided over the street.
- When proposing boundary treatments to the front of plots, consideration must be given to visibility of vehicles entering and exiting the plot.
- Plots which adjoin areas of public open space must clearly distinguish between public and private space through the provision of a boundary treatment of either:
 - » Railings.
 - » Knee rail.
 - » Hedging.

Boundary Treatment Guidance:

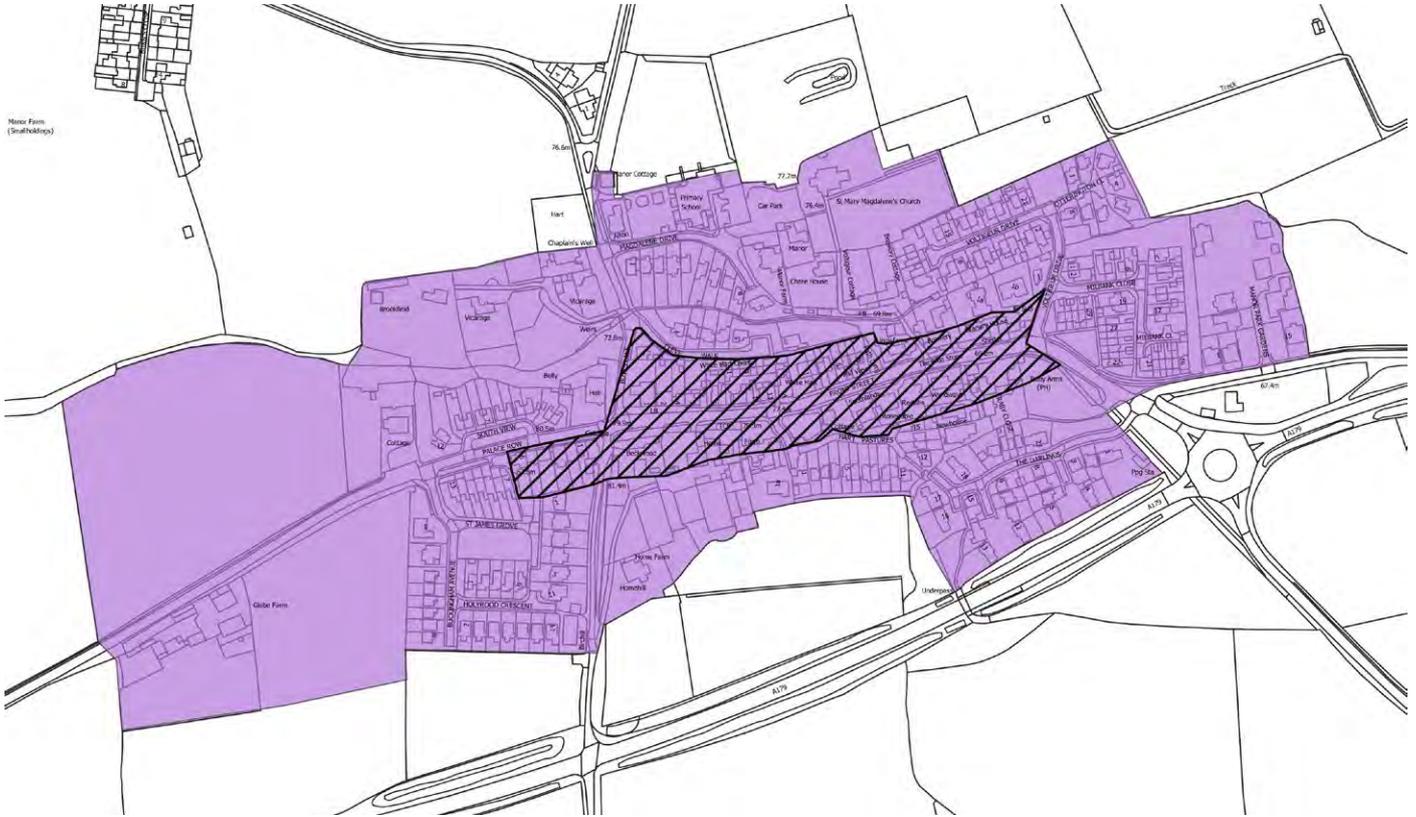
- Changes in the type of boundary treatment used on different street typologies is encouraged.
- Boundary treatments should not reduce the overall permeability of the development or reduce the level of openness of the street (see image below).
- Appropriate boundary treatments for Wynyard include:
 - » Metal railings.
 - » Hedge.
 - » Low brick wall (brick to compliment the primary material of the residential building).
 - » Brick and metal railing combination.
 - » Knee rail.
- For the established street types the following boundary treatments should be used, where required:
 - » Boulevard - metal railing, low brick wall, brick pillar and railing combination.
 - » Neighbourhood Street - metal railing, low brick wall, brick and metal railing combination.
 - » Wynyard Street - low brick wall, hedge.
 - » Wynyard Lanes - low brick wall, hedge.
 - » Wynyard Private Drive - hedge.
 - » Open Space - Knee rail, metal railing.

WYNYARD MATRIX

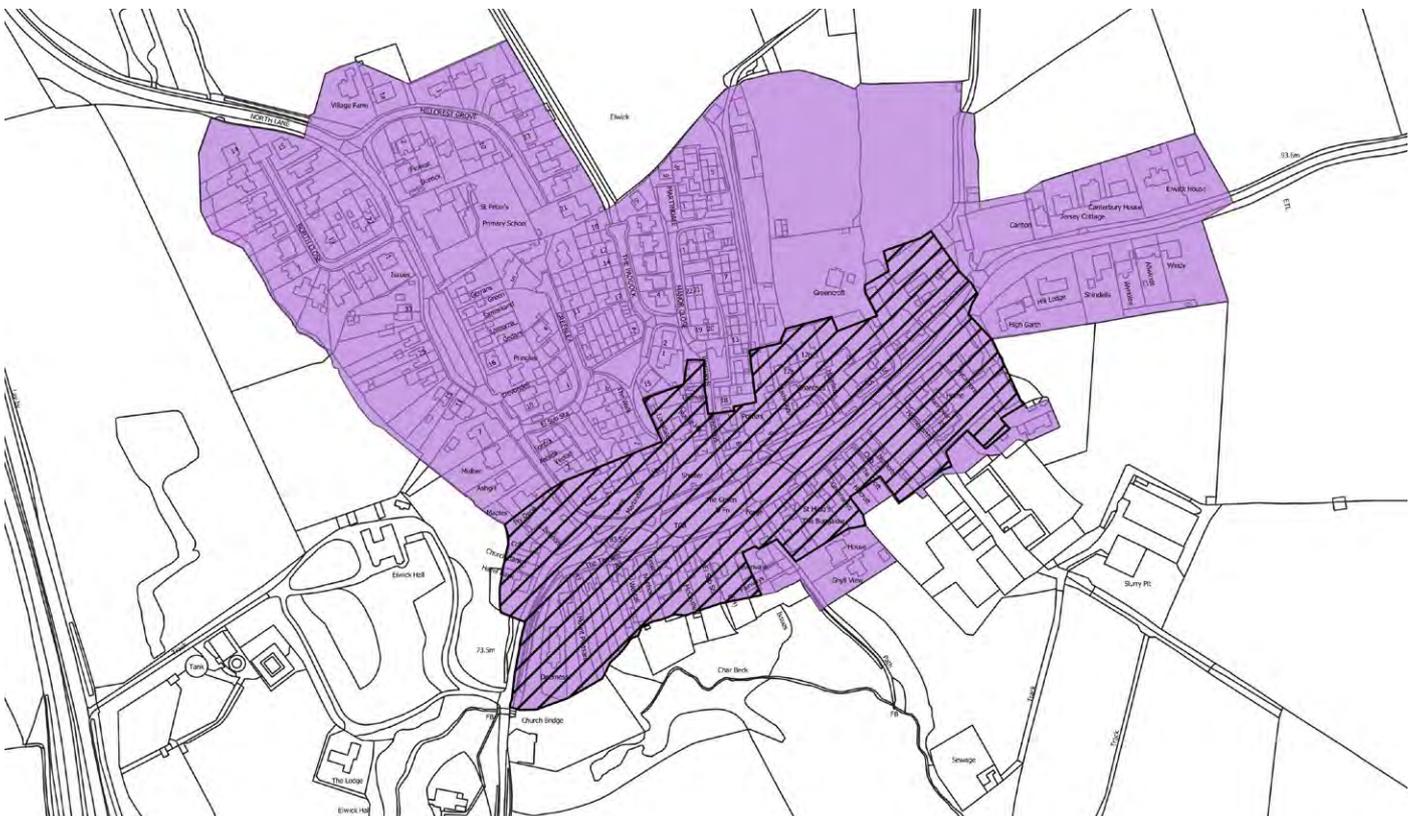
Street Type	Boulevard	Wynyard Neighbourhood Street	Wynyard Street	Wynyard Lanes	Wynyard Private Drives
Verge	2m on both sides of carriageway	2m on one side of carriageway or 1m either side	Non Mandatory	Non Mandatory	Non Mandatory
Landscaping	Planting and trees within verge.	Planting and trees within verge.	Landscaping pockets min.50m ²	Landscaping pockets min.50m ²	Front gardens
Street Trees	Yes in verge	Yes in verge	Yes in landscaping pockets	Yes in landscaping pockets	Front gardens, minimum of 2 trees per street.
Minimum Carriageway Width	6.75m	5.5m	5.5m	6.5m	3.7m minimum.
Minimum Footway Width	2m (both sides of carriageway)	2m (both sides of carriageway)	2m (both sides of carriageway)	Non defined in carriageway	N/A
Resting Places	Every 200m (approx)	Every 200m (approx)	Every 200m (approx)	In landscape pockets	No
Cycleway	Yes. 2m segregated or 3m shared.	Non defined in carriageway or 3m shared.	Non defined in carriageway or 3m shared.	Non defined in carriageway	No
Block Style	Formal	Formal	Mix	Mix	Mix
Building Line	Straight	Straight	Curved	Curved	Mix
Setback	2 - 6m	2 - 6m	2m minimum	2m minimum	2m minimum
Variation	2m	2m	3m	3m	6m
Cul-De-Sacs Permitted?	No	No	No	Yes	Yes
House Types	Detached Semi- detached Apartments Terraces	Detached Semi- detached Apartments Terraces	Detached Semi- detached Terraces	Detached Semi- detached	Detached Semi- detached
Boundary Treatment	Metal railing low brick wall. brick pillar and railing combination	Metal railing Low brick wall Brick wall and railing combination None	Low brick wall Hedge None	Low brick wall Hedge None	Hedge None
Building Heights	Up to 3 storey	Up to 3 storey	2 storey up to 3 at key locations	2 storey up to 2.5 at key locations	2 storey up to 3 storey at key locations

Village Area Type

VILLAGES CODING PLAN



Hart



Elwick

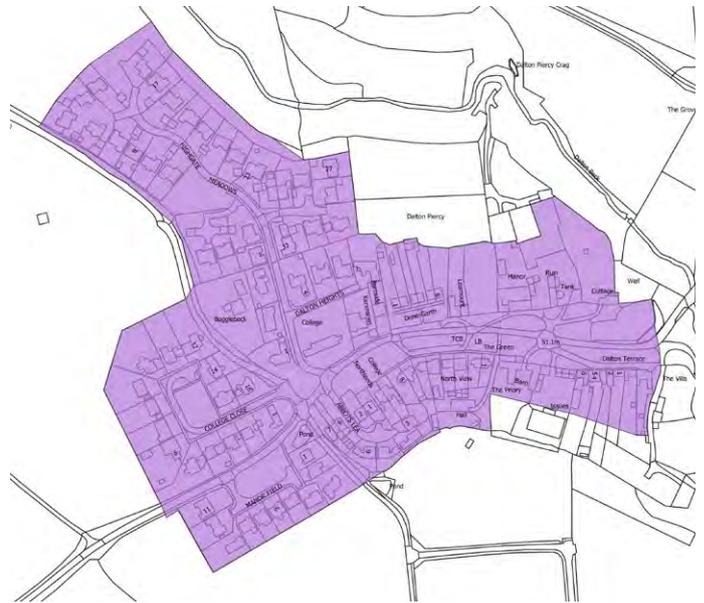
 Village Area Type

 Village Core

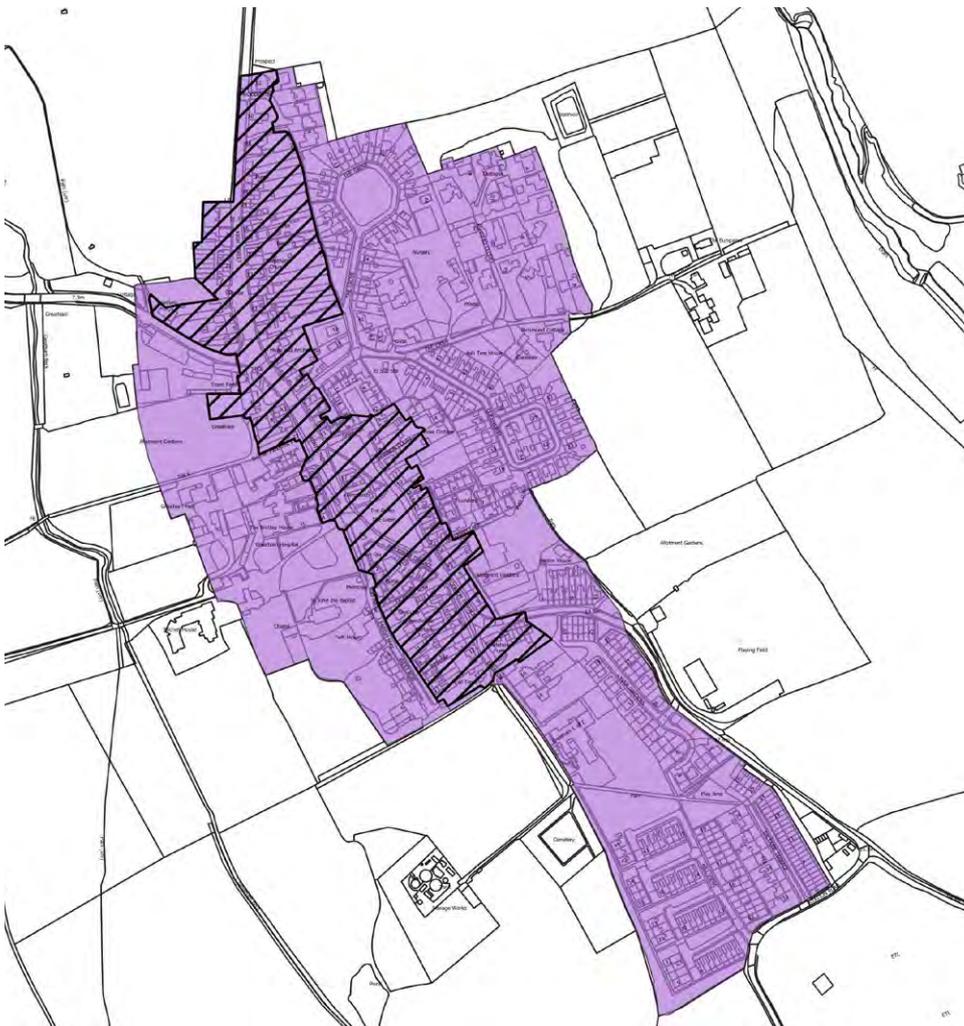
VILLAGES CODING PLAN



Newton Bewley



Dalton Piercy



Greatham



VILLAGES AREA TYPE

There are number of villages located within the rural area of Hartlepool these include:

- Dalton Piercy.
- Elwick.
- Greatham.
- Hart.
- Newton Bewley.

Should residential development come forward within these locations, applicants are expected to have regard to this code.

Summary of Design Analysis

The villages have all developed over time and as such, have a variety of styles and characters. Although each village has it's own identity there are similar characteristics between the villages.

Street Network

- Central historic linear street with variations in carriageway width along the street length reflecting their historic nature.
- Bus movement accommodated along central street.
- On street unallocated parking along central street, occasionally de marked through change in surface.
- Residential streets lead from the historic centre of the villages these are typically 5.5m in width and have a footway on either side.
- Parking at the edge of Villages is predominately in curtilage to the front of the plot for detached / semi-detached properties. For terraced properties parking is provided in courtyards or on-street unallocated.

Built Form

- The historic core of each village is characterised by terraced properties and detached houses.
- Properties are predominantly 2 storey nature however changes in era of construction and the original purpose of the building result in variations in scale and ridge height.
- Properties are mainly accessed directly from the pavement with limited or no setback.
- Recessed doors provide a small area of defensible space.
- Higher density areas with enclosed streets due to predominance of terraces and limited set backs.
- At the edge of the villages, properties are typically:
 - » Detached and semi-detached with landscaped front gardens. There are some instances of terracing with parking courts or unallocated on street parking.
 - » Have a varied setback from 1- 6+m with a predominance of over 6m.
 - » 2 storey in nature with limited variety in ridge height.
 - » Medium to low density areas, predominately suburban in nature, with instances of large detached dwellings in grounds.



Village Core street - Elwick

VILLAGES AREA TYPE

Materials

- Predominant building materials include:
 - » Red brick.
 - » Render – Full property cover in various colours.
 - » Stone.
 - » Red roof tiles.
 - » Grey roof tiles / slates.
- Later additions on the edge of the villages have introduced:
 - » Buff brick.
 - » Partial render.

Architectural Detailing

Each village has their own features which make them unique.

- Greatham and Elwick are within conservation areas and have detailed visual assessments.
- Properties at the edge of the villages are typical of the era in which they were built with some features reflecting back to the village core.
- Predominant architectural features include:
 - » Chimneys.
 - » Door surrounds.
 - » Window surrounds.
 - » Head and cill detailing.
 - » Recessed doors.
 - » Sash windows.
 - » Low boundary wall in either brick or stone.
 - » Bay windows.
 - » Mini gable protrusions (edge of village).
 - » Canopies (edge of village).
 - » Porch (edge of village).



Village Core images

VILLAGES AREA TYPE

Landscaping

- Typically a central village green with tree planting. The surrounding built form fronts onto the village green creating communal spaces.
- Incidental open space with tree and shrub planting along the core street.
- Occasional verge with planting on the edge of the villages.
- Landscaped front gardens (edge of village).



Village Edge images

STREET NETWORK (VL)

You Said:

- » The village identity needs to be retained
- » More trees should be planted.

To reflect the distinct variation between the traditional village core and the later expansion at the edge of the villages, the Village Area Type codes will reference Village Core or Village Edge. These areas are defined on the Coding Plan.

The Village Core relates to the historic centres of the villages and the Village Edge to the post-war developments leading away from the Village Cores.



Tree planting within street - Derwenthorpe, York

Street Network

The street network within the village core consist of existing linear streets leading to residential streets within the Village Edge Area Types. The existing built form has a strongly established building line with a limited set back.

Code VL1: Village Core Street

- New residential development, which is located fully or partially, within the Village Core must:
 - » Provide vehicle access directly from the Core Street, where required.
 - » Provide a pedestrian path, where one does not currently exist. The new footpath must connect into the existing pedestrian movement network along the Core Street.
 - » Provide a verge with tree planting where the Character Analysis (see Code BF1) has identified this is a characteristic of the Village Core.

Code VL2: Core Street Building Line and set back

- New residential development must:
 - » Follow the existing building line.
 - » Provide frontage onto the street.
 - » Be accessed directly from the footway.

Village Core Street Guidance

- Where the existing building line does not provide a set back:
 - » A variation of up to 0.5m from the existing building line should be considered to enable an area of defensible space or;
 - » Where a set back is not achievable a recessed doorway should be provided to create an area of defensible space.

STREET NETWORK (VL)

It is anticipated that residential development will be located at edge of existing villages and will take the form of small scale extensions as is the case for the housing allocations at the Villages of Elwick and Hart.

Code VL3: Street Hierarchy

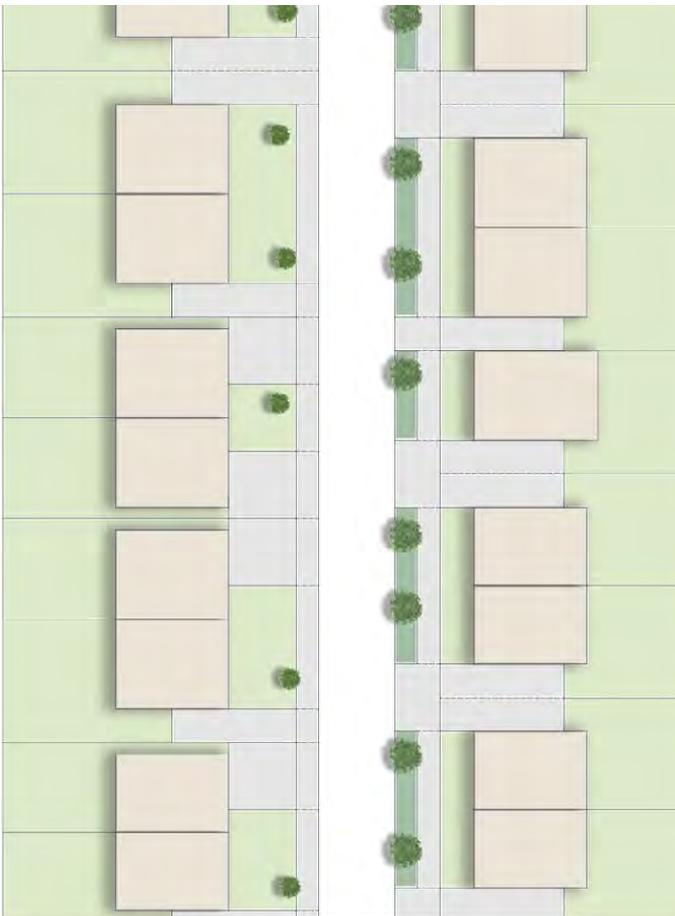
- Any new residential development which consists of more than one street must include a street hierarchy in accordance with Codes VL4 - VL11.

Village Street

The aim of a Village Street is to provide the main pedestrian, cycle and vehicle connections through a development proposal whilst also providing the main connection into the existing village movement network.

Code VL4: Village Street

- Village Streets must:
 - » Provide a 2m verge, as a minimum, on one side of the carriageway. This can alternate between sides but must be provided for the entirety of the street.
 - » Incorporate street tree planting (see Code N10).
 - » Provide a verge for at least 70% of the total street length taking into consideration driveway crossings. E.g. for a street 100m in length the verge must be provided for at least 70m.
 - » Provide a resting place in accordance with Code N6.
 - » Ensure where visitor parking is provided that a verge is retained for 70% of the total street length.
 - » Not provide barriers alongside verges or footways. This enables permeability and reduces visual clutter.
 - » Provide a carriageway width of 5.5m.
 - » Provide a minimum footway width of 2m on both sides of the carriageway.
- Cul-de-sacs are not encouraged on Village Streets, however, where these are required due to the sites location or limited availability of access points, pedestrian permeability must be maintained.



Example of Village Street with verge

STREET NETWORK (VL)

Code VL5: Village Street Connections

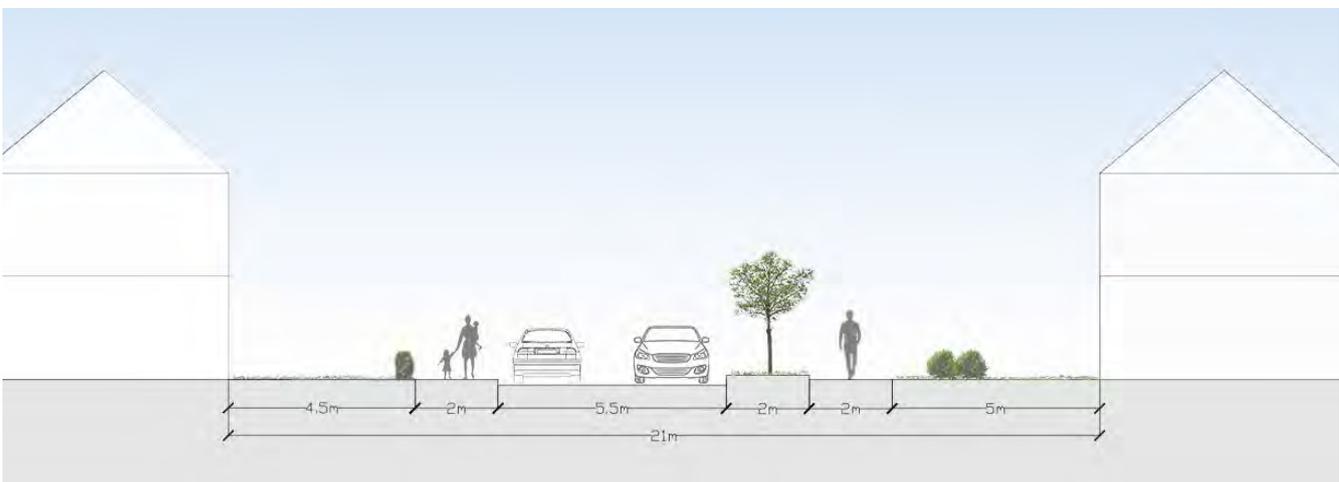
- To ensure the creation of sustainable developments within the rural area pedestrian connections must be provided:
 - » Into the Village Core.
 - » To public transport routes, where available.
- Pedestrian and cycle connections must connect into existing Public Rights of Way or Permissive Routes in accordance with Codes M2 - M7.

Code VL6: Village Street Setback

- Whilst allowing for the separation distances set out in HB1, a minimum set back of 2m must be provided from the pavement.
- Variation: the front face of buildings must not vary more than 3m from the building line.

Village Street Guidance

- A 1m verge will be considered where evidence can be provided that site restrictions do not allow for a 2m verge. In these instances, tree planting will still be required and underground rooting systems should be utilised.
- Drainage features such as rain gardens are encouraged in verges (see Code N11).
- The route of Village Streets should follow desire lines identified in the Site Analysis of the site (see Code SA1).
- Crossings should be located on or close to desire lines so that pedestrians find them convenient and pleasant to use.
- Cycle routes should be provided through sites, preferably through green spaces and connect into existing cycle networks.
- The level of setback provided should be consistent with the character analysis undertaken through Code CA1.
- Where the proposal is within view of the Village Core a minimal setback should be provided to correspond with the village characteristics.



Village Street cross-section

STREET NETWORK (VL)

Village Lane

Village Lanes will lead from the Village Street, providing access into lower traffic flow areas. They support active travel and encourage the use of streets as social spaces. Village Lanes can take the form of standard highway or shared surfaces.

Code VL7: Village Lane

- Provide a Village Lane as either a standard highway or shared surface.
- Standard highway design must comprise of
 - » A carriageway width of 5.5m with a 2m foot way provided on both sides of the carriageway.
- Shared surface carriageway design must comprise of
 - » A minimum carriageway width of 6m + 0.5m hardened maintenance margin.
 - » And include a 1.8m service corridor within the carriageway.
 - » A change in surface material complementary to the village character to distinguish the street type and signal to users that a change in behaviour is required.
- To ensure the provision of street trees that are not within private ownership, landscaping pockets of a minimum of 50m² must be provided within Lanes. See code N5.

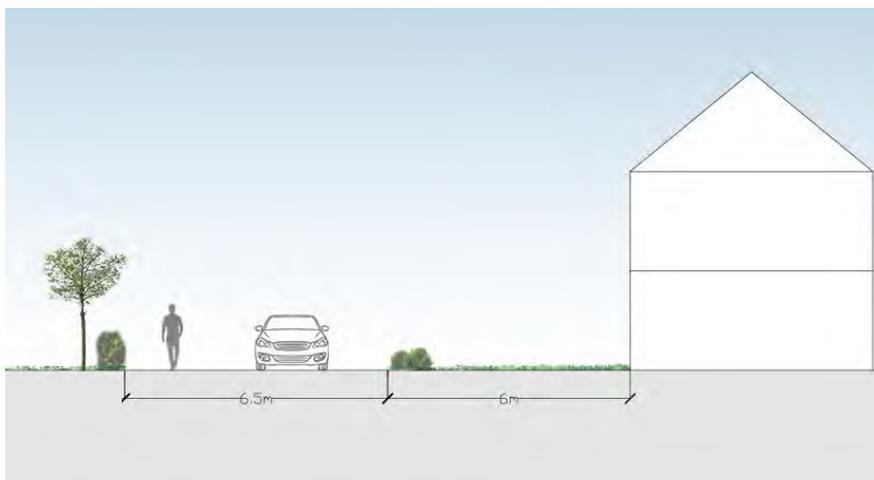
- Resting places to be provided every 200m (approx) see Code N6.
- Pedestrian only connections must be provided between neighbouring Village Lanes, e.g. modal filters (see page 51).

Code VL8: Village Lane Setback

- Whilst allowing for the separation distances set out in HB1, a minimum set back of 2m must be provided from the carriageway.
- Projections are permitted.
- Variation: the front face of buildings must not vary more than 3m from the building line.



Example shared surface Lane

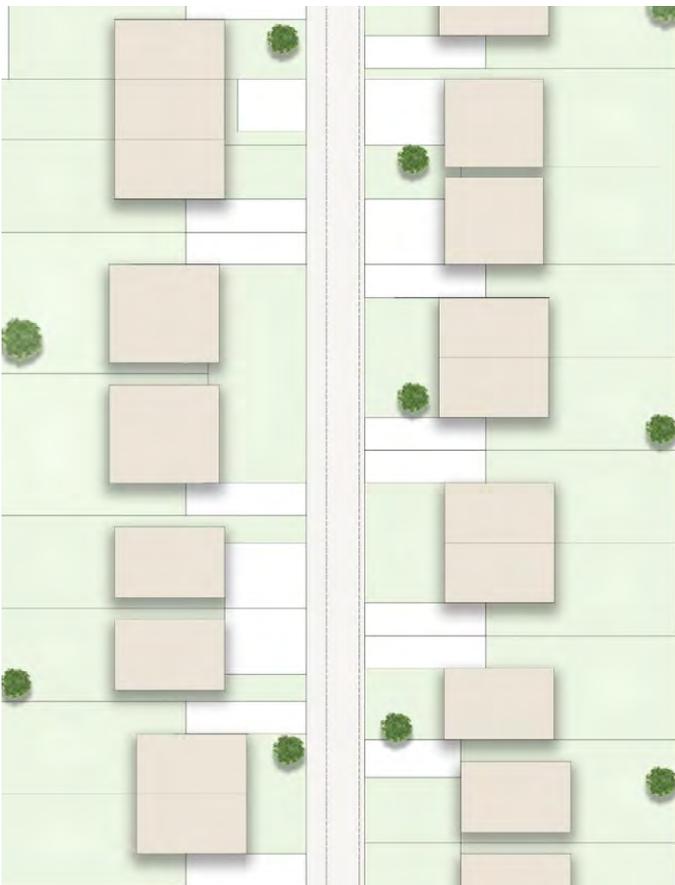


Village Lanes cross-section

STREET NETWORK (VL)

Village Lane Guidance

- Cycling should be provided for within the carriageway
- Crossings should be located on or close to desire lines so that pedestrians find them convenient and pleasant to use.
- The design of the street should ensure that parking is controlled and only taking place in designated areas.
- Visitor parking should be clearly marked.
- Tree planting should be utilised within the street to break up front of plot parking. This can be achieved through:
 - » Landscaping pockets or
 - » In landscaped areas between parking bays.
- Tree planting can be used as a means of traffic calming through diverting the carriageway.
- Note, coloured tarmac will be subject to a commuted sum for maintenance.



Lanes example layout

Private Drives

Provide connections for a small group of properties and are in private ownership. The streets are encouraged as private social spaces for the properties they serve and have no through traffic.

Code VL9: Private Drives

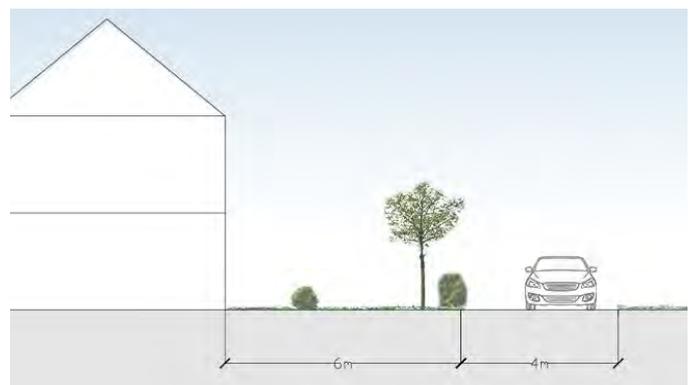
- Must serve no more than 5 properties.
- A maximum of 25m in length.
- Minimum of 3.7m in width.
- Bin collection points to be located a maximum of 25m from the adopted highway.
- a change in surface material must be provided to distinguish the street type and enhance the character of the street.

Code VL10: Private Drive Landscaping

- Each property on a Private Drive must be provide with a landscaped front garden following the house type plot ratio in Code VL14.
- Tree planting must be provided within front gardens for at least 2 properties per Private Drive.

Code VL11: Private Drive Setback

- Whilst allowing for the separation distances set out in HB1, a minimum set back of 2m must be provided from the carriageway.
- Projections such as bay windows, porches and balconies are permitted when not located adjacent the Village Core.
- Variation: the front face of buildings must not vary more than 3m from the building line.



Private Drive cross-section

PARKING (VL)

You Said:

- » The design of parking in new housing could be better and inappropriate parking should be discouraged

Parking Provision

A balance is to be made within residential areas on the provision of car parking and the creation of attractive streets.

Parking within new residential developments must sit comfortably within the street scene and not detract from the overall visual aesthetic of the residential area. To ensure this is achieved within new residential developments the following codes must be adhered to.

Code VL12: Parking Provision Standards

- Development proposals must provide as a minimum the parking standards as set out below.

Unit Size	Number of spaces
1 bedroom	2
2 bedroom	2
3 bedroom	2
4 bedroom	3
5 bedroom+	3
Flatted development	1.5

- The above parking standards adhere to the Highways Design Guide & Specification – Residential and Industrial Estates Development (amended May 2021).

Parking Methods

There are a range of parking solutions which are considered appropriate for the Village Area Type, these include:

- In curtilage to the front of properties.
- In curtilage to the side of properties.
- Garage.
- On street allocated and unallocated.
- Courtyard.

The parking dimensions specified within the following codes align with the Highways Design Guide & Specification – Residential and Industrial Estates Development (amended May 2021).

Code VL13: Parking Methods

- A maximum of 60% of parking proposed can be to the front of properties. This includes in curtilage (driveway) and allocated on-street parking bays.

Parking to plot ratio

Car parking has a negative impact on the street scene when there is also a lack of landscaping and greenery within the street.

Code VL14: Parking to Plot Ratio

- The following minimum requirements must be adhered to:
 - » Detached: A minimum of 40% of the front of the plot must be provided as landscaped garden.
 - » Semi detached: A minimum of 30% of the front of the plot must be provided as landscaped garden.

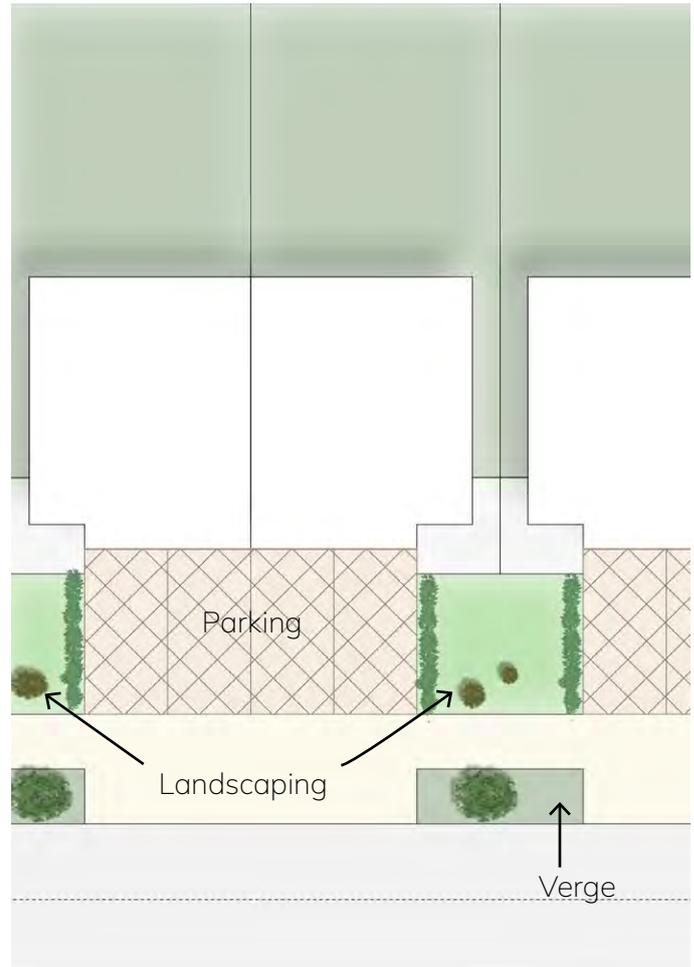
PARKING (VL)

In Curtilage - Front of Property

Providing parking in plot to the front or side of properties was considered the most popular during consultation. To ensure that parking does not dominate the street scene and the creation of green streets the following code and guidance are provided.

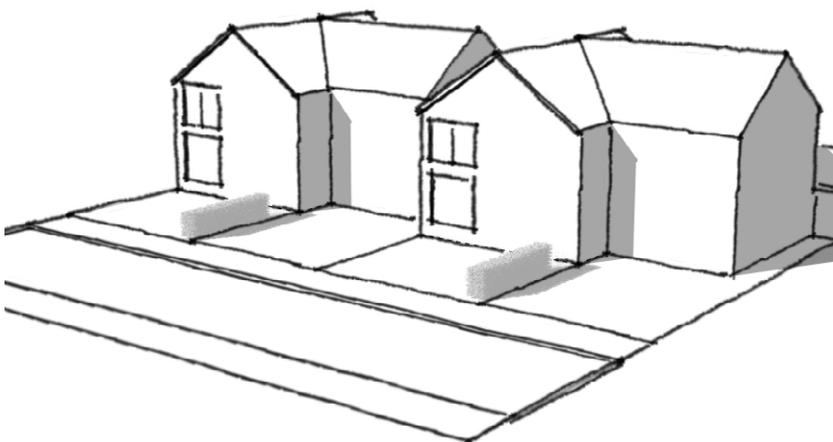
Code VL15: In Curtilage - Front of Property

- No more than 4 consecutive front of plot spaces.
- Where more than 4 spaces are required an area of soft landscaping with tree and / or hedge planting measuring a minimum of 2m in length x 5m in width must be provided after every 4th space. This includes front gardens.
- Single space dimensions: 6m in length x 3m in width.
- Double space dimensions: 6m in length x 5.5m in width.
- Additional 0.6m to enable the movement of people and refuse facilities.
- A maximum of 60% of parking proposed can be to the front of properties (see Code VL13)

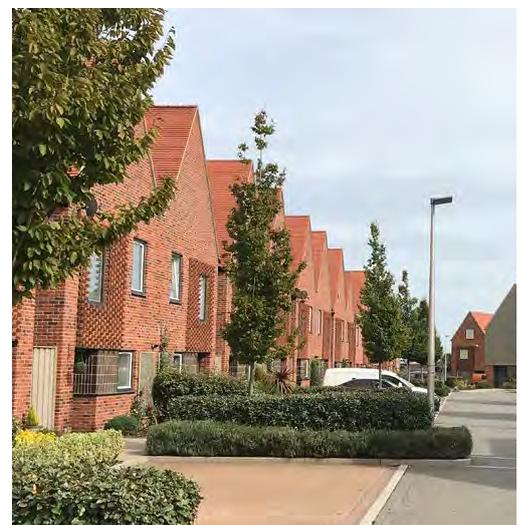


Front of Property Guidance

- The inclusion of hedge planting to the side of parking bays is encouraged to act as screening.



Front of plot parking



Screening through planting

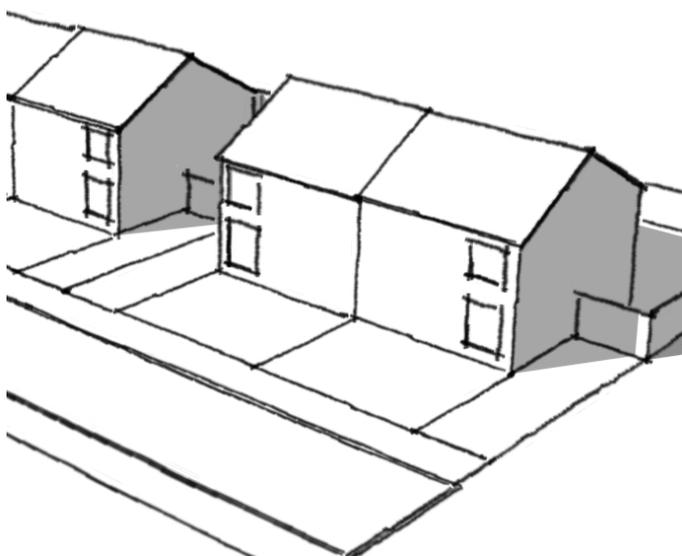
PARKING (VL)

In curtilage - Side of Property

Providing parking to the side of properties enables the provision of front gardens and tree planting and will also reduce the dominance of parking on the street scene.

Code VL16: In Curtilage - Side of Property

- Single space dimensions: 6m in length x 3m in width.
- Double space dimensions: 11m in length x 3m in width.
- Additional 0.6m to enable the movement of people and refuse facilities, where applicable.



Side of plot parking

Garages

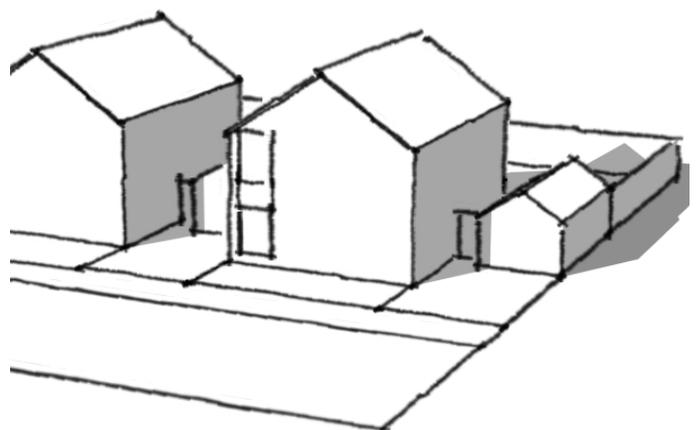
Public consultation indicated that garages are seen as a desirable form of parking in new residential developments. To ensure that they do not impact upon the street scene and encourage active frontages the following code and guidance are provided:

Code VL17: Garages

- To count toward parking provision garages must have minimum internal dimensions of 6m x 3m.
- Detached garages must be a minimum of 2m behind the building line of the associated residential dwelling.
- No more than 2 consecutive garages. This applies to both integrated and detached.

Garage Guidance:

- Integrated garages can impact upon the street scene through increasing the levels of inactive elevations. It is therefore important to ensure that the level of integrated garages does not dominate the street scene.
- Integrated garages should be recessed with the residential aspect of the dwelling being the primary focus of elevations.



Detached garage

PARKING (VL)

Courtyard Parking

The provision of courtyard parking can be a useful parking method in providing allocated on street parking in a positive setting.

It is important to ensure that courtyards are designed appropriately and must be in the form of small courtyards with landscaping, feature paving and street furniture.

Code VL18: Courtyard Parking

- Proposals for courtyard parking must incorporate the following:
 - » Safe pedestrian and cycle movement.
 - » Natural surveillance from adjoining houses, or by buildings entered from the parking area.
 - » Landscaping as either; a central space with parking around or, as landscaped areas measuring 2m in width and 4.8m in length occurring after every 4th space and include tree / hedge planting.
 - » Central landscaped areas should include planting and be designed as social spaces.
 - » Provide a careful balance between the desire of car owners to park as near to their homes as possible and the need to maintain the character of the overall setting.
 - » Prevent inappropriate parking through careful placement of street furniture / planting.



Courtyard Parking - Hatlepool

On Street Parking

Providing parking within the street can take the form of either allocated or unallocated space.

Code VL19: On Street Allocated & Unallocated

- Proposals for on-street parking must:
 - » Be 2.4m in width x 6m in length, per space.
 - » Provide a maximum of 4 consecutive spaces.
 - » Provide an area of soft landscaping with tree planting measuring a minimum of 2.4m in width x 6m in length after every 4th space. Where more than 4 spaces are provided.
 - » Clearly define the space as private, for allocated bays.



On-street parking with landscaping - St Andrews

PARKING (VL)

Terraced Properties Parking

Where terraced properties are provided, parking within courtyard is the preferred method of parking for the Village Area Type. The provision of front of plot parking for all properties can have a negative impact on the street scene and a reduction in the provision of front gardens.

Code VL20: Terraced Properties Parking

- Parking for short runs of terraced properties (up to 4 units) must consist of:
 - » End properties provided with side of plot parking and front gardens.
 - » Central units provided with front of plot parking (see image below).
- Parking for more than 4 terraced properties must:
 - » Take the form of courtyard parking (see Code VL18).
- Rear parking will not be considered appropriate.



Terraced parking (short runs)

Parking Materials

The choice of materials used for areas of parking will have an impact upon the over all street scene created. The over-use of black tarmac can create an urban feel and is therefore not encouraged in the Village Area Type

Materials Guidance

- Parking within the curtilage of the dwellings should consist of the following:
 - » Permeable paving.
 - » Blocks.
 - » Coloured tarmac.
- Proposals should provide a different surface material to that proposed on footways and roads to clearly distinguished between public and private space.

BUILT FORM DETAIL (VL)

You Said:

- » New housing should be terraces around a public green.

Urban form

Development should take the form of perimeter block development with housing facing onto streets/public realm and gardens facing inward creating a clear distinction between public and private space.

Perimeter blocks can be provided in either formal or informal styles.

- Formal perimeter blocks consist of regular rectangular blocks with consistent building lines and street geometry.
- Informal perimeter blocks are irregular in shape with variations in building lines and curved street geometry.

Code VL21: Urban Form

- Built form must successfully integrate into the existing developed area by:
 - » Providing frontage onto the public realm.
 - » Follow the existing building line (see Codes BF4 and BF5)
 - » Create perimeter block developments.
 - » Where the development will be adjoining existing built form, developments must create perimeter block style layouts, where feasible, and ensure separation distances are maintained (see Code HB1).

Urban Form Guidance:

- Formal perimeter blocks should be used for Village Streets and informal perimeter blocks should be used for development on Village Lanes and Village Private Drives and adjacent to areas of public open space.
- The analysis has identified village green's as a key feature of the Area Type. New residential developments should provide a central green space with the built form providing frontage and activation over it.

Building Line

The shape of the building line will contribute to the character of the area, the Village Area Type is characterised by predominance of straight building lines however more recent developments have create curved building lines toward the rural area.

Frontage Guidance:

- A mix of straight and curved building lines should be created. Curved building lines should be focused on the rural edge or adjacent to green spaces.
- Houses with side parking can be placed closer to the highway however this should not be at the detriment of front gardens, tree planting and residential amenity.



Housing around green, Nantwich, Cheshire



Housing around green, Sandycote, Exeter

BUILT FORM DETAIL (VL)

Building Heights

Within the Village Core there is a predominance of 2 storey properties however there is variation in the overall scale and ridge height between the properties. At the edge of the villages there is a variety of building heights between one and 2.5 stories. The changes in scale throughout the villages adds interest, creates varied roof lines and variety to the street scenes.

Code VL22: Building Heights and Roof scape

- New residential dwellings adjacent to or within the Village Core must
 - » Be consistent with the existing heights within the direct vicinity.
- Changes of scale must be provided through variation in storey height and ridge height.
- A varied roof line must be provided.

Building Heights Guidance:

- Properties within the Village Area Type should be up to 2 stories in height. 2.5 will be considered in key locations or to reflect the Character Analysis.
- Key locations include:
 - » Entrances.
 - » Corner locations.
 - » View terminus points.
 - » Focal points.
 - » Neighbouring areas for community facilities.

Housing Mix

The core of the villages is characterised by a predominance of terraced properties and detached farm houses. The village edge is characterised by semi-detached and detached properties.

There are elements of terraced properties at the edge of villages, particularly at Elwick and Greatham, which better reflect the housing mix of their Village Core's.

Code VL23: Housing Mix

- A mix of detached, semi detached, terraced, bungalows and apartments must be provide to reflect the varied nature of the village.
- Where proposals will adjoin a Village Core or existing built form the house types must be consistent with those existing. For example, where terraced properties are adjoining the site, the proposal should include terraced properties closest to the existing built form with detached properties located toward the edge of the proposal.

Housing Mix Guidance

- Terraced properties should be designed with:
 - » Alleys for rear access which do not negatively impact upon the rear garden sizes of adjoining properties and should be clearly defined as private space. Gates must be provided at their entrances.



Variations in scale, Greatham

BUILT FORM DETAIL (VL)

You Said:

- » History of the buildings is a valued quality of the villages.
- » Buildings should be varied in appearance and have detailing.

Materials and Elevations

It is not the intention of this code to be prescriptive in terms of setting materiality or elevational detailing of proposals or to stifle innovative designs.

Proposals within the Village Area Type must sit comfortably with and reflect the distinctive character of the surrounding built form.

Code VL24: Elevational Treatment

- House types must reflect the positive architectural detailing of the associated Village Core as identified through Code BF1.
- Standard house types must be altered to reflect the characteristics of the Village.
- Variety in elevational treatments and materials must be provided within the same street type to replicate the varied nature of the existing village streets.
- Proposals within Greatham and Elwick must be informed by the conservation area visual assessments / area appraisals.

Elevational Guidance

- In accordance with Code BF1 any proposals should have identified the key features of the existing built form, which have a positive influence on the character of the Village Area Type. Proposals should clearly set out how the identified key features and materials are reflected within the architectural detailing of proposals.
- Key elevational details of the Village Area Type which can be referenced within design proposals include:
 - » Window proportions and pattern.
 - » Head and cill detailing.
 - » Door surround detailing.
 - » Materials.
 - » Pattern of materials.
- Elevational treatment should sit comfortably when read within the context of the street
- The below images are provided as examples of potential elevational treatment and materials in the Village Area Type.



BUILT FORM DETAIL (VL)

Boundary Treatment

Boundary treatments must be used to clearly define changes from public to private space as well as enhancing the character and appearance of the area.

Code VL25: Boundary Treatment

- Where a boundary treatment is required for proposals adjacent to or within the Village Core. This must be a brick or stone wall up to 1m in height.
- Boundary treatments must be used sparingly in the Village Edge unless the Character Assessment (Code BF1) has identified them as a defining feature of the area or for locations where definition between public and private space is required.
- Boundary treatments to the front of plots must not exceed 1.2m to ensure that the level of natural surveillance from the associated property can be provided over the street.
- When proposing boundary treatment to the front of plots consideration must be given to visibility of vehicles entering and exiting the plot.
- Plots which adjoin areas of public open space must clearly distinguish between public and private space through the provision of a boundary treatment of either:
 - » Knee rail.
 - » Hedging.
 - » Low wall.

Boundary Treatment Guidance:

- Where boundary treatments are provided, changes in the type of treatment used between street typologies is encouraged.
- Boundary treatments should not reduce the overall permeability of the development or reduce the level of openness of the street.
- Appropriate boundary treatments for the Village Edge include:
 - » Hedge.
 - » Low brick or stone wall (material to compliment the primary material of the village).
 - » Post and rail timber fence
 - » Knee rail.
- For the established street types the following boundary treatments should be used, where required:
 - » Village Street – low wall, hedge.
 - » Village Lanes - low wall, hedge.
 - » Private Drive - hedge.

RURAL EDGE (RE)

Rural Edge

Proposals within the Village Area Type will adjoin the rural edge / open countryside of the borough. It is therefore important to ensure that future residential developments provide an appropriate response which enables developments to graduate into the rural landscape and respond to potential views.

Code RE1: Rural Edge Street Types

- Street typologies must include Village Lanes and Village Private Drives. Village Streets will not be permitted on the rural edge.

Code RE2: Rural Edge Parking

- The appropriate parking solutions for the rural edge include:
 - » Front of plot with a front garden following the ratio's set within Code VL14.
 - » Side of plot.
 - » Garage.

Code RE3: Rural Edge Connections

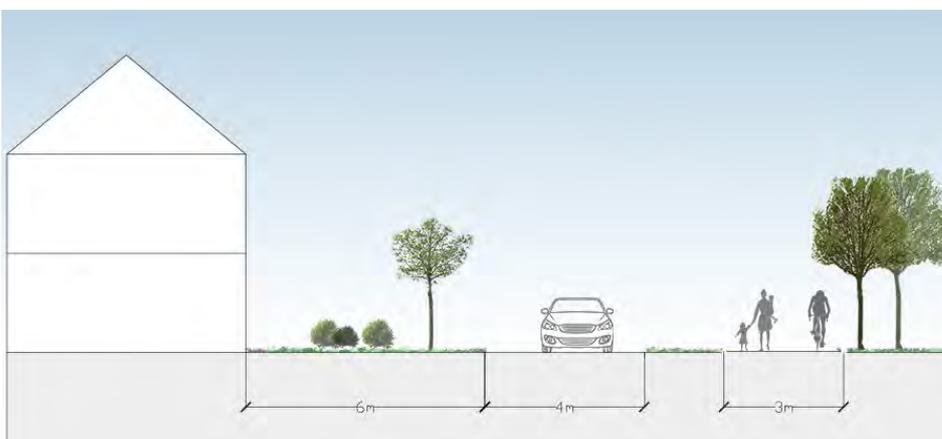
- To encourage active travel and promote health and well-being, connections by modes of active travel must be provided into the countryside.
- Pedestrian and cycle connections must connect into existing Public Rights of Way or Permissive Routes in accordance with Codes M4 - M7.

Code RE4: Rural Edge Frontage

- Properties must provide frontage onto the rural edge. Where side elevations are proposed these must be corner turning properties with dual frontages (see Code BF2).
- Presenting rear elevations and boundary fencing onto the rural edge will not be considered an appropriate response.

Rural Edge Guidance:

- Streets should be more curved in nature to create a graduation between the urban and rural characters.
- Where Private Drives are proposed on the rural edge these should not reduce the overall permeability of proposals. Where the predominant use of Private Drives prevents the movement of users, pedestrian connections to an adoptable standard should be placed within a verge or adjacent green space.



Rural Edge cross-section

VILLAGE MATRIX

Street type	Village Street	Village Lane	Village Private Drives
Verge	2m on one side of carriageway.	Non Mandatory	Non Mandatory
Landscaping	Planting and trees within verge.	Landscaping pockets min.50m ²	Front gardens
Street Trees	Yes in verge	Yes in landscaping pockets	Front gardens, minimum of 2 trees per street.
Minimum carriageway width.	5.5m	5.5m or 6.5m shared surface.	3.7m minimum.
Minimum footway width.	2m (both sides of carriageway)	2m (both sides of carriageway) or shared surface	N/A
Resting Places	Every 200m (approx)	Every 200m (approx)	No
Cycleway	Non defined in carriageway or defined within green space.	Non defined in carriageway.	No
Block Style	Formal	Informal	Informal
Building Line	Mix	Mix	Mix
Setback	Minimum of 2m	Minimum of 2m	Minimum of 2m
Variation	up to 3m	up to 3m	up to 3m
Cul-de-Sacs Permitted?	Yes, where appropriate	Yes	Yes
Permitted on Rural Edge?	No	Yes	Yes
House Types	Mix	Mix	Mix
Boundary Treatment	Low wall Hedge None	Low wall Hedge None	Hedge None
Building Heights	2 storey up to 2.5 at key locations	2 storey up to 2.5 at key locations	2 storey up to 2.5 at key locations

Appendices

APPENDIX 1: LIST OF RELEVANT POLICIES

Relevant Hartlepool Local Plan Policies (2018)

- Policy CC1: Minimising and adapting to Climate Change
- Policy CC2: Reducing and Mitigating Flood Risk
- Policy INF1: Sustainable Transport Network
- Policy INF2: Improving Connectivity in Hartlepool
- Policy QP3: Location, Accessibility, Highway Safety and Parking
- Policy QP4: Layout and Design of Development
- Policy QP5: Safety and Security
- Policy QP6: Technical Matters
- Policy QP7: Energy Efficiency
- Policy HE1: Heritage Assets
- Policy HE3: Conservation Areas
- Policy HE4: Listed Buildings and Structures
- Policy HE5: Locally Listed Buildings and Structures
- Policy NE1: Natural Environment
- Policy NE2: Green Infrastructure
- Policy NE4: Ecological Networks

Relevant Rural Neighbourhood Plan Policies

- Policy GEN2: Design Principles
- Policy H4: Housing in the Countryside
- Policy NE1: Natural Environment
- Policy NE2: Renewable and Low Carbon Energy
- Policy HA1: Protection and Enhancement of Heritage Assets
- Policy HA2: Protection and Enhancement of Conservation Areas
- Policy HA3: Protection and Enhancement of Listed Buildings
- Policy HA4: Protection and Enhancement of Locally Important Buildings

APPENDIX 2: GLOSSARY

Term	Definition
Accessibility	The ability of people to move around an area and reach places and facilities.
Active frontage	The front of a buildings with windows and doors onto the space to generate activity and engagement between the inside of the building the space outside.
Allocated	Lands which has been identified on the Local Plan (2018) for a specific type of development.
Amenity	The pleasant aspects of a location which contributes to its overall character and its enjoyment.
Amenity space	The outside space associated with a home or homes. It may be private or shared, depending on the building it serves.
Area Types	Parts of the local area that share common features and characteristics. For example, a suburban area type might bring together a number of different housing estates with common densities, heights, building line, party wall condition etc, under the umbrella term “outer suburbs”.
Boundary treatments	The way the edges of developments are designed and enclosed. For example, the boundary treatment around a house will usually be a fence, wall or hedge.
Building line	The line formed by the frontages of buildings along a street. The individual features or appearance that give an identity to an area
Built form	The three-dimensional pattern or arrangement of development blocks, streets, buildings and open spaces.
Context	The physical and social setting of a site or area, including factors such as traffic, activities and land uses as well as landscape and built form.
Connectivity	The degree to which something has connections, for example road connections, footway connections or the connection between area A and area B.
Conservation area	An area of special historical and/or architectural interest which is designated by the Local Planning Authority as being important to conserve or enhance.
Defensible space	Areas of private space that have some form of demarcation so that people know the space is private and not public. This can often be a low fence or wall around a front garden.
Gateway	The design of a building, site or landscape to symbolise an entrance or arrival to a special district.
Green space	Grassed areas with landscaping within housing estates. It enhances the visual appearance, provides opportunities for informal recreational activities close to home such as well-observed children’s play, exercising dogs, and an area for walking and to relax.

APPENDIX 2: GLOSSARY

Term	Definition
Green infrastructure	All of the natural and managed green spaces, features and water bodies that together make up a multifunctional network across rural and urban areas. The network includes green spaces such as parks and gardens on private or public land, and green links between spaces such as hedgerows and rights of way, as well as features such as blue corridors, green roofs/walls and ponds.
Landmarks	Buildings structures and spaces which create distinct visual orientation points that provide a sense of location to the observer within the neighbourhood.
Legibility	How easy it is for people to understand and find their way around a place and how memorable it is.
Natural surveillance	The ability of people to be seen from surrounding buildings or space without the need for CCTV.
Rainwater harvesting	The collection of water directly from the surface it falls on e.g. a roof or hard standing area. This water would otherwise have gone directly into the drainage system or been lost through evaporation and transpiration. Once collected and stored it can be used for flushing toilets, watering gardens and washing clothes using a washing machine.
Renewable energy	Those energy flows that occur naturally and repeatedly in the environment from the wind, the fall of water, the movement of the oceans, from the sun and also from biomass.
Roofscape	A view of roofs.
Scale	The impression of a building, or parts of a building when seen in relation to its surroundings.
Set-Back	The distance that buildings are set back from the pavement.
Street scene:	The appearance of all the elements of a street, including the carriageway, pavement, street furniture, planting, and the buildings or structures along its edges, particularly the composition of buildings on each side of the street.
Sustainable Drainage Systems (SuDS)	Efficient drainage systems which seek to minimise wastage of water including the use of groundcover to enable maximum penetration of run off into the ground and, where appropriate recycling grey water within the development
Variation:	The extent to which buildings can be set forward or back from the building line.



Neighbourhoods and Regulatory Services Committee

2 March 2026

Report of: Director for Neighbourhood and Regulatory Services

Subject: PLANNING OBLIGATIONS SUPPLEMENTARY PLANNING DOCUMENT (SPD) AND HOUSES IN MULTIPLE OCCUPATION (HMO) ARTICLE 4 DIRECTION AND SPD UPDATE.

Decision Type: Key decision (DNRS 114 / 26)

1. Council Plan Priority

Hartlepool will be a place:
where people live healthier, safe and independent lives. (People)
that is connected, sustainable, clean and green. (Place)
that is welcoming with an inclusive and growing economy providing opportunities for all. (Potential)
with a Council that is ambitious, fit for purpose and reflects the diversity of its community. (Organisation)

2. Purpose of Report

2.1. The purpose of this report is to update members on the progress of three key consultations undertaken by the Land Use Policy Team in recent months. The consultations focused on:

- A borough wide Houses in Multiple Occupation (HMO) Article 4 Direction.
- A HMO Supplementary Planning Document (SPD)
- A Planning Obligations Supplementary Planning Document (SPD).

2.2. The report also seeks permission to, consult upon the two SPDs for a further 8 week period, this is to allow for the publication of the Consultation Statement and to allow additional time to obtain residents and stakeholders view, to, in consultation with the Director of Neighbourhood and Regulatory Services and the Chair of committee update the two draft SPDs and submit to Full Council for Adoption in May 2026.

3. Background

3.1. In summer 2025 the Land Use Policy Teams began preparing for the process of consulting upon a borough wide HMO Article 4 Direction, a HMO SPD and an update to the 2015 Planning Obligations SPD. This process involved liaison with key HBC officers such as the legal team, highways team, licensing team, the drafting of documents and then formal consultation.

3.2. On 30th June 2025 a report was taken to Neighbourhood Services Committee to inform members of a proposed statutory consultation for potentially introducing a HMO Article 4 Direction within Hartlepool. The report was in response to concerns raised by members of the public and elected members in relation to HMOs across the borough.

3.3. On 20th October 2025 two reports were taken to Neighbourhood Services Committee for permission to consult upon a draft HMO SPD and the updated Planning Obligations SPD. The reports detailed that there is a need to introduce HMO guidance to ensure that applicants are aware of what will be expected should they seek planning permission for a HMO in Hartlepool. It was also set out that the 2015 Planning Obligations SPD required an update due to inflation, to provide more clarity to residents and stakeholders and to set out the parameters and information relating to Discount Market Value Affordable Homes.

3.4. With regard to the Article 4 Direction, a 12 month statutory consultation commenced on 24th September 2025, this consultation is necessary to ensure the Article 4 Direction can be implemented lawfully. In addition to the statutory 12 months consultation, the council undertook a 17 week (24th September 2025 to 23rd January 2026) information seeking consultation to gain residents and stakeholders views.

- 3.5. With regard to the two SPDs covering HMO Standards and Planning Obligations, information seeking consultations were undertaken for 11 weeks (3rd November 2025 to 16th January 2026) in a bid to obtain residents and stakeholders views.
- 3.6. For all of the consultations the HBC platform Your Say was utilised and consultation notifications were sent to those signed up to the platform. Key stakeholders and those within the Land Use Policy consultation database who have requested to be informed about changing HBC policy and guidance were sent e mails with all the key information and survey links contained. The consultations were advertised in Hartbeat magazine, a press release was issued, and documents were available online and in Hartlepool Civic Centre. Officers received a number of phone calls in which the consultations were discussed.
- 3.7. The consultations have now closed, but, as stated in Section 3.4, the statutory HMO Article 4 Direction consultation remains in place until 24th September 2026.

4. Proposals

- 4.1. Officers seek to undertake an additional eight week consultation period in order to publish the consultation statements that were drafted alongside the draft SPDs but not available at the first consultation period. Officers also consider it prudent to undertake additional consultation to ensure residents and stakeholders have sufficient time to comment upon and assist in shaping the SPDs prior to adoption.
- 4.2. The council is proposing to adopt the two SPDs in June 2026 and may bring into force the HMO Article 4 Direction on 1st December 2026 subject to Member agreement. A decision on adopting the SPDs will be taken by Full Council, a decision on how to proceed with the HMO Article 4 Direction will be brought to Neighbourhoods and Regulatory Services Committee and then Full Council in due course.
- 4.3. Officers are in the process of addressing the comments received to draw out key points. A comprehensive assessment and response to all comments will be included within the legally required consultations statements that will be provided alongside future Council reports to formally adopt the SPDs.
- 4.4. The paragraphs below set out information relating to the engagement undertaken to date and a summary of the key issues raised.

Houses in Multiple Occupation Article 4 Direction

4.5. 331 online responses were received; no letters were received to date, consultation is still open.

4.6. The key issues raised were as follows:

- 269 respondents were in support of introducing the HMO Article 4 Direction (81%).
- 62 respondents were not in support of introducing the HMO Article 4 Direction (18%).
- HMOs give rise to anti-social behaviour.
- HMOs take up car parking spaces and put pressure on the network.
- HMOs reduce community cohesion as people come and go.
- HMO Article 4 Direction will allow the council better control.
- HMO Article 4 Direction will give residents a voice and the chance to have their say.
- HMOs are needed for those who can't afford private rent.
- More affordable homes are needed across the borough.

Houses in Multiple Occupation Supplementary Planning Document

4.7. 30 online responses were received along with 1 letter from Historic England.

4.8. The key issues raised were as follows:

- Broad support for the draft SPD to be used as a tool to better manage and maintain HMOs.
- Broad support for avoiding the sandwich effect of HMOs with non HMO residential properties.
- Broad support that there should be no more than 3 HMOs in a row.
- Those disagreeing with limiting to no more than 3 in a row mostly think there should be fewer than this.
- Uncertainty about whether HMOs above commercial units is appropriate.
- General preference for there to be specific parking requirements for HMOs rather than flexibility.
- Concerns raised regarding parking pressures, antisocial behaviour and that there is already perceived to be too many HMOs in the town.

Planning Obligations SPD Supplementary Planning Document

4.9. 14 online responses were received and 2 letters were submitted. Letters submitted were from Sport England and Lichfileds (planning agent) on behalf of Taylor Wimpey.

4.10. The key issues raised were as follows:

- Should prioritise brownfield sites.
- There is a need for updated guidance from the council.
- Priorities for s106 spending –affordable housing, green infrastructure and community facilities.
- Broad support for play facilities.
- Broad support for outdoor sport (tennis courts and bowling greens).
- Broad support for the council’s approach to green infrastructure provision.
- Broad support for contributions towards preserving heritage assets.

5. Other Considerations/Implications

Risk Implications	There are identifiable risks associated with: (a) the Article 4 confirmation and scope (including potential Secretary of State modification/cancellation and Judicial Review risk), (b) the adoption of SPDs (procedural Judicial review risk if statutory steps are missed), and (c) operational resourcing due to an increase in planning applications once the Article 4 takes effect. Mitigations include adherence to Schedule 3 GPDO procedure, robust evidence justifying geography (smallest area necessary), legal review of notices. (d) weight given to the SPDs if they are not adopted by the Government’s deadline of 30th June 2026.
Financial Considerations	There are limited financial implications, including statutory advertising and site notice costs for the Article 4 making/confirmation, potential additional casework costs due to increased planning applications following confirmation, and publication costs for SPD adoption. These will be managed within existing budgets.
Subsidy Control	None
Legal Considerations	The work relating to the HMO Article 4 Direction has and will continue to be in accordance with HMO Article 4(1) of the Town and Country Planning (General Permitted Development) (England) Order 2015 (as amended).

	<p>The work relating to the SPDs has been and will continue to be in accordance with the Planning and Compulsory Purchase Act (2004) (as amended) and The Town and Country Planning (Local Planning) (England) Regulations 2012.</p> <p>The Leveling up and Regeneration Act (2023) allows for the provision of regulations that will alter the way that Local Development Framework Documents (LDF) are produced. On 27th November 2025 the Government issued guidance on what to expect within the regulations when they are brought into force in spring 2026. Of note is that the Government intends to set out that SPDs cannot be adopted after the 30th June 2026. If this deadline is not achieved then it could be argued that the SPDs do not hold weight and cannot be relied upon in decision making.</p>
Single Impact Assessment	<p>During the preparation of the HMO Article 4 Direction and the draft SPDs along with during the consultation process, officers had regard to the protected characteristics within the Equality Act (2010) and the additional protected characteristics that HBC deem appropriate (care leavers, armed forces and poverty and disadvantage). Officers are of the view that the drafts, consultation and thus update reports have no negative impact upon protected characteristics but could in fact have a positive impact upon some protected characteristics by virtue of creating a better environment for all, including those with protected characteristics.</p>
Staff Considerations	None
Asset Management Considerations	None
Environment, Sustainability and Climate Change Considerations	<p>The report provides an update following the closure of the consultations and provides a time frame on when members are likely to be asked to progress the HMO Article 4 Direction and/or adopt the SPDs. This report has no environmental, sustainability and climate change impact however any such will be discussed when the HMO Article 4 Direction and SPDs are discussed at a future committee.</p>
Consultation	<p>Statutory consultation on the HMO Article 4 Direction is ongoing but will cease on 24th September 2026. The information seeking consultation has now closed.</p>

	<p>Consultation on the two SPDs ran from 3rd November to 16th January 2026. A second consultation is proposed from 3rd March for an 8 week period.</p> <p>Engagement with residents and key stakeholders will continue in an informal and ad hoc manner up until the implementation of the HMO Article 4 Direction.</p>
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6. Recommendations

- 6.1. Members note the content of the report.
- 6.2. Members allow officers to consult upon the two SPDs for an eight week period.
- 6.3. Members agree to delegate to the Director of Neighbourhood and Regulatory Services, to consider representations and make modifications to the SPDs, in consultation with the Committee Chair.
- 6.4. That Members agree to permit the modified SPDs to be presented to Full Council for adoption In May/June 2026.

7. Reasons for Recommendations

- 7.1. To ensure members are kept informed of SPD progress, to ensure that the correct permission is in place prior to consultation and to ensure that the SPD can be adopted in June 2026.

8. Background Papers

- 8.1. Neighbourhood Services Committee report 30th June 2025 (Article 4 Direction).
- 8.2. Neighbourhood Services Committee report 20th October 2025 (Two SPDs).

9. Contact Officers

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Sign Off:-

Chief Executive	Date: 10/02/2026
Director of Finance, IT and Digital	Date: 06/02/2026
Director of Legal, Governance and HR	Date: 19/02/2026



Neighbourhoods and Regulatory Services Committee

2 March 2026

Report of: Director of Neighbourhoods and Regulatory Services

Subject: AUTHORITY'S MONITORING REPORT (AMR) 2024/25

Decision Type: Non-key decision

1. Council Plan Priority

Hartlepool will be a place:
- where people live healthier, safe and independent lives. (People)
- that is connected, sustainable, clean and green. (Place)
- that is welcoming with an inclusive and growing economy providing opportunities for all. (Potential)
- with a Council that is ambitious, fit for purpose and reflects the diversity of its community. (Organisation)

2. Purpose of Report

2.1 The purpose of this report is to inform Neighbourhood Services Committee of the Hartlepool Local Plan Authority's Monitoring Report (AMR) 2024/25 (**Appendix 1**).

3. Background

3.1 The AMR is a statutory Local Development Framework (LDF) document produced annually by Planning and Development Services. It

assesses the effectiveness of local plan policies and the extent to which they are being implemented.

4. Proposals

Housing

- 4.1 The local plan evidence base identifies an Objectively Assessed Need (OAN) need of 287 dwellings per annum. For this reporting period the local plan housing trajectory sets a baseline delivery target of 400 dwellings per annum. This housing requirement is set higher than the OAN of 287 dwellings per annum as replacement of demolitions, previous under delivery, aspirations of the Tees Valley Combined Authority and a 20% buffer have been applied
- 4.2 This year, there is a net delivery of 470 dwellings (i.e. 467 new builds and 3 change of use). The net delivery is above both the baseline housing target of 400 and the revised target of 464 dwellings/annum.¹ It is also above the Objectively Assessed Need (OAN) of 287 dwellings/annum, and this is notably positive development again this year, like last year. The cumulative under delivery continues to decline and this year it is -36 compared to last year at -106.
- 4.3 There were a total of 166 new starts in this year, mainly from sites at Wynyard, Upper Warren, Marine Point, Quarry Farm 2, Elwick Park, Antler Park and Woodside Meadows Seaton. Completions from these will add to next year's net completions and hence it is anticipated that the net annual delivery target will continue to be met and help reduce the cumulative under-delivery.
- 4.4 A total of 93 completed affordable homes was achieved this year and it remains higher than the local plan target of 74 affordable homes. The completions were mainly from existing large housing sites including Marine Point, Upper Warren, Woodside Meadows and Antler Park. A total of £363,000.00 section 106 funds were received for offsite affordable housing this year and £59 000 was spent on affordable housing schemes.
- 4.5 In this year section 106 agreements have secured a total of 67 onsite affordable homes and a financial contribution of £9,641.00. It is anticipated there will be more delivery of affordable homes throughout next year from large housing sites as they continue to build out at pace.
- 4.6 The 400 dwellings per year figure, the overall average annual target over the 15 year plan period of 410 dwellings and the higher figure of

¹ When considering the March 2025 five-year housing land supply that factors in housing backlog and a with the 5% buffer.

464 are all still considered to be achievable meaning that there is currently no need to revise the local plan housing policies.

Economy, Employment, Town Centre and Tourism

- 4.7 For the third year running, there is no net employment land uptake. However, there is 30.7 ha loss of allocated employment land to housing at Wynyard (application ref H/2022/0181). Available employment land has thus reduced to 164.5 ha from 195.2ha. There is no major negative, unjustified diversion of employment policies, the loss of employment land at Wynyard was justified via the planning application and therefore there is no need to amend any of the employment policies.
- 4.8 Overall, as illustrated by the assessment above, all policies are performing in accordance with the local plan. Policies will continue to be monitored annually throughout the local plan period to ensure they are being implemented and that targets are also being met as planned and any poor performing or diverting policies will be flagged up and recommendations given.
- 4.9 For the past 6 years the town centre vacancy rate in terms of floorspace has been increasing. This year it is 24.8%, up from 19.6% last year. Lack of high-quality shops, vacant units remain and continued closure of shops remain a challenge. At the time of writing this report, Boyes departmental store opened, but the additional floor space will be reported next year, and this should reduce vacancy rates next year provided existing open shops do not close.
- 4.10 Although the vacancy rate is higher this year, the retail and commercial policies are still considered to be performing as expected since the town centre continues to develop as a retail hub with E and F1 uses accounting for more than 50% of all uses in the town centre, followed by F2 uses in accordance with the local plan. This indicates that appropriate planning permissions are being granted for town centre uses within the town centre and there is therefore no evident policy diversion. There is currently no need to amend any of the town centre retail and commercial policies as they continue being implemented effectively.
- 4.11 Tourism policies within the local plan identify the Town Centre, the Marina, the Headland and Seaton Carew as main tourism destinations and the policies encourage appropriate developments within these areas. However, rural tourism through recreation and leisure developments could be allowed under certain circumstances. There have been no approved tourist-related planning applications this year. Building at the former Jackson's Landing is progressing well at the Marina (i.e. erection of a Active Wellbeing Hub including swimming pools, fitness suites, studios, cafe and ancillary spaces, external landscaping including public events space, car parking, vehicle drop off/collection and infrastructure improvements). Tourism policies

continue to be implemented and there is currently no need to update them.

Environmental Quality

Natural Environment

- 4.12 Whilst data is collected on the loss or creation of biodiversity habitat it was not available at the time of writing the report. However policy NE1 continues to be implemented as applications are securing biodiversity improvements, but these take time to come to fruition hence the lack of data this reporting year. There were no losses or gains in the total area of habitat in international or national designated sites. Planning approval and compliance will continue to be closely monitored to ensure compensation/mitigation measures are appropriately put in place to avoid losses of wildlife habitat as this is detrimental to the natural environment and biodiversity gain.
- 4.13 The quantity and type of green infrastructure lost or gained is not routinely measured or monitored, therefore no definitive measurement can be provided. However, funding for green infrastructure is secured through S106 developer contributions from planning applications and this can be routinely measured. This year a total of £70,000 has been received for green infrastructure and £58,000 for ecological mitigation.
- 4.14 Despite the lack of habitat data in this reporting year, officers are of the view that the natural environment policies are being implemented, habitat mitigation is being secured as is green infrastructure and green spaces continue to be protected with development on such space strictly prohibited, therefore there is currently no need to update them.

Countryside/Rural Area and the Historic Environment

- 4.15 The council continues to monitor applications for developments in the countryside, in particular isolated residential developments outside development limits through the adopted New Dwellings Outside of Development Limits SPD. There were no permitted residential developments in the outside development limits and no rural proposals that were contrary to policy therefore it is considered that the rural development policies continue to be implemented effectively and do not require amending.
- 4.16 The council published a total of 10 historic buildings at risk this year. This highlights there are vulnerable heritage assets across the borough that still need attention. There has been no Conservation Area Appraisal this year but work is still on-going on reviewing the Seaton Carew Conservation Area Appraisal.

Waste and Renewable Energy

- 4.17 The total amount of waste arising this year increased in comparison to the previous year and recycled/composted waste slightly decreased from 27.71% last year to 27.2% this year. The increase in waste could be attributed to more properties being serviced because of more house completions and families moving in during the year. The council continues to communicate with residents and the wider community via *Hartbeat* magazine, the council's own website and social media, to help them to recycle more of their waste. A new officer will also be recruited to help drive up recycling rates.
- 4.18 There were no approved planning applications for renewable energy this year. At the time of writing the AMR it was noted that a council motion was discussed on 8th May 2025 with regard to the protection of residential amenity from inappropriately sited onshore wind turbines. The AMR sets out that the key policy for the determination of wind turbine development is policy CC4 (Strategic wind turbine developments), policy CC4 allocates two areas for strategic wind turbine development within the borough and sets out parameters in relation to capacity and tip height. The policy does not set out separation distances between turbines and dwellings as this is not supported by Government policy as such an approach does demonstrate positivity and flexibility. It is considered that the policy is robust and fit for purpose, it is flexible yet still ensures that any impacts upon residential amenity are addressed on a case by case basis.

Sustainable Transport and Access to the Countryside

- 4.19 The local plan makes provision for the continued improvement of roads and development of a comprehensive network of cycle routes linking the main areas of the borough. The programme of cycleway improvements supported under the Tees Valley Combined Authority (TVCA) Local Growth Fund (LGF) 'Sustainable Access to Employment' programme concluded in 2021. The programme saw seven major projects completed linked to the Hartlepool Cycling Development Plan which is a constantly evolving document (www.hartlepool.gov.uk/cycleplan).
- 4.20 Following the successful LGF programme the council is currently working with TVCA on the Tees Valley Local Cycling and Walking Infrastructure Plan (LCWIP). This is a Tees Valley wide plan which includes Hartlepool. Key projects will be the development of cycling corridors. Hartlepool schemes currently being explored are as follows:
- Transport Interchange to Headland (LCWIP scheme via City Region Sustainable Travel Settlement funding).
 - Transport Interchange to Wolviston (LCWIP via CRSTS).
 - A689 to Summerhill (LUF).
 - A689/Victoria Road to Mill House area (LUF/MDC funding)

- Town Centre – Waterfront (LUF)

- 4.21 The above schemes are in various stages of development with 3 having been submitted to an Active Travel England Design Review Panel, and it is anticipated they will be onsite by spring 2026.
- 4.22 In addition to these schemes the transport department is looking at carrying out improvements to the existing cycleway along the full length of Catcote Road (LUF). There is a proposal to implement a footway cycleway replacing the existing footway at the A689 southwest development access to Dalton back Lane (section 106 works) and an informal crossing across the A689 to Greatham Back Lane
- 4.23 No new roads have been constructed this year, however, there is one new cycleway constructed at the A689 Sappers Corner to Southwest Extension (Greatham Meadows) development access (section 278 works). It replaces the existing footway and is 472 metres long. At the A179 / A1086 / West View Road roundabout there has been a provision of 2 toucan crossings with pedestrian / cycleway links to existing pedestrian/ cycleway routes. (section 278 works).
- 4.24 There has been 1.08 km of public rights of ways and 0.55 km of permissive paths created this year. Policy INF1,2 continues to be implemented and moving in the right direction hence there is no need for the policies to be reviewed. The council will continue to ensure the provision of a safe, efficient and sustainable transport network, accessible to all and to strengthen transport links with the Tees Valley sub-region, northeast region and beyond.

Neighbourhood Plans, Community Infrastructure Levy, Duty to Cooperate and S106 Developer Contributions

- 4.25 The Rural Neighbourhood Plan forms part of the development plan for the borough. Work on the Wynyard and Headland Neighbourhood Plans ceased several years ago with no appetite from the community to progress them. In fulfilling the requirements of section 33A of the Planning and Compulsory Purchase Act 2004 (Duty to co-operate in relation to planning of sustainable development) and as set out in the Town and Country Planning (Local Planning) (England) Regulations 2012, part 2, duty to cooperate, (4) (1); the council has continued to cooperate and actively engage with public bodies, especially the Tees Valley Combined Authority to develop strategic policies.
- 4.26 Whilst the council will keep the situation under review, the present stance is that the Community Infrastructure Levy (CIL) will not be taken forward within the borough. **Appendix 1** shows details of breakdown of section 106 funds; amounts received/secured, where they should be spent and the type of development they should be spent on.

4.27 Overall, as illustrated above and set out in the AMR, all policies are performing in accordance with the local plan. There are no evident policy diversions although available employment land at Wynyard has decreased, this was justified at application stage and would not warrant a policy update at this time. Policies will continue to be monitored annually throughout the local plan period to ensure they are being implemented and that targets are being met. Any poor performing or policy diversion will be reported and recommendations given.

5. Other Considerations/Implications

RISK IMPLICATIONS	None
FINANCIAL CONSIDERATIONS	None
SUBSIDY CONTROL	None
LEGAL CONSIDERATIONS	The AMR has been produced in accordance with Part 8, 34 (3) of the Town and Country Planning (Local Planning) (England) Regulations 2012.
SINGLE IMPACT ASSESSMENT	None – the AMR is a factual based report that assesses the implementation of the local plan.
STAFF CONSIDERATIONS	None.
ASSET MANAGEMENT CONSIDERATIONS	None.
ENVIRONMENT, SUSTAINABILITY AND CLIMATE CHANGE CONSIDERATIONS	This report advises on the implementation of the local plan and considers all policies to be robust and thus environment, sustainability and climate change considerations are being factored into planning decisions.
CONSULTATION	Consultation has not been undertaken, nor is it legally required to be undertaken.

6. Recommendations

- 6.1 That Members note the content of the report and endorse it as part of the Local Development Framework.

7. Reasons for Recommendations

- 7.1 The council is legally required to produce an AMR at least annually. The AMR provides a detailed assessment of planning policy implementation and assists in identifying policies that are not robust and either need amending to ensure that they are implemented or they are in future excluded from the local plan.

8. Background Papers

- 8.1 Hartlepool Local Plan (2018)
https://www.hartlepool.gov.uk/downloads/file/4393/hartlepool_local_plan_-_adopted_may_2018pdf
- 8.2 2024 Housing review and 5 Year Housing Land Supply Report
[5 Year Housing Land Supply Review | Hartlepool Borough Council](#)

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Sign Off: -

Chief Executive	Date: 09/02/2026
Director of Finance, IT and Digital	Date: 06/02/2026
Director of Legal, Governance and HR	Date: 06/02/2026



Hartlepool
Borough Council



Hartlepool Borough Council Local Plan

Authority Monitoring Report 2024/25

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EXECUTIVE SUMMARY

Introduction

This Authorities Monitoring Report (AMR) covers the financial year 2024/25 and is the seventh AMR of the 2018 local plan (adopted May 2018). The local plan covers the period 2016 to 2031, the assessment of policies covers this period.

The AMR is produced by Planning and Development Services on behalf of the council and relates to the period 1st April 2024 to 31st March 2025. It reviews the progress made on the implementation of the Local Development Scheme (LDS) and assesses the effectiveness of planning policies within the local plan and the extent to which they are being implemented. The LDS that relates to the adopted local plan was produced in December 2017, the LDS relating to delivery of a future local plan was produced in March 2025.

The planning policies assessed in this report, are those of the 2018 Hartlepool Local Plan. A list of the local plan policies is shown in appendix 1. Chapter 4 of this report details how the local plan policies have, overall, been effective in both the management of planning proposals and in the economic, social and environmental development of the borough.

Housing

In accordance with Part 8, 34 (3) of the Town and Country Planning (Local Planning) (England) Regulations 2012, Chapter 4 includes annual numbers of net additional dwellings which have been allocated in the local plan policy.

The local plan identifies an objectively assessed need (OAN) of 287 dwellings per annum which considers the predicted household projections over 15 years up to 2031. Added to the OAN is a backlog from previous years and aspirations of the Tees Valley Combined Authority to create more jobs and overcome economic development barriers of which housing is one as outlined in their Strategic Economic Plan (2016-2026). The average number of dwellings needed is 410 per annum for the 15-year period. The local plan housing numbers are phased over the 15 years, this allows for planning application and site preparation with lower numbers expected in years 1-5 increasing in years 6 to 10 and increasing further in years 11 to 15. This year the local plan sets a baseline delivery target of 400 dwellings per annum, and this is the second phase of the plan period (year 7 of 15). It also factors in a replacement of demolitions on housing market renewal (HMR) sites and an additional 20% affordable housing buffer.

This year, there is a net delivery of 470 dwellings (i.e. 467 new builds and 3 change of use). The net delivery is lower than last year but is above both the baseline housing target of 400 and the revised target of 464 dwellings/annum¹. It is also above the Objectively Assessed Need (OAN) of 287 dwellings/annum, and

¹ When considering the March 2025 five-year housing land supply that factors in housing backlog and a with the 5% buffer.

this is notably positive development again this year, like last year. The cumulative under delivery continues to decline and this year it is -36 compared to last year at -106.

There was a total of 166 new starts mainly from greenfield sites such as at Wynyard, Upper Warren, Marine Point, Quarry Farm 2, Elwick Park, Antler Park and Woodside Meadows in Seaton. Completions from these will add to next year's net completions and hence it is anticipated that the net annual delivery target will still be met and help reduce the cumulative under-delivery.

A total of 93 completed affordable homes (all new builds) was achieved this year and it remains higher than the local plan target of 74 affordable homes. The completions were mainly from existing large housing sites such as the Marine Point, Upper Warren, Woodside Meadows and Antler Park. A total of £363,000.00 S106 funds was received for offsite affordable housing this year (appendix 5) and £59 000 was spent on affordable housing schemes. Through the year, signed S106 agreements have secured a total of 67 onsite units (this includes 42 Discount Market Value (DMV) properties and £9,641.00 in money (appendix 6). It is anticipated there will be more delivery of affordable homes throughout next year from large housing sites as they continue to build out.

The overall averaged annual target of 400 dwellings is still considered to be an achievable figure meaning that there is currently no need to revise it or any of the housing policies. Housing policies are continually being implemented in accordance with the local plan and the housing targets are still considered to be achievable. There is therefore currently no need to revise any of the housing policies or any of the housing target figures.

Economy, Employment, Town Centre and Tourism

For the third year running, there is no net employment land uptake. However, there is 30.7 ha loss of allocated employment land to housing at Wynyard (application ref H/2022/0181). Available employment land has thus reduced to 164.5 ha from 195.2ha. There is no major negative, unjustified diversion of employment policies since precedence has been set for housing at Wynyard in previous years. There is therefore no need to revise and amend any of the employment policies.

For the past 6 years the vacancy rate in terms of floorspace has been increasing in the town centre. This year it is 24.8%, up from 19.6% last year. Lack of high-quality shops and vacancy rates in the town centre remains a challenge. Several shops closed, and this has significantly increased the vacancy rate this year. At the time of writing this report, Boyes departmental store opened doors to the public, but the additional floor space will be reported next year, and this will reduce vacancy rates next year provided existing open shops do not close.

Although the vacancy rate is higher this year, the retail and commercial policies are still considered to be performing as expected since the town centre continues to develop as a retail hub with E and F1 uses accounting for more than 50% of all uses in the town centre, followed by F2 uses in accordance with the local plan.

This indicates that appropriate planning permissions are being granted for town centre uses within the town centre and there is therefore no evident policy diversion. There is currently no need to amend any of the town centre retail and commercial policies as they continue being implemented

Tourism policies within the local plan identify the Town Centre, the Marina, the Headland and Seaton Carew as main tourism destinations and the policies encourage appropriate developments within these areas. However, rural tourism through recreation and leisure developments could be allowed under certain circumstances. Same as last year, there are no approved tourist-related planning applications this year. Building at the former Jackson's Landing for a new leisure centre is progressing well at the Marina. Tourism policies continue to be implemented and there is currently no need for an update.

Environmental Quality

Natural Environment

This year there has been no available data with regard to recorded loss or creation of biodiversity habitat. There were no losses or gains in the total area of habitat in international or national designated sites. It is still recommended that housing applications are more closely monitored to ensure compensation/mitigation measures are appropriately put in place to avoid losses of wildlife habitat as this is detrimental to the natural environment and biodiversity gain. Notwithstanding the above the natural environment policies are being implemented and there is currently no need to update them.

The quantity and type of green infrastructure lost or gained is not routinely measured or monitored, therefore no definitive measurement can be provided. However, funding for green infrastructure is secured through S106 developer contributions from planning applications and this can be routinely measured (appendices 5 and 6).

Countryside/Rural Area and the Historic Environment

The council continues to monitor applications for developments in the countryside, in particular isolated residential developments outside development limits through the New Dwellings Outside of Development Limits Supplementary Planning Document (SPD). There were no permitted unjustified residential developments in the countryside outside development limits hence the rural development policies continue to be implemented.

The council published a total of 10 historic buildings at risk this year. This highlights there are vulnerable heritage assets across the borough that still need attention. There has been no Conservation Area Appraisal this year but work is still on-going with regard to the Headland and Seaton Carew Conservation Area Management plans.

Waste and Renewable Energy

The total amount of waste arising this year increased in comparison to the previous year. The increase in waste could be attributed to more properties being serviced because of more house completions and families moving in during the year. Recycled/composted waste has decreased marginally from 27.71% last year to 27.2% this year. The council continues to communicate with residents and the wider community via Hartbeat magazine, the council's own website and social media, to help them to recycle/compost more of their waste. At the time of writing HBC are in the process of rolling out a new recycling programme, this will be reported in the next AMR and it is anticipated that recycling/composting will increase.

There were no approved planning applications for renewable energy this year. The two key areas for wind turbine development continue to be supported by robust policies that can be relied upon to determine any applications within them locations.

Sustainable Transport and Access to the Countryside

The local plan makes provision for the continued improvement of roads and development of a comprehensive network of cycle routes linking the main areas of the borough. The programme of cycleway improvements supported under the Tees Valley Combined Authority (TVCA) Local Growth Fund (LGF) 'Sustainable Access to Employment' programme concluded in 2021. The programme saw seven major projects completed linked to the Hartlepool Cycling Development Plan which is a constantly evolving document (www.hartlepool.gov.uk/cycleplan).

Following the successful LGF programme the council is currently working with TVCA on the Tees Valley Local Cycling and Walking Infrastructure Plan (LCWIP). This is a Tees Valley wide plan which includes Hartlepool. Key projects will be the development of cycling corridors.

Hartlepool schemes currently being explored are as follows:

- Transport Interchange to Headland (LCWIP scheme via City Region Sustainable Travel Settlement funding).
- Transport Interchange to Wolviston (LCWIP via CRSTS).
- A689 to Summerhill (LUF).
- A689/Victoria Road to Mill House area (LUF/MDC funding)
- Town Centre – Waterfront (LUF)

The above schemes are in various stages of development with 3 having been submitted to an Active Travel England Design Review Panel, and it was anticipated that all will be on site by autumn 2025. However, at the time of writing the report, these schemes are not on site yet and start date has been pushed back to winter / spring 2026.

In addition to these schemes HBC transport department are looking at carrying out improvements to the existing cycleway along the full length of Catcote Road (LUF). There is a proposal to implement a footway cycleway replacing the existing footway at the A689 Southwest Extension (Greatham Meadows) development

access to Dalton Back Lane and an informal crossing across the A689 to Greatham Back Lane.

No new roads have been constructed this year, however, there was one new cycleway constructed at the A689 Sappers Corner to Southwest Extension development access. It replaces the existing footway and is 472 metres long. At the A179 / A1086 / West View Road roundabout there has been a provision of 2 toucan crossings with pedestrian / cycleway links to existing pedestrian/ cycleway routes.

There has been 1.08 km of public rights of ways and 0.55 km of permissive paths created this year. Policy INF1 and INF2 continue being implemented and moving in the right direction hence there is no need for the policies to be reviewed. The council will continue to ensure the provision of a safe, efficient and sustainable transport network, accessible to all and to strengthen transport links with the Tees Valley sub-region, northeast region and beyond.

Neighbourhood Plans, Community Infrastructure Levy, Duty to Cooperate and S106 Developer Contributions

The Rural Neighbourhood Plan forms part of the development plan for Hartlepool and is available on the council website. Work on the Wynyard Neighbourhood Plan and the Headland Neighbourhood Plan has ceased with no known desire to recommence.

In fulfilling the requirements of section 33A of the Planning and Compulsory Purchase Act 2004 (Duty to co-operate in relation to planning of sustainable development) and as set out in the Town and Country Planning (Local Planning) (England) Regulations 2012, part 2, duty to cooperate, (4) (1); the council has continued to cooperate and actively engage with public bodies including the TVCA to develop strategic policies. Further details are available in appendix 3 of this report.

Whilst the council will keep the situation under review, the present stance is that the Community Infrastructure Levy (CIL) will not be taken forward within the borough. Further details on CIL are in appendix 4 of this report. Appendices 5 and 6 show the breakdown of the S106 funds; amounts received/secured, where they should be spent and the type of development they should be spent on.

Overall, as illustrated by the assessment above, all policies are performing in accordance with the local plan. There are no major policy diversions although 30.7ha of employment land was lost to housing at Wynyard reducing borough wide available employment land from 195.2 ha to 164.5 ha. Policies will continue to be monitored annually throughout the local plan period to ensure they are being implemented and that targets are also being met as planned and any poor performing or diverting policies will be flagged up and recommendations given.

1. INTRODUCTION

- 1.1 Government legislation requires all Local Planning Authorities to prepare a local plan monitoring report, the Authority's Monitoring Report (AMR). The AMR is prepared in accordance to the provisions of the Localism Act 2011 which have led to Regulation 34 in The Town and Country Planning (Local Planning) (England) Regulations 2012 prescribing minimum information to be included in monitoring reports, including net additional dwellings, net additional affordable dwellings, community infrastructure levy receipts, the number of neighbourhood plans that have been adopted, and action taken under the duty to co-operate. In essence it is a matter for each local planning authority to decide what to include in their AMR over and above the prescribed minimum information as outlined in The Town and Country Planning (Local Planning) (England) Regulations 2012.
- 1.2 This report is based on the ongoing monitoring of the local plan over the financial year (2024/2025) and will assist the council to spatially plan better for the borough. Where policies are failing the council will seek to find out why and look to address them so that they perform more appropriately in the future.

Planning Legislation

- 1.3 The Planning and Compulsory Purchase Act 2004 introduced a system of the Local Development Framework (LDF). The LDF comprises a portfolio of Local Development Documents which together deliver the spatial planning strategy for Hartlepool (see Diagram 1 below). Some documents are known as Local Development Documents (LDDs) and include Development Plan Documents (DPDs), Supplementary Planning Documents (SPDs) and Neighbourhood Plans.² The LDDs set out the spatial planning strategy for Hartlepool. The 2012 regulations³ set out what each LDF document should contain and the formal process they should go through.
- 1.4 The other documents that are within the LDF system, but are not termed LDDs, are:
- The Local Development Scheme (LDS) which sets out the programme for preparing LDDs.
 - The Statement of Community Involvement (SCI) which sets out how the council will involve residents and other interested persons and bodies in the planning process; and
 - The Authorities Monitoring Report⁴ (AMR) which assesses the implementation of the Local Development Scheme, the extent to which policies in the LDDs are being achieved, provides information with regard to CIL and sets out how the council has cooperated with other Local Authorities and relevant bodies.⁵

² Schedule 9, part 2 (6) (b) of the Localism Act amends 38 (3) of the Planning and Compulsory Purchase Act 2004 to include Neighbourhood Plans as LDD's.

³ Town and Country Planning (Local Planning) (England) Regulations 2012

⁴ Formally termed the Annual Monitoring Report in line with the Town and Country Planning (Local Development) (England) Regulations 2004, as amended.

⁵ Part 2, 4 of the Town and Country Planning (Local Planning) (England) Regulations 2012 sets out the bodies that the council must cooperate with.

Diagram 1: Hartlepool Local Development Framework

LOCAL DEVELOPMENT FRAMEWORK A portfolio of local development and other documents															
Local Development Documents										Other Documents					
Development Plan Documents				Supplementary Planning Documents											
Hartlepool Local Plan	Tees Valley Minerals & Waste DPD	Hartlepool Local Plan Policies Map	Neighbourhood Plans	Travel Plans and Transport Assessments SPD	Hartlepool Green Infrastructure SPD	Trees and Development SPD	Planning Obligations SPD	Shop Fronts SPD	Green Infrastructure SPD	Residential Design SPD	New Dwellings outside of Development Limits SPD	Seaton Carew Masterplan SPD	Public Rights of Way and Other Access SPD	Statement of Community Involvement	Authorities Monitoring Report
These documents will comprise the Development Plan for the area.				These documents help to give further information and detail to support the Development Plan Documents.								These Documents and the highlighted Development Plan Documents must be prepared.			

The Authorities Monitoring Report

1.5 Local planning authorities are required to examine certain matters in their Monitoring Reports.⁶ The key tasks for this monitoring report are as follows:

- Review progress in terms of the preparation of documents specified in the Local Development Scheme against the timetable and milestones in the scheme, identify any delays together with the reasons and setting out a timetable for revising the scheme (section 3).
- Assess the extent to which local plan policies are being implemented, including any justification for no implementation and identify steps that the council intend to take to secure policy is implementation. (Section 4).

⁶ Part 8 Town and Country Planning (Local Planning) (England) Regulations 2012.

- Contain details of any Neighbourhood Development Order or a Neighbourhood Development Plan that are being prepared or have been made (Appendix 4).
- Provide information regarding the progress of the Community Infrastructure Levy (appendix 5).
- Provide information regarding who the council has cooperated with in relation to planning of sustainable development (appendix 6).

1.6 In terms of assessing the implementation of policies, the Authorities' Monitoring Report should:

- identify whether policies need adjusting or replacing because they are not working as intended; identify any policies that need changing to reflect changes in national or regional policy; and
- set out whether any policies are to be amended or replaced.

1.7 In order to assess the effectiveness of planning policies, it is important to set out the social, economic and environmental context within which the policies have been formulated, the problems and issues they are intended to tackle, and the opportunities of which advantage can be taken to resolve such problems and issues. Section 2 of this report considers the key characteristics of Hartlepool and the matters to be addressed.

1.8 This report, considers the policies of the Hartlepool Local Plan adopted and the Tees Valley Minerals and Waste Core Strategy and the Policies and Sites DPD adopted in September 2011.

2 HARTLEPOOL – KEY CHARACTERISTICS, STATISTICS, PROBLEMS AND CHALLENGES FACED

- 2.1 The key contextual indicators used in this chapter describe the wider characteristics of the borough and provide the baseline for the analysis of trends, as these become apparent and for assessing in future authorities' monitoring reports, the potential impact future planning policies may have had on these trends.

Hartlepool & the Sub-regional Context.

- 2.2 The borough forms part of the Tees Valley along with the boroughs of Darlington, Middlesbrough, Redcar & Cleveland and Stockton-On-Tees.
- 2.3 Hartlepool is an integral part of the Tees Valley region. It is a retail service centre serving the borough and parts of County Durham, in particular Easington, Wingate and Peterlee. Over recent years the borough has developed as an office and tourism centre. The development of Seaton Carew Sea front, the Maritime Experience and the Marina forms an important component of coastal regeneration exploiting the potential of the coast as an economic and tourist driver for the region.

Hartlepool in the Local Context

- 2.4 The original settlement of Hartlepool dates to Saxon times. Originally an important religious settlement the town's early development resulted from the existence of a safe harbour and its role as a port for the city of Durham and subsequent grant of a Royal Charter from King John in 1201. The town as it is today has grown around the natural haven which became its commercial port and from which its heavy industrial base developed.
- 2.5 Hartlepool covers an area of approximately 9400 hectares (over 36 square miles). It is bounded to the east by the North Sea and encompasses the main urban area of the town of Hartlepool and a rural hinterland containing the five villages of Hart, Elwick, Dalton Piercy, Newton Bewley and Greatham. The main urban area of Hartlepool is a compact sustainable settlement with many of the needs of the residents in terms of housing, employment, shopping and leisure being able to be met within the borough. The Durham Coast railway line runs through the centre of the town and connects Hartlepool to Newcastle, the rest of Tees Valley, York and London. The A19 trunk road runs north/south through the western rural part of the borough, the A19 and the A1 (M) are readily accessed via the A689 and the A179 roads which originate in the town centre.

Population

- 2.6 The population of Hartlepool continues to increase; from 93,640 last year to 98,180 this year (table 1). This is the largest increase recorded in the past five years, and it indicates that more people moved into Hartlepool this financial year. Despite the increase, at 13.8% Hartlepool still has the lowest proportion of residents in the Tees Valley sub region (table 2).

Table 1: Population

Area	Population				
	2020/21	2021/22	2022/23	2023/24	2024/25
Darlington	107,400	107,800	108,200	108,200	112,489
Hartlepool	93,800	93,800	92,600	93,640	98,180
Middlesbrough	141,300	141,300	143,700	148,285	156,161
Redcar & Cleveland	137,200	137,200	137,000	137,175	139,228
Stockton	197,400	197,400	197,000.	197,348	206,800
Tees Valley Total	677,100	677,100	678,500	684.648	712,858
North East	2,680,800	2,680,800	2,646.800	2,731,853	2,760,678
Great Britain	65,185,700	65,185,700	65,121,700	68,682,962	69,300,000

Source: NOMIS, ONS Official Labour Market Statistics mid-year estimates 2024/25.

Table 2: Population Proportions in the Tees Valley Sub region.

Area	Proportion %				
	2020/2021	2021/2022	2022/23	2023/24	2024/25
Darlington	15.9	15.9	15.9	15.8	15.8
Hartlepool	13.9	13.9	13.6	13.7	13.8
Middlesbrough	20.9	20.9	21.2	21.7	21.9
Redcar & Cleveland	20.3	20.3	20.2	20.0	19.5
Stockton	29.2	29.2	29.0	28.8	29.0
Tees Valley Total	100	100	100	100	100

Source: NOMIS, ONS Official Labour Market Statistics mid-year estimates 2024/25.

2025 Index of Multiple Deprivation (IMD)

- 2.7 At the time of writing this report, the current 2019 IMD has been updated in October 2025. The previous IMD being that of September 2019. The English Indices of Deprivation measure relative levels of deprivation in 32,844 small areas or neighbourhoods, called Lower-layer Super Output Areas (LSOAs), in England.
- 2.8 The deprivation in its broadest sense is measured by assessing indicators relating to income, employment, health and disability, education, skills and training, barriers to housing and services, crime and the living environment and combining them into a single deprivation score for each small area in England. This allows each area to be ranked relative to one another according to their level of deprivation.

2.9 For Hartlepool, key points include:

- Average Rank of LSOAs: Hartlepool has improved slightly, moving from the 25th most deprived local authority in 2019 to the 30th in 2025. (This isn't widely used)
- Average Score Rank (commonly used summary): Hartlepool has worsened, moving from 10th most deprived to 6th most deprived nationally.
- Proportion of LSOAs in the Most Deprived 10% Nationally:
- In 2019: 36% of Hartlepool's LSOAs (10th highest nationally)
- In 2025: 42% of LSOAs (now 3rd highest, behind Middlesbrough and Birmingham).

NB – the above figures reflect relative deprivation. This does not necessarily mean Hartlepool has become more deprived in absolute terms, but rather that its position has worsened compared to other local authorities since the last release in 2019. It is also worth noting that although this is the 2025 IMD. Many of the underlying indicators are based on data from as early as 2021. A full list of the underlying indicators can be found here:

[https://assets.publishing.service.gov.uk/media/6900df8dab5cc9c8937994e6/File_8_IoD2025_Underlying I...](https://assets.publishing.service.gov.uk/media/6900df8dab5cc9c8937994e6/File_8_IoD2025_Underlying_I...)

Tourism Economy

2.10 As set out in the council Plan 2025 – 2030, the council's vision is that Hartlepool is a place that is “connected, sustainable, clean and green” and “is welcoming with an inclusive and growing economy providing opportunities for all”. The council published its first Town Investment Plan in 2020, followed by a Town Centre Masterplan in 2021 which set out the priorities for investment and the framework for a pipeline of regeneration which is successfully in delivery.

2.11 The town has strong attractions including a multi-million-pound 500 berth marina, arts and cultural venues, a comprehensive range of eating establishments predominantly situated in Hartlepool Marina, and historic visitor attractions. Hartlepool successfully hosted the internationally renowned Tall Ships Race in 2023, and the National Museum of the Royal Navy in partnership with the council continues to develop exciting expansion plans as part of a Waterfront Regeneration programme. The Museum of Hartlepool is in the design stages of being reimagined and expanded over the coming years, and the restoration of the historic paddle steamer ship, PS Wingfield Castle, on the waterfront is underway as the council invests and expands attractions and events to significantly boost visitor numbers.

2.12 The Town's Masterplan includes “Creating a new heart for Hartlepool” and the council is delivering £25m of renewal through five high profile, place-making capital projects under the government's Town Deal programme as part of a transformational programme of change. 2024/25 saw the successful completion of both a new Fabrication and Welding Institute at Hartlepool College of Further Education and a Civil Engineering Skills Academy on Brenda Road as part of new significant skills infrastructure. These two facilities have already enrolled and are training over 1900 learners. Progress continued restoring the derelict former

Wesley Chapel into a high-end boutique hospitality venue which will complete later in 2025, and this catalytic restoration project has in part led to private, external investment in adjacent vacant assets including hotel accommodation and business workspace. The new state of the art Health & Social Care skills academy launched in September 2024 continues to exceed its targets to train and qualify the next generations of skilled health professionals with over 800 students enrolled.

- 2.13 Proposals to Reimagine Middleton Grange Shopping Centre and create town centre multi-functional spaces remain in development in partnership with the Hartlepool Development Corporation with redevelopment expected to begin in Spring 2026. A further 8 phases of improvement around the impressive waterfront continue, with improved pedestrian links and cycleways in delivery at several locations and key links, as well as significant investment in wayfinding and legibility. These include new routes linking through to Highlight Leisure Centre opening in 2026, to Maritime Avenue, the train station, visitor attractions and the waterfront which will contribute to tourism and visitor development. These projects are all due for completion in the next 12 months from the time of writing this report.
- 2.14 The council is continuing the re-development of the Hartlepool Waterfront and in 2026 Hartlepool will see the opening of 'Highlight' the brand new, unique leisure and destination facility at Jacksons Landing.
- 2.15 Work continues to deliver a £33.5m 'Screen Industries Production Village' at the heart of the town with a series of vacant assets being restored and re-purposed in addition to a significant expansion of the existing Northern Studios. In partnership with The Northern School of Art and with significant funding from the UK Government and Tees Valley Investment Zone, planning permission has been secured in 2025 through the Hartlepool Development Corporation to deliver a transformational town centre project. This will see the restoration of the Grade II listed Shades Hotel and eight other heritage assets including Victoria Buildings on Tower Street, as well as improved public realm, to provide high end pre and postproduction space and support facilities for film and TV production when completed. The Studios will be expanded and workshop facilities and other infrastructure built, creating a critical mass of TV and film production and providing over 130 new jobs. Early phases of construction start late in 2025 and full main construction in the summer of 2026, with completion expected at the end of 2027.
- 2.16 The development and technical design of the Production Village is already encouraging significant external investment in neighbouring areas, such as the continuing renovation of empty properties in Scarborough Street delivering impressive accommodation, food and beverage offers. In Seaton Carew, the town's seafront has seen successful and popular seasons following the Coronavirus pandemic, with the completion of seafront and environmental improvements. The now vacant Longscar site at the heart of the seafront is earmarked for regeneration and will soon see Master planning and improvements to come in future years.
- 2.17 In terms of connectivity, the restoration and opening of the former redundant

Platform 2 at Hartlepool Station has now been completed providing improved and expanded rail facilities with a new entrance and exit to the Waterfront to follow as a next stage of development. The scheme led by Network Rail includes the installation of a pedestrian footbridge which has opened the new second platform and increased train capacity.

Economy and Employment

2.18 Like last year, the proportion of economically active people in Hartlepool continued to decrease from 70.9% to 69.2% this year (table 3). Concurrently, the number of economically inactive people, unemployed and workless households increased. In comparison to other local authorities in the Teesside, Hartlepool performs only better than Middlesbrough with Darlington remaining at the top.

Table 3: Labour Supply.

Area	Economically Active (16+ years)		Economically Inactive (16+ years)		Unemployed (16+ years)		Workless Households	
	Number	%	Number	%	Number	%	Number	%
Darlington	52,800	80.4	12,700	19.6	2,300	4.5	3,700	11.5
Hartlepool	40,100	69.2	17,400	30.8	2,100	5.3	5,500	19.1
Middlesbrough	64,200	71.4	25,200	28.6	3,900	6.1	9,200	21.8
Redcar & Cleveland	61,000	74.8	20,100	25.2	2,400	3.8	7,400	19.9
Stockton	99,100	78.0	27,000	22.0	4,200	4.2	8,300	14.9
Tees Valley	63,440	74.8	20,480	25.2	2,980	4.8	6,820	17.4
North East	-	74.0	-	26	-	4.4	154,000	18.5
Great Britain	-	78.5	-	21.5	-	3.9	2,862,100	13.9

Source: ONS Annual Population Survey 2024/25 (numbers are for those aged 16 and over, % are for those aged 16-64).

2.19 Table 4 illustrates the breakdown of percentage of working age population (16-64 years) in employment by main occupation groups. The proportion of people employed as managers, directors, senior officials, professional occupations, caring and leisure all decreased in comparison to last year. The rest of the occupation groups increased in proportion.

Table 4: Percentage of working age population (16-64 years) in employment by main occupation groups.

Socio-Economic Class	Area							
	Darlington	Hartlepool	Middlesbrough	Redcar & Cleveland	Stockton	Tees Valley	North East	Great Britain
Managers, directors, senior officials	8.2	6.0	5.4	7.5	10.6	7.5	8.1	11.1
Professional occupations	21.8	21.3	19.1	22.2	26.4	22.2	24.1	26.6
Associate professional & technical	11.2	12.1	11.7	13.2	14.5	12.5	15.6	15.1
Administrative and secretarial occupations	11.7	9.9	6.2	9.1	6.5	8.7	9.3	9.2
Skilled trades occupations	9.8	9.8	11	8.9	8.7	9.6	9.4	8.5
Caring, leisure & other	11.0	9.0	17.7	13.5	10.8	12.4	9.6	8.6
Sales and customer service occupations	7.2	7.5	8.9	8.3	6.2	7.6	6.6	5.6
Process plant and machine operatives	10.2	12.5	6.8	9.1	5.3	8.8	6.8	5.6
Elementary occupations	8.9	11.6	13.1	7.8	11.1	10.5	10.3	9.1

Source: NOMIS, Official Labour Market Statistics, 2025.

2.20

Under Universal Credit, a broader span of claimants are required to look for work than under Jobseeker's Allowance. As universal credit full service continues to be rolled out, the number of people recorded as being on the Claimant Count in Hartlepool has fallen down this year to 4.8% as shown in table 5.

Table 5: Unemployment Claimant Rate (% of the working age population claiming job seekers allowance).

Area	2021 Claimant Rate	2022 Claimant Rate	2023 Claimant Rate	2024 Claimant Rate	2025 Claimant Rate
Darlington	5.8	4.4	4.3	4.4	3.9
Hartlepool	6.8	5.1	5.1	5.0	4.8
Middlesbrough	8.5	6.0	5.9	6.5	6.1
Redcar & Cleveland	5.7	4.1	4.0	4.1	3.7
Stockton	5.4	4.2	4.0	4.4	4.1
Tees Valley Average	6.4	4.8	4.7	4.9	4.5
North East	5.7	4.1	4.1	4.4	3.9
Great Britain	5.0	3.7	3.7	4.3	4.0

Source: NOMIS, Official Labour Market Statistics, 2025

Health

2.21

Public Health England has been replaced by UK Health Security Agency and Office for Health Improvement and Disparities. They report that the

health of people in Hartlepool is generally worse than the England average. Hartlepool is one of the 20% most deprived districts/unitary authorities in England and approximately 28.6% (5,020) of children live in low-income families. Generally, life expectancy for men is lower than that of women (table 6). In terms of child health, in school Year 6, 26.7 % of children are classified as obese, worse than the average for England.

Table 6: Health statistics

	Darlington	Hartlepool	Middles Borough	Redcar	Stockton	Tees Valley	North east	Great Britain
Health and well being								
Healthy Living Cigarette Smokers (% of adults 18+ years)	7.9	13.5	18.6	13	7.0	12	11	11.9
Healthy weight prevalence in children at Year 6 age	63.3	57	59.2	59.5	60.2	59.8	60.1	-
Obesity prevalence in children at Year 6 age (% of children aged 10-11 classified as living with obesity)	20.7	26.7	25.6	25.2	24.6	24.6	24.5	-
Underweight prevalence in children at Year 6 age (% of children aged 10-11 classified as living with obesity)	1.3	1.4	1.0	1.7	1.2	1.3	1.3	-
Adult obesity prevalence (% of adults classified as living with obesity)	33.3	37.9	35.3	33	36.4	35	32.5	-
Disease and Preventable Mortality								
Cancer diagnosis at Stage 1&2 (new cases as a % of all new cases diagnosed at any known stage 2013-2021)	51.8	49.2	46.8	53.7	53.8	51.6	51.4	-
Preventable cardiovascular mortality age standardised mortality from cardiovascular diseases considered preventable for people under 75 per 100,000 people 2001-3, to 2021-23	31.8	36.4	42.6	34.7	30.2	35.1	-	--
Life Expectancy								
Female healthy life expectancy (average years expected to be lived in good health for females born during the year)	58	53.3	55	56.2	56.9	55.9	57.5	61.9
Male healthy life expectancy (average years expected to be lived in good health for males born during the year)	57.6	53.3	54.2	55.5	56.5	55.4	56.9	61.5

Source: ONS Official Labour Market Statistics 2025

Lifelong Learning and Skills

- 2.22 Table 7 shows qualifications statistics; the Regulated Qualifications Framework (RQF) level attained by the working age population i.e. the 16–64-year age for both males and females. In comparison to last year more people in Hartlepool attained higher level qualifications and those with lower-level qualifications decreased.

Table 7: Qualifications/no qualifications % of working age residents (16-64 years)

Area	Qualifications					No Qualifications
	RQF1+	RQF2+	RQF3+	RQF4+	Other	
Darlington	88.4	86.0	62.3	36.8	5.1	6.5
Hartlepool	84.3	83.1	58.4	34.9	4.0	11.7
Middlesbrough	87.4	82.3	58.5	37.6	3.4	9.2
Redcar	89.8	85.6	60.4	34.2	3.3	6.9
Stockton	89.4	84.7	60.4	40.2	3.1	7.5
Tees Valley	87.9	84.3	60.0	36.7	3.8	8.4
North East	87.1	84.1	62.5	39.4	2.9	10.0
Great Britain	88.9	86.5	67.6	47.2	4.3	6.8

Source: Office for National Statistics, 2025

Housing

Strategic Housing Market Assessment (2015)

- 2.23 The 2015 Hartlepool Strategic Housing Market Assessment (SHMA) study helped inform the production of the council's local plan and housing strategy. The SHMA shows the proportion of dwellings in each housing group as assessed (table 8).

Table 8: Property type and size of occupied dwellings across Hartlepool.

Property Type	No. Bedrooms (table %)					Total
	One	Two	Three	Four	Five/more	
Detached house	0.0	0.6	4.3	7.9	1.7	14.5
Semi-detached house	0.1	5.5	19.9	2.9	0.7	29.2
Terraced/town house	0.0	10.5	16.9	1.7	0.7	29.7
Bungalow	2.5	6.9	2.4	0.2	0.0	12.1
Maisonette	0.8	0.2	0.0	0.0	0.0	1.1
Flat/apartment	7.3	5.0	0.1	0.1	0.0	12.5
Other	0.5	0.4	0.0	0.0	0.0	0.9
Total	11.3	29.2	43.6	12.8	3.1	100

Source: Hartlepool Strategic Housing Market Assessment (2015)

- 2.24 Overall, the vast majority (74.1%) of properties are houses, 12.1% are bungalows, 13.6% are flats/apartments and maisonettes and 0.9% are other types of property including park homes/caravans. Of all occupied properties, 11.3% have one

bedroom, 29.2% have two bedrooms, 43.6% have three bedrooms and 15.9% have four or more bedrooms.

- 2.25 The imbalance in the housing stock is being addressed on a holistic basis through the local plan and the planned future housing market renewal (HMR) initiatives. The HMR initiatives for clearance and improvement will tackle problems associated with the existing older housing stock and new housing development. They will also help to change the overall balance of housing stock and provide greater choice.
- 2.26 Affordability is still a key issue in Hartlepool as highlighted in the 2015 Hartlepool Strategic Housing Market Assessment and the council is continuing to invest in more affordable housing in partnership with private developers and registered providers.

Current House Prices

Table 9: House prices (simple average) and house price index.

Area	£ per House Type				All property types	
	Detached	Semi detached	Terraced	Flat/maisonette	Average Price	House Price Index
Darlington	276,011	170,666	126,615	98,299	156,224	104.4
Hartlepool	234,831	140,856	102,100	76,872	130,749	102.3
Middlesbrough	254,071	151,732	110,609	80,088	141,214	105.9
Redcar	251,561	153,873	117,644	82,824	149,367	103.2
Stockton	278,526	163,018	127,431	89,955	170,296	104.9
Tees Valley	259,000	156,029	116,880	85,608	149,570	104.1
North East	296,226	173,901	141,798	108,914	168,088	115.5
Great Britain	441,341	274,980	231,787	199,844	272,494	103.1

Source: Land Registry (<http://landregistry.data.gov.uk/app/ukhpi>), as at 31st March 2025.

- 2.27 Table 9 shows house prices and House Price Index (HPI) for the five local authorities in the Tees Valley as well as regional and national (i.e. Great Britain). House prices are expressed in terms of simple average price for each house type. The simple average is calculated quarterly taking the sum value of all sales transactions divided by the total number of sales transactions (within a 3-month period). In comparison to last year, house prices in Hartlepool have increased across the board for all house types. However, they remain the lowest and cheapest to purchase.
- 2.28 A House Price Index (HPI) is a weighted, repeat-sales index, meaning that it measures average price changes in repeat sales or re-financings on the same properties. Since the HPI index only includes houses with mortgages within the conforming amount limits, the index has a natural cap and does not account for 'jumbo' mortgages which are for large luxury type of housing. The HPI calculation is based on Land Registry data and the figure is adjusted for sales of differing mixes of house types.

Community Safety

2.29 This year Cleveland Police recorded an overall decrease in crime. Total recorded crime in Hartlepool decreased by 4.6% which equates to 671 less offences than last year. However, it is noted that there was an increase in some crime categories and those will be the main focus for crime reductions throughout next year:

- Robbery (which was up by 33.7%)
- Shoplifting (up 40.9%)
- Weapon Offences (up 28.2%)

NB - The figures above Initially paint a poor picture however, it's not the full story, as we've seen some good gains with Burglary Dwellings down 26%, Theft from Person is down 29.5% and Stalking & Harassment cases are down 22.8%.

Table 10: Notifiable offences recorded by the police 2023/24

	Crime Type	2022/23	*2023/24	Change number)	Change (%)
Publicly Reported Crime (Victim Based Crime)	Violence against the person	11393			
	Homicide	4			
	Death/serious injury by driving	3			
	Violence with injury	1225			
	Violence without injury	1926			
	Stalking and Harassment	2314			
	Sexual Offences	398			
	Rape	141			
	Other sexual offences	257			
	Robbery	106			
	Business robbery	11			
	Personal robbery	95			
	Acquisitive Crime	3840			
	Burglary – residential	686			
	Burglary – business & community	143			
	Bicycle Theft	114			
	Theft from the Person	67			
	Vehicle Crime (Inc Inter.)	609			
	Shoplifting	1331			
	Other Theft	890			
	Criminal Damage & Arson	1577			
	Public Order Offences	1675			
Police Generated Offences (Non - Victim Based Crime)	Drug Offences	355			
	Trafficking of drugs	102			
	Possession/Use of drugs	253			
	Possession of Weapons	107			
	Misc. Crimes Against Society	404			
TOTAL RECORDED CRIME IN HARTLEPOOL		13934			

Source Community Safety, Hartlepool Borough Council.

*2023/24 data breakdown is not available.

The Environment

- 2.30 Hartlepool has a rich environmental heritage and very diverse wildlife habitats. The built, historic and natural environment within Hartlepool plays host to a wide range of buildings, heritage assets including archaeological remains, wildlife habitats, geological and geomorphological features, landscape types and coastal vistas.

The Built Environment

- 2.31 The town has a long maritime tradition and a strong Christian heritage with the twelfth century St Hilda's Church, on the Headland (a Grade I Listed Building) built on the site of a seventh century monastery. Some of the medieval parts of borough, on the Headland are protected by the Town Wall constructed in 1315; the Town Wall is a Scheduled Ancient Monument and Grade I Listed Building. There are eight conservation areas within the borough and 201 Listed Buildings, eight Scheduled Ancient Monuments and one Protected Wreck. One of the town's Victorian parks (Ward Jackson Park) is included on the list of Registered Parks & Gardens.

Geological & Geomorphological Features

- 2.32 The geology of Hartlepool comprises two distinct types:
1. The north of the borough sits on the southern reaches of the Durham Magnesian Limestone Plateau, which is of international geological importance. Although the Magnesian Limestone in Hartlepool is generally too far below the overlying soils to give rise to the characteristic Magnesian Grassland flora found further north, it is exposed in several quarries and road cuttings and forms a spectacular gorge in West Crimdon Dene along the northern boundary of the borough.
 2. The southern half of the borough sits on Sherwood Sandstone from the Triassic period; a rare exposure on the coast at Long Scar & Little Scar Rocks is a Regionally Important Geological Site. Of more recent geological origin is the Submerged Forest SSSI, which underlies Carr House Sands and is intermittently exposed by the tide. This area of waterlogged peat has yielded pollen, mollusc and other remains, which have been used to establish the pattern of sea-level change in Eastern England over the past 5,000 years.

Wildlife Characteristics

- 2.33 The borough is bordered on the east by the North Sea and features extensive areas of attractive coastline including beaches, dunes and coastal grassland. Much of the inter-tidal area of the coast is internationally important for its bird species and is protected as Teesmouth & Cleveland Coast Special Protection Area/Ramsar site. There are nationally protected Sites of Special Scientific Interest at Hart Warren, the Hartlepool Submerged Forest and Seaton Dunes and Common. Other areas of the coast include part of the Teesmouth National Nature Reserve and Sites of Nature Conservation Interest. Hartlepool only has one inland

Site of Special Scientific Interest (SSSI), Hart Bog. This is a small area which has four distinct plant communities and is of particular botanical interest.

- 2.34 The prominent location of Hartlepool Headland, as a first landfall on the east coast, makes it of national significance for the birdwatching community. Inland is an attractive, rolling agricultural landscape including areas of Special Landscape Value. Interspersed in this landscape are a number of fragmented but nevertheless diverse and important wildlife habitats. There are six Local Nature Reserves in the borough and 40 non-statutory geodiversity and biodiversity sites protected as Local Wildlife Sites (LWS) and Local Geological Sites (LGS).
- 2.35 In addition to internationally important numbers of shorebirds, the borough contains some notable examples of wildlife species: grey and common seals are frequent along the coastline with the latter breeding in Seaton Channel. The area of sand dunes, cliffs and grazing marsh along the coast support important species such as burnt tip orchid, northern marsh orchid, early marsh orchid, sea holly and other notable species. There is extensive priority woodland habitat in the Wynyard area and the northern denes.

Bathing Water

- 2.36 Seaton Beach covers an extensive area and attracts significant numbers of visitors for walking, bathing and windsurfing activities. Seaton Carew Centre and Seaton Carew North Gare (south) both meet the Bathing Water Directives guideline standard which is the highest standard and Seaton Carew North passed the imperative standard which is a basic pass.

Air Quality

- 2.37 Air quality in Hartlepool currently meets statutory standards with no requirement to declare any Air Quality Management Areas.

Culture and Leisure

- 2.38 Museums associated with Hartlepool's maritime heritage and other important cultural facilities including the art gallery and Town Hall theatre which are all located within the central part of the borough and comprise a significant focus for Hartlepool's growing tourism economy. In particular, the Hartlepool Maritime Experience is a major regional/national visitor attraction. There are a number of parks and recreation facilities throughout the town and three green wedges that provide important links between the countryside and the heart of the urban areas. On the fringes of the built-up area are three golf courses and a country park at Summerhill.

Future Challenges

2.39 Hartlepool has, over recent years, seen substantial investment, particularly from government funding streams; this investment has completely transformed the environment, overall prosperity and above all Hartlepool's image. The council wish to build on the previous successes but are faced with severe budget cuts. Below is an analysis of the main strengths, weaknesses, opportunities and threats facing the borough.

Table 11: Hartlepool SWOT Analysis

Strengths	Weaknesses	Opportunities	Threats
<ul style="list-style-type: none"> • Successful allocation of Enterprise Zones • Compactness of main urban area • Expanding population • Sense of community / belonging • Partnership working • Good track record in delivering physical regeneration • Diverse, high quality and accessible natural environment • Diverse range of heritage assets including the maritime, industrial and religious • Availability of a variety of high quality housing • Successful housing renewal • High levels of accessibility by road • Lack of congestion • Good local road communications • Direct rail link to London • Good local rail services • Active and diverse voluntary and community sector • Positive community engagement • Successful event management • Small business and SME development • Growth of visitor market • High quality tourist attractions • High quality expanding educational facilities. 	<ul style="list-style-type: none"> • Perceived image • Location off main north-south road corridor • High deprivation across large areas of the town • Low employment rates and high level of worklessness • Legacy of declining heavy industrial base • Small service sector • Imbalance in the housing stock • Shortage of adequate affordable housing • Poor health • Low level of skills • High crime rates • Exposed climate • Range and offer of retail facilities • Reductions in public resources have affected regeneration and employment levels. 	<ul style="list-style-type: none"> • Young population, possible asset for future prosperity • Can improve the economy and the growing house choice thus improving the recent stabilisation of population levels • Availability of land to enable diversification of employment opportunities • Potential for development of major research, manufacturing and distribution facilities on A19 corridor • Potential for further tourism investment • Potential for integrated transport links • Major high quality employment opportunities at Victoria Harbour, Queens Meadow and Wynyard Park • Success of Tall Ships races and opportunity to bid for the event in the future • Plans for development of Tees Valley Metro • Established housing market renewal programme • New state of the art hospital site in Wynyard • Potential New Nuclear Power Station • Renewable Energy and Eco Industries • Developing indigenous business start-up and growth • New government guidance in the form of the NPPF and CIL regulations. 	<ul style="list-style-type: none"> • Closure of major employer/s • Expansion of area affected by housing market failure • Climate change and rising sea levels • Lack of financial resources / budget deficits • Increasing car ownership and congestion • Loss of Tees Crossing Project • Access to New hospital • Competition from neighbouring out of town retail parks • Competition from outlying housing markets • Uncertainty in relation to council budgets • Uncertainty in relation to government funding programmes.

Source Hartlepool Local Plan (2018)

2.40 The main challenges this year and the coming years are similar to those in previous years. In particular Hartlepool is challenged by further public expenditure cuts which are still having to be made, meaning that local services will have to be further scaled down and operated on a more constrained budget. Job losses across the borough are a real threat to the local economy and this is likely to lead

to an increase in the number of people seeking welfare benefits in the coming years. Despite the expenditure cuts Hartlepool will continue to support the development of the local economy and to address the imbalance in the housing stock (including the lack of affordable housing) so as to at least maintain the population at its current level and to ensure that the borough remains sustainable and an attractive place to live, work and play.

- 2.41 Planning policies: enable an improvement in the range of housing available (both through demolition and replacement of older terraced housing and provision of a range of new housing); enable the diversification of the local economy and the growth in tourism; encourage the provision of improved transport links and seeks to improve the built and natural environment which will all assist in achieving this aim and improve the quality of life within Hartlepool.
- 2.42 Through policies in the local plan and various other strategies and incentives the council will continue to seek ways to achieve higher economic growth rates in Hartlepool in order to bridge the gap with more prosperous authorities in the region and provide greater opportunities and prosperity for residents.
- 2.43 The attraction and retention of highly skilled workers is viewed as critical to regional and sub-regional economic success, the council will work with other Tees Valley authorities to ensure the right housing and environmental conditions are available to contribute to population growth and the attraction of key highly skilled workers to the region.

3 IMPLEMENTATION OF THE HARTLEPOOL LOCAL DEVELOPMENT SCHEME

- 3.1 The Planning and Compulsory Purchase Act 2004 (as amended) (the 2004 Act) introduced a new way of plan making and set out that Local Planning Authorities should produce a suite of planning documents known as the Local Development Framework (LDF) (diagram 1). The Local Development Scheme (LDS), forms part of Hartlepool's LDF. An LDS is required under section 15 of the 2004 Act. It sets out a programme for the preparation of documents relating to spatial planning in Hartlepool. It is specifically concerned with documents being prepared over the next five years (February 2025 – February 2030). The LDS that relates to this report was approved by Regeneration Services Committee in February 2025 and it will be reviewed as necessary when circumstances change.
- 3.2 Planning policies for Hartlepool are set out in the following Development Plan Documents (DPDs):
- Hartlepool Local Plan which sets out the spatial vision, spatial objectives, strategic policies and allocations for the area.
 - Hartlepool Local Plan Policies Map which depicts what planning policies apply to parcels of land across the borough.
 - The 2011 Tees Valley Minerals and Waste Core Strategy and DPD which contain policies relating to land allocations and policies for assessing minerals and waste application.
 - The 2018 Hartlepool Rural Neighbourhood Plan that sets out the spatial vision, spatial objectives and strategic policies and allocations for the identified rural area.
- 3.3 The Supplementary Planning Documents (SPD) adopted in Hartlepool are:
- Transport Assessments & Travel Plans SPD (January 2010).
 - Trees and Development SPD (2013).
 - Green Infrastructure SPD (February 2014).
 - Shop Fronts SPD (2014).
 - New Dwellings outside of Development Limits (August 2015).
 - Seaton Carew Regeneration SPD (September 2015).
 - Planning Obligations SPD (November 2015).
 - Residential Design SPD (2019).
 - Public Rights of Way SPD (2020).
- 3.4 It is not a statutory requirement to include the SPDs within the LDS, however, in the interests of providing residents and stakeholders a clear view, below is a list of what documents were set out in the LDS as currently being updated.
- Planning Obligations SPD (2015).
 - Public Rights of Way SPD (2020).
- 3.5 The council are working on a residential design code SPD, that will set out a clear vision for how housing and housing areas should be designed, consultation took place in 2024, and the council received a great insight into what residents want.
- 3.6 Other documents that comprise the Local Development Framework include:

- Local Development Scheme (LDS) – which sets out the details of each of the Local Development Documents to be commenced over the next five years and the timescales and arrangements for their preparation.
- Authorities Monitoring Report (AMR)– assessing the implementation of the Local Development Scheme and the extent to which policies in Local Development Documents are being achieved.
- Statement of Community Involvement (SCI) – which sets out the policy for involving the community and key stakeholders both in the preparation and revision of local development documents and with respect to planning applications. The SCI will be updated in 2025, and all local development documents will be prepared in accordance with the arrangements set out in the SCI.

Implementation of the 2025 Local Development Scheme

3.7 Table 12 details the timetable for the 2025 LDS outlining key dates for different stages and delivery of the LDS’s main DPD document; the current local plan which is due for an update.

Table 12: Timetable of the Hartlepool Local Plan (2030).

Table 1: LOCAL PLAN TIMEFRAME FOR ADOPTION	
OVERVIEW	
Role and content	To set out the vision and spatial strategy for Hartlepool and the objectives and primary policies for meeting the vision.
Geographical Coverage	Borough-wide
Status	Development Plan Document
Conformity	Must comply with legislation, reflect the Hartlepool Community Strategy and be in general accordance with the National Planning Policy Framework.
TIMETABLE / KEY DATES	
Stage	Date
Evidence base Production and pre consultation	2025 – December 2027
LP programme (January 2028 – June 2030)	
Evidence base update	January 2028 – May 2028 (5 months)
Drafting stage	June 2028 – September 2028 (4 months)
Consultation and on draft (including editing sessions)	October 2028 – December 2028 (3 months)
Amendments to the draft and Submission for examination	January - April 2029 (4 months)
Public Hearings	July 2029 (3 months after submission)
Inspector’s Interim Findings	September 2029
Redrafting Stage and Consultation on Main Mods	October – December 2029

Inspector's Fact Checking Report	February 2030
Inspector's Final Report	April 2030
Adoption	June 2030
ARRANGEMENTS FOR PRODUCTION	
Lead Organisation	Hartlepool Borough Council
Management arrangements	The local plan will be approved by Neighbourhood Services Committee during the various stages of consultation and ratified by full Council prior to adoption.
Resources Required	Primarily internal staffing resources with use of consultants if necessary for any special studies required.
Community and Stakeholder Involvement	In accordance with the Statement of Community Involvement.
POST PRODUCTION / REVIEW	
<p>The effectiveness of the policies in the local plan will be assessed in the Authorities Monitoring Report. The local plan may be amended in the following circumstances:</p> <ul style="list-style-type: none"> • A significant review of the Community Strategy • A significant amendment to the Council's Corporate direction • Policies failing against the Monitoring Framework Associated with the local plan – this may trigger a partial update of a particular area of the plan or a full update. 	

Source: Hartlepool Borough Council Local Development Scheme February 2025.

3.8 The evidence base production and pre-consultation is due to commence in the next financial year.

4 ASSESSMENTS OF POLICIES

Introduction

- 4.1 This section of the Authorities Monitoring Report (AMR) assesses the implementation and effectiveness of current planning policies contained in the local plan. The local plan covers the period 2016 to 2031 therefore assessment of policies will use baseline information as of April 1st, 2016. A comparison will be made between the measured actual effects of policies, baseline information and predicted effects as outlined in the Sustainability Appraisal of the local plan policies. This comparison will form the basis of how the policies are performing. Targets and indicators to measure policy performance have been chosen in-line with the adopted monitoring framework as outlined in appendix 12 of the local plan.
- 4.2 The 2012 Regulations⁷ outline the minimum information to be contained in the AMR and these will all be reported accordingly. Specifically, Local Planning Authorities (LPAs) are required to provide information on annual numbers of net additional dwellings, net affordable dwellings as specified in any local plan policy within the monitoring period and since the date the policy was first published, adopted or approved.
- 4.3 This section considers the vision and objectives of the local plan along with the policies (see appendix 1) relating to these objectives and some related output indicators for assessing the effectiveness of the policies. A selected number of targets have been included in this report.

Hartlepool Local Plan Objectives, Policies and Indicators

The Hartlepool Local Plan Vision is that:

“Hartlepool by 2031 will be a more sustainable community having achieved the substantial implementation of its key regeneration areas as set out within the Hartlepool Vision; raised the quality and standard of living; increased job opportunities through developing a strong, diverse and thriving local economy which contributes positively to the sub-regional economy; maximised quality housing choices and health opportunities to meet, in full, the current and future needs of all residents. The built, historic and natural environment will have been protected, managed and enhanced, and will contribute to making Hartlepool a safe and attractive place to live, and an efficient and sustainable transport network will integrate its communities within the Tees Valley City Region and beyond. The town will have become a focal destination for visitors and investment.”

In the context of this vision, the strategy for the local plan covers four broad themes each with associated spatial objectives, main policies flowing from the objectives, core output indicators to measure policy performance and targets where set.

⁷ Part 8, 34 (3) of the Town and Country Planning (Local Planning) (England) Regulations 2012.

Table 13: local plan themes, objectives, policies, targets and core output indicators (2016-2031).

'Hartlepool's Ambition' Themes	Spatial Objectives for the Local Plan	Local Plan Policies	Core Output Indicators & Targets (if set)
<p>Jobs and the Economy, Lifelong Learning and Skills.</p>	<ol style="list-style-type: none"> 1. To diversify the economic base of Hartlepool and promote an entrepreneurial culture to create more employment opportunities for local people. 2. To develop Hartlepool as a destination of choice for inward investment. 3. To enhance the tourism offer. 4. To support the development of educational and training facilities that will develop a skilled workforce. 5. To facilitate development in the key investment areas in the borough. 6. To continue to protect and enhance the vitality and viability of the Town Centre. 	<p>LS1, INF1, INF2, INF5, CC3, HSG3-8, EMP1, EMP2, EMP3, EMP4, EMP5, RC1 – RC21, NE1, .NE2, LT1, LT2, LT3, LT4, LT5, LT6, RUR5</p>	<p>Targets:</p> <ul style="list-style-type: none"> - 1950m² of retail/community floorspace - Create 4350 net new jobs (290 jobs/annum) <p>Core Output Indicators</p> <p>LE1: Total gross amount of additional employment floor space by use class (B1/B2/B8)</p> <p>LE2: Total gross amount of employment floor space lost to non-employment developments</p> <p>LE3: Amount of employment land developed by type (Brownfield or Greenfield)</p> <p>LE4: Available employment land (ha)</p> <p>LE5: Vacancy rates in the Town Centre (number and gross floor space area of empty units)</p> <p>LE6: Number, Type &, Location of permitted applications for tourism/leisure/educational/training facilities</p> <p>LE7: Completed gross floorspace or area for tourism/leisure/educational/training facilities</p>
<p>Strengthening Communities, Community Safety, Housing, Health and Wellbeing</p>	<ol style="list-style-type: none"> 7. To make Hartlepool a safer place by reducing crime and the fear of crime and anti-social behaviour. 8. To support growth in the housing sector and improve the choice, quality and affordability of all types and tenures of housing. 9. To strengthen social cohesion and reduce inequalities by protecting and encouraging access to local facilities. 10. To encourage healthier and more sustainable lifestyles. 	<p>INF3, INF4, QP3, QP4, QP5, HSG1-13, NE1, NE2, NE3, NE5, NE6</p>	<p>Targets:</p> <ul style="list-style-type: none"> - 6150 new dwellings (410/annum (15 year average) - Of which 74/annum should be affordable and 65/annum should be from HMR sites <p>Core Output Indicators</p> <p>H1: Housing delivery (and demolitions) at HMR sites</p> <ul style="list-style-type: none"> - 1a: Number of new dwellings on HMR Sites - 1b: Number of Demolitions on HMR sites - 1c Net dwellings delivered on HMR sites <p>H2: Annual Net Housing Delivery (excludes demolitions and replacements on HMR sites)</p> <ul style="list-style-type: none"> - 2a:net delivery in previous years - 2b:net additional for the reporting year - 2c:Housing delivery target - 2d Actual Net Delivery (per reporting year) - 2e Cumulative Delivery (over local plan period)

			<p>H3: Number and types of affordable dwellings delivered</p> <p>H4: Housing types completed</p>
<p>Environment (excluding Transport) Culture and Leisure</p>	<p>11. To protect, promote and enhance the quality and distinctiveness of the borough's natural, rural and built environment.</p> <p>12. To protect and enhance the borough's unique <i>historic environment</i>, cultural heritage and coastline.</p> <p>13. To reduce the causes and minimise the impacts of climate change.</p> <p>14. To maximise the re-use of previously developed land and buildings.</p> <p>15. To reduce the causes and minimise impacts of climate change in particular through the delivery of renewable and low carbon energy development. To provide a safe, attractive and well-designed environment.</p>	<p>LS1, CC1, CC2, CC3, CC4, CC5, QP4, QP7, HSG3, NE1, NE2, NE3, NE4, NE5, NE6, EMP6, RUR1, LT2, LT3, HE1, HE2, HE3, HE4, HE5, HE6, HE7</p>	<p>E1: Changes to sites designated for conservation as a result of planning permissions (i.e. international sites, national sites and local wildlife sites)</p> <p>E2: Quantity & type of green infrastructure gained/enhanced/lost through planning permissions (ha)</p> <p>E3: Amount of wildlife habitat lost/gained as a result of planning permissions (ha)</p> <p>E4: Amount of ancient woodland habitat lost/gained as a result of planning permissions (ha)</p> <p>E5: Amount of priority species lost/gained as a result of planning permissions (ha/number)</p> <p>E6: Amount of municipal waste arising and % recycled</p> <p>E7: Number and capacity of permitted and installed renewable energy developments</p> <p>E8: Number of approved planning applications in rural areas</p> <p>E9: Types of approved developments in rural areas</p> <p>E10: Number of locally listed buildings and structures</p> <p>E11: Number of locally listed buildings /structures at risk</p> <p>E12: Number of conservation area appraisals taken</p>
<p>Environment (Transport)</p>	<p>16. To ensure the provision of a safe, efficient and sustainable transport network, accessible to all.</p> <p>17. To strengthen transport links with the Tees Valley sub-region, region and beyond.</p>	<p>LS1, INF1, INF2</p>	<p>T1: Number & lengths of roads created/ improved to reduce congestion</p> <p>T2: Number & lengths of cycleways created, improved or lost</p> <p>T3: Number, Types & lengths of public walkways and coastal routes created, diverted improved or lost</p>

Source: Hartlepool Local Plan 2018 – 1st three columns.

4.4 The core output indicators and set targets have been selected to ensure they follow the SMART principle i.e. they are Specific, Measurable, Achievable and Realistic and have a Time element. Data relating to the indicators is collected and collated annually. It will be analysed and compared to the baseline information and predicted effects from the Sustainability Appraisal. A commentary will be given to indicate how the policies are performing and whether they are being implemented.

- 4.5 Policies can divert from their intended objectives while monitoring over a period. It is important to note that policy diversion does not necessarily mean policy failure but instead point out unintended policy consequences which can either be positive or negative. Unintended consequences act as trigger points to consider policy review. The review will depend on the nature and extent of the policy diversion and whether there is any justification for the diversion or not. All policies that divert from their intended path will be flagged up for possible review and intervention measures to be taken.
- 4.6 The next section assesses the policies under the following sub-sections:
- A) Local Economy (Output Indicators LE1-LE7)
 - B) Housing (Output Indicators H1-H5)
 - C) Environmental Quality (Output Indicators E1-E7)
 - D) Historic Environment and Rural Area (Output Indicators HR1-HR5)
 - E) Transport and Infrastructure (Output Indicators T1-T3)

A LOCAL ECONOMY

- 4.7 This section assesses policies that impact on the local economy covering all land allocated or designated for employment, i.e. industrial, commercial, retail, education, leisure and tourism.

Employment Objectives and Policies

Local Plan Spatial Objectives 1, 2 and 5: To diversify the economic base of Hartlepool and promote an entrepreneurial culture to create more employment opportunities for local people; to develop Hartlepool as a destination of choice for inward investment and to facilitate development in the key investment areas in the borough.

Related Policies

- Identification and criteria for business development at a prestige industrial site Wynyard Business Park (**EMP1**), and a high-quality industrial site Queens Meadow (**EMP2**)
- Identification and allocation of sites for a wide range of general employment uses including light and general industry at Oakesway, Longhill/Sandgate, Usworth Rd/Park View West, Sovereign Park, Brenda Road, Tofts Farm East/Hunter House, Tofts Farm West, Graythorp Industrial Estate, Zinc Works Road (**EMP3**).
- Identification and criteria for specialist industrial development at Hartlepool Port, West of Seaton Channel, Philips Tank farm, South Works, North Graythorp, Graythorp Waste Management and Able Seaton Port (**EMP4**)
- Identification and safeguarding of land for a nuclear power station at Zinc Works Road/North Gare/Seaton Snook/Able Seaton Port areas (**EMP5**)

Local Plan Spatial Objectives 3, 4 and 6: To continue to protect and enhance the vitality and viability of the Town Centre, enhancing the tourism offer and supporting the development of educational and training facilities that will develop a skilled workforce.

Related Policies

- Identifying and defining a hierarchy of retail and commercial centres that will offer a variety of sites that are economically attractive, diverse and in appropriate sustainable locations and/or locations where connectivity can easily be enhanced throughout the borough (**RC1**)
- Protecting, supporting and enhancing the retail character of the Town Centre ensuring that development is in accordance with policy RC1 and that the Town Centre is the sequentially preferable location for main town centre uses (**RC2**)

- Supporting, protecting and encouraging diversification of commercial and retail development in the Edge of Town Centers (**RC4-RC11**), Retail Parks (**RC12-RC15**) and Local Centers (**RC16**)
- Developing, supporting, enhancing and protecting leisure and tourism facilities including high quality accommodation and cultural assets to build on successful regeneration schemes in designated tourist areas across the borough including sustainable rural tourism (**LT1-LT6, RUR5**).

Industrial Policies Assessment

- 4.8 A significant number of industries in Hartlepool are located in the southern part of Hartlepool, and this area is known as the Southern Business Zone (SBZ). The SBZ is a key employment area and a major driver of economic prosperity for the Tees Valley sub-region. A list of all allocated industrial sites is shown in table 14. The amount of developed land and available land on each site is also shown, and this will be reviewed each year to give an indication of land take up for employment purposes.
- 4.9 The council adopted the Targeted Training and Employment Charter in 2007. The Charter considered education and employment levels in the borough and discussed actions that would seek to bring more people into employment. At the time there were insufficient training opportunities either in educational establishments or apprenticeships within the development industry and this led to a shortage of a skilled workforce in Hartlepool.
- 4.10 To address the skills shortage The Charter allows the council to incorporate training and employment matters into development proposals and local plan policy QP1 (Planning Obligations) enshrines this requirement within planning policy.
- 4.11 On many proposals the council has required developers to sign up to a training and employment charter that would ensure that the development undertakes steps to ensure local people are employed. These charters are secured via a section 106 legal agreement and monitored by Planning Services and Economic Development.
- 4.12 In light of the increase in opportunities within educational establishments and apprenticeships schemes, it is considered that gap in upskilling residents has been filled by the industry and thus the training and employment charters are no longer considered necessary and thus Planning Services will cease to include them within legal agreements.
- 4.13 The Employment Land Review (ELR) endorsed by the council's Regeneration Services Committee in January 2015 has been used as evidence for the adopted local plan and as material consideration whilst making planning decisions. Based on the ELR findings and recommendations, the adopted local plan reveals a realistic baseline supply of 197.2ha of available employment land from 19 sites across the borough and these have been allocated and safeguarded for employment purposes until 2031.

4.14 Table 14 shows the sites allocated for employment (industrial) and shows employment land uptake rate. For the third year running, there is no net employment land uptake. However, there is 30.7 ha loss of allocated employment land to housing at Wynyard (application ref H/2022/0181). Available employment land has thus reduced to 164.5ha from 195.2ha. There is no major negative, unjustified diversion of employment policies since precedence has been set for housing at Wynyard in previous years. (table 14). The information on table 14 will be used as a baseline for comparison as policies get implemented and developments happen over the course of the plan period up to 2031.

Table 14: Available allocated employment land 2024/25

Name	Market Segment	Allocated Uses	Policy Number	Site Area (ha)	Developed/ Committed (ha)	Reserved Land (ha)	Available Land (ha)
Brenda Road East	general employment land	B1,potentially B2,B8 in certain circumstances	EMP3e	3.1	3.1	0	0.0
Graythorp Waste Management	specialist industrial site	B2,potentially polluting or harzadous development	EMP4f	4.1	0.0	0	4.1
Graythorp Industrial Estate	general employment land	B1,potentially B2,B8 in certain circumstances	EMP3h	13.1	13.1	0	0.0
Graythorp Yard/Able Seaton Port	specialist industrial site	B1,potentially B2,B8 in certain circumstances	EMP4g	47.8	0.0	47.8	0.0
Longhill /Sandgate	general employment land	B1,potentially B2,B8 in certain circumstances	EMP3b	73.8	72.8	0	1.0
North Graythorp	specialist industrial site	B2,potentially polluting or harzadous development	EMP4e	28.1	14.5	0	13.6
Oakesway	general employment land	B1,potentially B2,B8 in certain circumstances	EMP3a	38.8	20.2	0	18.6
Park View West	general employment land	B1,potentially B2,B8 in certain circumstances	EMP3c	19.2	17.4	0	1.8
Philiphs Tank Farm	specialist industrial site	B2,potentially polluting or harzadous development	EMP4c	150.4	47.0	103.4	0.0
Queen's Meadow	high quality employment site	B1,potentially B2,B8 in certain circumstances	EMP2	65.0	20.3	0	44.7
South Works	specialist industrial site	B1,potentially B2,B8 in certain circumstances	EMP4d	131.3	110.8	20.5	0.0
Sovereign Park	general employment land	B1,potentially B2,B8 in certain circumstances	EMP3d	20.9	9.5	0	11.4
The Port	specialist industrial site	B1,potentially B2,B8 in certain circumstances	EMP4a	106.0	45.9	0	60.1
Tofts Farm East/Hunter House	general employment land	B1,potentially B2,B8 in certain circumstances	EMP3f	44.2	43.6	0	0.6
Tofts Farm West	general employment land	B1,potentially B2,B8 in certain circumstances	EMP3g	34.1	25.9	0	8.2
Usworth Road	general employment land	B1,potentially B2,B8 in certain circumstances	EMP3c	14.1	13.7	0	0.4
West of Seaton Channel	specialist industrial site	B2,potentially polluting or harzadous development	EMP4b	77.1	33.1	44	0.0

Wynyard Park	prestige employment site	B1,potentially B2,B8 in certain circumstances	EMP1	30.7	30.7	0	0.0
Zinc Works Road	general employment land	B1,potentially B2,B8 in certain circumstances	EMP3i	3.0	3.0	0	0.0
Total Area (ha)				904.8	493.9	215.7	164.5

Source: Hartlepool Borough Council 2025

- 4.15 This year there has been no recorded additional employment floorspace or employment land uptake in allocated industrial land.
- 4.16 Several core output indicators have been selected to measure the effectiveness of the policies which seek to diversify and improve the economy and employment opportunities. These include measures relating to business, leisure and tourism development within the borough.

Core Output Indicator (B1/B2/B8)	LE1: Total gross amount of additional employment floor space by use class (B1/B2/B8)
Core Output Indicator developments	LE2: Total gross amount of employment floor space lost to non-employment developments
Core Output Indicator	LE3: Amount of employment land developed by type (Brownfield or Greenfield)
Core Output Indicator	LE4: Available employment land (ha)

Table 15: Completed Employment Floorspace 2024/25

	E(g)(i)	B2	B8	Other	Total
LE1: Gross additional employment floor space (m ²)					
	0	0	0	0	0
LE2: Gross employment floor space lost (m ²)					
	0	0	0	0	0
LE3: Total amount of employment land developed by type					
Greenfield (m ²)	0	0	0	0	0
Brownfield (m ²)	0	0	0	0	0
LE4: Available employment land (ha) = *195.2 – 0 (total lost) = 195.2					

*Available employment land as at Table 14

Source: Hartlepool Borough Council 2025

- 4.17 The council is currently working closely with the Hartlepool Development Corporation (HDC) to bring economic development to the town and for this year there is no notable developments that have taken place. However, there are live planning applications from the HDC in areas across the town, notably at Queens meadow Business Park. The Tees Valley Local Growth Plan is also progressing well, and it envisaged that it would help steer economic development in the town.
- 4.18 To date there has been no recorded employment land uptake at the allocated prestigious employment site Wynyard. As the years progress, Wynyard continues to attract more housing developments as evidenced by various housing applications received at the site. However, it is important to note that planning decisions should be made in accordance with the development framework.

- 4.19 To comply with policy Hsg6 of the Hartlepool Local Plan, development at Wynyard must accord with an approved Masterplan. The Wynyard Masterplan is material consideration and should be adhered to. The Wynyard Masterplan gives a detailed framework for development at Wynyard in a way which enables the delivery of the local plan policies. The Wynyard Masterplan was endorsed by Regeneration Services Committee in October 2019 and adopted by Stockton on Tees Borough Council (who jointly produced the Masterplan) in late 2019.
- 4.20 At the time of writing this report, the nuclear power station has received funding and commissioned to be replaced with one of the same sizes; this will create lots of jobs for the town. There will be no overall loss or gain in employment floor space since the replacement is like for like. At the time it is replaced there is likely to be a short/medium term increase in employment in terms construction jobs associated with the overlap between decommissioning of the existing plant and creation of the new one. It is highly likely that there will be new supplier chain businesses created within this period of decommissioning and building of a new power station. Employment policies are being delivered and there is currently no need for their revision.

Town Centre and Town Centre Uses

- 4.21 The Town Centre continues to be the primary retail and commercial centre in the borough. In accordance with policy **RC1**, the council will seek to diversify, support and protect the town centre as the sequentially preferable location for main town centre uses which include:
- Shops - E(a)
 - Financial and Professional Services - E(b)
 - Food and Drink - E(c)
 - Drinking Establishments - (Sui Generis)
 - Hot Food Takeaways - (Sui Generis)
 - Business - E(g)
 - Hotels (C1)
 - Non-residential institutions - E(e-f) and F1
 - Assembly and Leisure - E(d) and F2(c-d)
 - Theatres and Nightclubs - E(d) and F2(c-d)
 - Residential (C2, C2a, C3 and C4)

The above use classes came into effect in September 2020; they can be accessed on the following link:

https://www.planningportal.co.uk/info/200130/common_projects/9/change_of_use

- 4.22 Information on vacancy rates provides a useful indication of the viability of the town centre. Each year the council carries out a retail and commercial uses survey in March/April covering all allocated retail areas, commercial areas and any other uses in the borough. The result from the annual survey is used to monitor business activity in the borough. Core output indicator **LE5** in the local plan has been selected to monitor vacancy rates and town centre uses.

Table 16: Vacancy Rates in the Town Centre 2024/25.

	2020/21	2021/22	2022/23	2023/24	2024/25
Total number of retail units	478	477	476	476	476
Total number of vacant units	113	111	106	118	145
Vacancy Rate (%)	23.6	23.3	22.3	24.8	30.1
Total Floor Space (m ²)	138,963	138,893.9	136,710.41	136,710.2	136,710.2
Vacant Floor Space (m ²)	21,055	22,347.7	22,619.4	26,846.7	33,894.2
Vacancy Rate (%)	15.2	16.1	16.5	19.6	24.8

Source: Hartlepool Borough Council, Annual Retail and Commercial Uses Survey, March 2024/25.

- 4.23 For the past 5 years (as shown on table 16) the vacancy rate in terms of floorspace has been increasing. This year the vacancy rate is 24.8%, up from 19.6% last year. Lack of high-quality shops and vacancy rates in the town centre remains a challenge. Several shops closed, and this has significantly increased the vacancy rate this year. At the time of writing this report, Boyes departmental store is refitting to open a branch in Hartlepool. This should reduce vacancy rates next year provided existing open shops do not close.
- 4.24 There is a considerable number of smaller businesses opening and shutting down in a short space of time as seen in the retail survey. The continued increase in the vacancy rates points that the retail sector still has not recovered post Covid 19 pandemic and retail activity continues to decline with properties remaining vacant for long periods of time. It is however noted that some shops are seasonal, opening only during a certain time of the year to clear or sell goods and services for events and shutting down for the rest of the year.
- 4.25 Despite the notably high vacancy rate, retail policies continue to perform as expected because the town centre continues to develop as a retail hub with E and F1 uses accounting for more than 50% of all uses in the town centre, followed by F2 uses. All uses in the town centre are as allocated in the local plan and are in accordance with the retail policies. There is no evident diversion of policy performance.
- 4.26 There have been no town centre uses completions recorded outside of the town centre, edge of town centre, or local centre boundaries. This indicates further that town centre policies are being implemented with appropriate planning permissions being granted for town centre uses within the town centre. There is therefore no need to update any of the town centre retail policies.

Tourism and Leisure Policies Assessment

- 4.27 Tourism has become very important to the Hartlepool economy, the developments at the Marina and Seaton Carew acting as main catalysts to its success. The local plan identifies the Town Centre, the Marina, the Headland and Seaton Carew as main tourism destinations and outlines related policies.

4.28 Rural tourism in terms of holiday accommodation, camping sites and caravan sites also contributes towards a good tourism offer in the borough. The borough has in the past years seen a growth in the number of planning applications being determined for holiday cottages mainly in the countryside.

LE6: Number, Type &, Location of permitted applications for tourism/leisure/educational/training facilities.

LE7: Completed gross floorspace or area for tourism/leisure/educational/training facilities.

4.29 Like last year, there were no determined planning applications for tourism both in the rural and urban areas (table 17).

Table 17: Planning permissions granted for tourism, leisure, education and training developments 2024/25.

General Location	Site / Location	Development	Development progress
Town Centre	None	None	None
Edge of town centre	None	None	None
Other Urban areas	None	None	None
Marina area	None	None	None
Headland	None	None	None
Seaton Carew	None	None	None
-Countryside/Rural Area	None	None	None

Source: Hartlepool Borough Council, Development Control, approved planning permissions 2024/25

B. HOUSING

4.30 This section assesses the implementation of housing policies.

Local plan Spatial Objectives 2, 3 & 4: To support growth in the housing sector and improve the choice, quality and affordability of all types and tenures of housing and to encourage healthier and more sustainable lifestyles.

Related Policies

- Ensuring that new housing provision in the borough is delivered through housing sites that have already been permitted and newly identified sites as allocated in the local plan (**HSG1, HSG3-HSG8**).
- Ensuring that all new housing, and/or the redevelopment of existing housing areas, contributes to achieving an overall balanced housing stock that meets local needs and aspirations, both now and in the future (**HGS2**).
- Seeking to deliver affordable housing in respect of all applications or proposals for C3 residential developments that consist of a gross addition of 15 dwellings or more. The affordable housing target of 18% will be sought on all sites above the 15-dwelling threshold (**HSG9**).
- Tackling the problem of the imbalance of supply and demand in the existing housing stock through co-ordinated programmes including Housing Market Renewal. Priority will be given to the housing regeneration areas in central Hartlepool identified in the Hartlepool Housing Strategy (**HSG10**).
- Setting out the criteria for residential annexes and residential extensions (**HGS11-12**).
- Seeking contributions/planning obligations from developers where viable and deemed to be required to address affordable housing provision and fund the HMR programme as set out in the Planning Obligations Supplementary Planning Document (**QP1**).

Housing Policies Assessment

- 4.31 The current Strategic Housing Market Assessment (SHMA, 2015) identifies an objectively assessed need (OAN) of 287 dwellings per annum which takes into account the predicted household projections over the plan period of 15 years (2016 to 2031); a backlog from previous years; and aspirations of the Tees Valley Combined Authority to create more jobs and overcome economic development barriers of which housing is one as outlined in their Strategic Economic Plan (2016-2026). The adopted local plan therefore sets an aspirational 15-year average housing target of **410 net dwellings per annum** which also factors in a replacement of demolitions on Housing Market Renewal (HMR) sites and an additional 20% affordable housing buffer. The HMR initiative identifies a total of 1950 dwellings to be demolished over the plan period at a replacement rate of 50%. Therefore, a total of 975 dwellings (i.e. 65 per annum) will be required from HMR sites over the plan period.
- 4.32 Since demolitions from HMR sites have already been accounted for in the 410 15 year average dwellings/annum delivery rate, actual demolitions from HMR sites

during the plan period will be excluded in calculating the net housing delivery. Demolitions and delivery from HMR sites will therefore be monitored separately from the rest of other housing sites to avoid double counting in working out total annual demolitions.

Core Output Indicator **H1**: Housing delivery (and demolitions) at HMR sites

- **H1a**: number of new dwellings on HMR sites
- **H1b**: Number of Demolitions on HMR sites
- **H1c**: Net dwellings delivered on HMR sites (H1a-H1b=H1c)

Core Output Indicator **H2**: Annual Net Housing Delivery (excludes demolitions and replacements on HMR sites)

- **H2a**: net delivery in previous years
- **H2b**: net additional for the reporting year
- **H2c**: Housing delivery target
- **H2d** actual net delivery
- **H2e** Cumulative Delivery (over Local Plan period).

Housing targets

Table 18: Housing Targets

	Total net housing required 2016-2031	Source of plan target
Baseline Housing Target	410 per annum (15 year average)	Hartlepool Borough Council Local Plan
Phased Delivery Target for the reporting year	400 per annum	Table 7 of the Hartlepool Local Plan and Hartlepool Borough Council Local Plan Housing Delivery Report (2020)
Revised Delivery Target	464 per annum	Table 3 of the Housing Delivery Report and 5-year housing land supply

Source: Hartlepool Borough Council 2018 Local Plan.

- 4.33 Table 18 above shows that the overall baseline housing delivery target over the plan period is 410 dwellings/annum. However, it is important to note that the local plan has phased the baseline housing delivery target with lower housing delivery in years 1 to 5 increasing in year 6 to 10 and increasing more in years 11 to 15.
- 4.34 The revised target has also been calculated to demonstrate the supply, availability of housing land in the borough and how the housing requirement will be met up to 2028/29 considering changing economic circumstances, previous years' under delivery and a 5% buffer.
- 4.35 Table 19 shows that there have been 12 dwellings (mainly bungalows) delivered on the Carr & Hopps HMR site. The site is now built out as of this year. These were the last remaining dwellings since the demolition of old stock dwellings took place prior to adoption of the local plan.

Table 19: Housing delivery and demolitions at HMR sites 2024/25.

HMR Site Location/Name	Number of new dwellings on HMR sites (H1a)	Number of Demolitions on HMR sites (H1b)	Net dwellings delivered on HMR sites (H1c)
Carr & Hopps (Rodney Street)	12	None	10

Source: Hartlepool Borough Council 2025.

- 4.36 Table 20 shows information on indicator H2 and will be used to demonstrate the annual net delivery of housing as set out in policy HSG1.

Table 20: Annual net housing delivery

Core Output Indicator H2		2018/19 1	2019/20 2	2020/21 3	2021/22 4	2022/23 5	2023/24 6	2024/25 7
H2a	Net additional dwellings in previous years	371	254	161	351	507	585	
H2b	Net additional dwellings for the reporting year							470
H2c	Housing delivery target (Local Plan)	350	350	350	400	400	400	400
H2d	Actual net Delivery (per reporting year)	+21	-96	-189	-49	+107	+185	+70
H2e	Cumulative Delivery (over local plan period)	-64	-160	-349	-398	-291	-106	-36

Source: Hartlepool Borough Council 2025; NB: In relation to actual and cumulative delivery + denotes above target delivery and – (minus) under delivery.

- 4.37 In 2024/25 there is a net delivery of 470 dwellings (i.e. 467 new builds and 3 change of use). The net delivery is lower than last year but is also above both the baseline housing target of 400 and the revised target of 464 dwellings/annum. It is also above the Objectively Assessed Need (OAN) of 287 dwellings/annum, and this is notably positive development again this year, like last year. The cumulative under delivery continues to decline and this year it is -36 compared to last year at -106 (table 20).
- 4.38 The house building rate slightly declined this year in comparison to last year. The new build completions were mainly from existing large sites Wynyard housing development sites, Upper Warren, Marine Point, Quarry Farm 2, Antler Park and Woodside Meadows in Seaton. In addition to the ongoing building progress on various housing sites, this year there is a total of 166 starts (i.e. 164 new build, 2 conversions) mainly from Greenfield sites hence expected completions from these new starts will add on to next year's net completions. It is anticipated that some of the strategic housing sites allocated in the adopted local plan may obtain planning permission. It is therefore expected that housing delivery will continue to increase next year and meet the delivery target.
- 4.39 The overall 15 year average annual target of 410 dwellings is still considered to be achievable, the cumulative under delivery is low and given that a number of sites are building out across the borough at a healthy rate the council expect the under

delivery to be met within the next reporting year. In light of the recorded figures and professional assumptions the council consider that there is currently no need to revise it or any of the housing policies. For further information on housing delivery and the 5-year supply position please refer to the 2024/29 Housing Review and Five-Year Supply document which is available on the council's website on following link:

[5 Year Housing Land Supply Review | Hartlepool Borough Council](#)

Core Output Indicator **H3**: Number and types of affordable dwellings delivered.

Core Output Indicator **H4**: Housing types completed.

4.40 Affordable housing is defined as follows:

- *Social rented homes* are owned by either local authorities or registered providers (RP). These organisations charge a rent which is set by government guidelines through the national rent regime.
- *Affordable rent* is subject to rent controls that require a rent of no more than 80% of the local market rent (including service charges, where applicable). As local market rents vary from place to place, these are measured by the amount of local housing allowance administered in a particular area.
- The term '*intermediate housing*' includes Discounted Market Sales and a range of routes to home ownership for those who could not achieve home ownership through the market as set out in Annex 2 of the National Planning Policy Framework's (NPPF).

4.41 The SHMA identifies a need for delivery of 144 affordable dwellings per annum. However, due to viability issues the local plan only requires 18% of the 410 net 15-year average delivery targets and this equates to an average of 74 affordable units per year.

4.42 Table 21: Number and types of affordable dwellings delivered (H3).

Year	Type			
	Social rent	Intermediate tenure Shared Ownership	Affordable rent	Total
2024/2025	8	40	45	93
2023/2024	35	40	80	155
2022/2023	0	33	63	96
2021/2022	7	53	80	140
2020/2021	0	0	26	26
2019/2020	22	0	52	74
2018/2019	0	0	62	62
2017/18	20	0	16	36

Source: Hartlepool Borough Council, March 2025.

4.43 Table 21 shows that this year there is a total of 93 completed affordable homes (all new builds). For the fourth-year running, it is above the local plan target of 74 affordable homes and is positive development for the borough. The completions

were mainly from large housing sites such as the Marine Point, Upper Warren and Antler Park. Thirteen Group own over 50% of the completed affordable homes. The council will continue to support the delivery of additional affordable housing through building on council-owned land, partnership working with Registered Providers in the borough and through securing affordable housing contributions as part of private residential developments.

- 4.44 A total of £363,000.00 S106 funds was received for offsite affordable housing this year (appendix 5) and £59 000 was spent on affordable housing schemes. Through the year, signed S106 agreements have secured a total of 67 onsite units (this includes 42 DMVs) and £9,641.00 in money (appendix 6). It is anticipated there will be more delivery of affordable homes throughout next year from large housing sites as they continue to build out.

Table 22: Housing types completed (Indicator H4).

Type		2018/19	2019/20	2020/21	2021/22	2022/23	2023/24	2024/25
Flat/apartment	gross	8	8	15	65	18	4	0
	% gross	2.1	3.1	8.9	18.5	3.6	0.7	0
Terraced house	Gross	103	41	27	12	44	94	64
	% gross	27.4	16.1	16.1	3.4	8.7	16.1	13.6
Semi-detached house	Gross	71	39	31	62	118	187	152
	% gross	18.9	15.3	18.5	17.7	23.3	32	32.3
Detached house	Gross	162	150	80	151	283	270	213
	% gross	43.1	58.8	47.6	43.0	55.8	46.1	45.3
Bungalow	Gross	32	17	15	61	44	30	41
	% gross	8.5	6.7	8.9	17.4	8.7	5.1	8.72

Source: Hartlepool Borough Council, 2025.

- 4.45 There is a steady flow in the delivery of detached houses in Hartlepool since the adoption of the local plan as shown in table 22. At 45.3%, detached houses still account for most dwelling types completed and flats/terraced houses the least, there were no flats completed this year.

C. ENVIRONMENTAL QUALITY

4.46 This section analyses policies related to the borough's environment (i.e. natural, built, rural and historic environment). Policies that seek to improve the environment and quality of life by adapting, minimising and mitigating against the effects of climate change will also be analysed in this section.

Local Plan Spatial Objectives 11, 12, 13, 14, 15: To protect, promote and enhance the quality and distinctiveness of the Borough's natural, rural, built and historic environment and to mitigate against the impacts of climate change.

Related Policies

- Protecting, managing and enhancing the borough's natural environment and green networks ensuring that sites designated for nature conservation are protected and unharmed by permitted developments (**NE1-7**).
- Protecting, managing and enhancing the rural area (**RUR1-6**).
- Protecting, managing and enhancing the borough's historic environment (**HE1-7**).
- Working with partner organisations, developers and the community to help minimise and adapt to climate change, in particular to minimise flooding risk and encourage reduction, reuse and recycling of waste (**CC1, CC2, QP7, EMP4f**).
- Encouraging renewable and low carbon energy generation, strategic wind turbine developments and large scale solar photovoltaic developments (**CC3, CC4, CC5**).

Natural Environment Policies Assessment

Core Output Indicator **E1**: Changes to sites designated for conservation as a result of planning permissions (i.e. international sites, national sites and local wildlife sites)

Core Output Indicator **E2**: Quantity & type of green infrastructure gained/enhanced/lost through planning permissions (ha)

Core Output Indicator **E3**: Amount of wildlife habitat lost/gained as a result of planning permissions (ha)

Core Output Indicator **E4**: Amount of ancient woodland habitat lost as a result of planning permissions (ha)

Core Output Indicator **E5**: Amount of priority species lost/gained as a result of planning permissions (ha/number)

4.47 The Local Wildlife Site (LWS) 'High Newton Hanzard Verges' was noted as being de-designated by the Natural Assets Working Group of the Tees Valley Local Nature Partnership (TVLNP) in the previous ARM. It has been destroyed incrementally over several phases of Wynyard housing development. Some compensatory habitats were secured; however, the replacement habitats are not of a quality that will ever become a LWS. The habitat of the destroyed LWS was Lowland Meadow which is a Priority habitat under section 41 of the Natural

Environment and Rural Communities (NERC) Act 2006. There is no available data with regard to recorded losses or additions to biodiversity habitat.

- 4.48 The quantity and type of green infrastructure lost or gained is not routinely measured or monitored, therefore no definitive measurement can be provided. However, the most frequently lost habitat types are Arable, Species-poor Grassland and Scrub. The habitats most frequently created as compensation are Amenity Grassland, Formal Planted Beds, Copses (native deciduous woodland), Hedges, Ponds and occasionally Wild Bird Cover Crops. When determining planning applications, the aim is to achieve 'No Net Loss' of biodiversity and some Biodiversity Gain. Funding for green infrastructure is secured through S106 developer contributions from planning applications, and this can be routinely measured. This year a total of £70,000 has been received and £58,000 was received for ecological mitigation (appendix 5). Appendix 6 shows signed S106 developer contributions during the year; £607,850.00 was secured for green infrastructure and £5,000.00 secured for ecological mitigation.
- 4.49 Ancient woodland is an 'irreplaceable habitat' due to the length of time required for the habitat to develop its intrinsic value. As such, creation of this habitat is not feasible. This year there have been no recorded losses of ancient woodland.
- 4.50 Priority species are those listed as 'of principal importance for the purpose of conserving biodiversity' under the provisions of section 41 of the Natural Environment and Rural Communities Act (NERC) (2006). There are a total of 943 priority species, including algae, fungi, non-vascular and vascular plants, invertebrates, fish, amphibians, reptiles, birds and mammals.
- 4.51 It is not possible to understand losses and gains across all Priority Species because of development. Instead, assessment focuses on identifying and preventing potential for significant harm, which could include losses to Priority species populations. When determining planning applications, the aim is always to minimise losses and secure gains. However, some Priority species are more frequently adversely impacted than others and the following are likely to be suffering long-term adverse impacts from development: brown hare, hedgehog, common toad, dingy skipper butterfly, wall butterfly and small skipper butterfly.
- 4.52 The suite of ground nesting arable birds (often referred to as 'farmland birds') are particularly vulnerable to development on arable land. These include lapwing, skylark, meadow pipit and yellow wagtail, all of which are likely to be suffering long-term adverse impacts from development.
- 4.53 Obtaining definitive counts of numbers of animals using an area of land, or likely to be affected by a proposed development, is often impractical due to the cost and time involved in undertaking the required surveys. Instead, survey effort is focused on identifying the most important areas for biodiversity, often concentrating on legally protected species and NERC Act species. Absolute counts are rarely obtained unless legally protected species licencing is required.

Core Output Indicator **E6**: Amount of municipal waste arising and % recycled.

Core Output Indicator **E7**: Number and capacity of permitted and installed renewable energy developments.

Table 23: The amount of household municipal waste arising.

Indicator E6	Landfill	Incineration with E.F.W.	Incineration without E.F.W.	Recycled/Composted	Other	Total Waste Arising	% Recycled/composted
2024/25	885	27,517	Nil	10,344	Nil	38,005	27.2
2023/24	270	26,604	Nil	10,217	Nil	36,821	27.7
2022/23	1,888	23,985	Nil	11,116	Nil	36,989	30.1
2021/22	511.48	26,742.37	Nil	12,892.90	Nil	39,552.21	32.6
Tonnes	2020/21	643	26,307	Nil	11,625	38,575	30.1
	2019/20	714.01	25,377.37	Nil	13,217.08	39,308.46	33.62
	2018/19	172.61	26,952.91	Nil	12,921.85	39,765.64	32.5
	2016/18	619.3	31,591.08	Nil	14,313.85	46,524.06	30.77

Source: Hartlepool Borough Council, 2025.

4.54 According to table 23; the total amount of waste arising this year increased in comparison to the previous year and recycled/composted waste continued to decrease from 27.71% last year to 27.2% this year. The increase in waste could be attributed to more properties being serviced because of more house completions and families moving in during the year.

4.55 The council continues to communicate with residents and the wider community via Hartbeat magazine, the council's own website and social media, to help them to recycle more of their waste.

Table 24: The amount of renewable energy generation by installed capacity and type 2024/25.

Core Output Indicator E7	Wind Onshore	Solar photovoltaics	Hydro	Biomass						Total
				Landfill gas	Sewage sludge	Municipal (& industrial) solid waste combustion	Co-firing of Biomass with fossil fuel	Animal biomass	Plant biomass	
Applications Permitted & installed capacity in MW	Nil	Nil	nil	nil	Nil	Nil	Nil	nil	nil	nil
Completed installed capacity in MW	Nil	Nil	nil	nil	Nil	Nil	Nil	nil	nil	nil

Source: Hartlepool Borough Council approved planning permissions 2024/25

- 4.55 At the time of writing it is noted that a council motion was discussed on 8th May 2025 with regard to the protection of residential amenity from inappropriately sited onshore wind turbines. The motion set out concerns that the local plan does not offer adequate protection to residents from the impacts of large-scale onshore wind turbines and suggested that separation distances could be introduced into policy and/or an SPD. While the motion did stress that the council fully supports the shift toward renewable energy, it stressed that renewable provision must not come at the cost of public wellbeing, visual landscape, or residential amenity.
- 4.56 This AMR sets out that there have been no renewable energy planning applications approved (table 25). The key policy for the determination of wind turbine development is policy CC4 (Strategic wind turbine developments), policy CC4 allocates two areas for strategic wind turbine development within the borough (High Volts area near Hart Village and the Brenda Road area). Policy CC4 sets out parameters in relation to mega watt capacity and tip height. The policy does not set out separation distances between turbines and dwellings as this is not supported by Government policy as such an approach does demonstrate positivity and flexibility. A case by case approach with regards to separation distances is considered to be more cost effective, proportionate and still allows for impacts upon residential amenity to be addressed and this is the approach that policy CC4 and the local plan as a whole takes.
- 4.57 Policy CC4 is considered to be in accordance with national policy, it has been tested at appeal and there is no evidence to suggest that it is failing and cannot be relied upon to make effective decision in the future. Policy CC4 does not require updating, if applications are submitted then the council will continue to use the policy and any other material considerations to determine such proposals.

Rural area Development Policies Assessment

E8: Number of approved planning applications in rural areas.
E9: Types of approved developments in rural areas.

- 4.58 The information shown on table 25 relates to planning applications approved for development on land outside the limits to development (urban fence and village envelopes). There are no approved developments outside development limits this year, as shown on table 25.

Table 25: Developments approved outside limits to development 2018-2025.

Developments Approved	2019/20	2020/21	2021/22	2022/23	2023/24	2024/25
Agricultural buildings	0	0	1	0	0	0
New dwellings – no agricultural justification	0	0	0	0	0	0
New dwellings associated with agricultural existing developments	0	0	0	0	0	0

New dwellings associated with rural business developments	0	0	0	0	0	0
Extensions to existing dwellings	0	0	0	0	0	0
Temporary residence in connection with rural business	1	0	0	1	0	0
Replacement dwellings	0	0	0	0	0	0
Residential conversions of rural buildings	0	0	0	0	0	0
Business conversions of rural buildings (buildings for business)	0	0	0	0	0	0
Extensions of gardens	0	0	0	0	0	0
Recreational and leisure uses	1	1	1	0	0	0
Extensions and other works relating to existing businesses	1	1	0	1	0	0
New buildings associated with business	0	0	1	1	1	0
Telecommunications development	0	0	0	0	0	0

Source: Hartlepool Borough Council, 2025

4.59 The adopted New Dwellings Outside Developments Limits SPD (2015) continues to assist preserve the countryside from unwanted, unsustainable and unjustified developments of isolated dwellings. The purpose of the SPD is to prevent the proliferation of isolated residential developments in unsustainable locations in the rural area and potentially reduce the compactness of the urban area. The rural development policies continue to be implemented, therefore there is no need to amend them.

Historic Environment Policies Assessment

E10: Number of locally listed buildings and structures.

E11: Number of locally listed buildings /structures at risk.

E12: Number of conservation area appraisals taken.

4.60 The National Heritage 'At Risk Register' includes a Grade I church in Hartlepool i.e. St Hilda church on the Headland. In addition, two Scheduled Ancient Monuments are considered to be at risk i.e. Heugh coastal artillery battery northwest of Heugh Lighthouse in the Headland and Low Throston deserted medieval village. Three conservation areas in Hartlepool also appear on the 'At Risk Register', these are Headland, Park and Seaton Carew.

Table 26: Numbers of listed buildings at risk 2024/25.

Grade	Buildings at risk
Grade I	Church of St Hilda, High Street, Headland
Grade II	Shades, 16 Church Street
	Church of St Paul, St Paul's Rd
	Beacon Tower, East End of North Pier
	Friarage Manor House, Friar Street
	Throston Engine House, Old Cemetery Rd
	Former Wesley Methodist Church, Victoria Road
	Steel Workers Ward Memorial, Westbourne Rd Social Club
	Church of St Mary, Durham Street
	Former Yorkshire Bank, 65 Church Street

Source: Hartlepool Borough Council, 2025

- 4.61 A derelict buildings and sites working group has been established for many years. The Working Group seeks to bring back into use and/or improve a priority list of buildings which does include some of the buildings from the list in table 26. The council has continued to work with owners to assist in bringing buildings back into use and/or improving them for safety reasons or so that they do not appear an eyesore on Hartlepool's Street scene.
- 4.62 Work on the Former Wesley Methodist Church on Victoria Road is ongoing. It has a listed building consent and planning permission for change of use to upper floors of main building and annex building into 36-bed hotel; change of use of upper ground floor of main building to mixed A3/A4 Use (restaurant and bar); change of use of lower ground floor of main building to form 5no. commercial units (to be in flexible A1 Use (retail), A2 Use (financial and professional services), A3 Use (restaurants and cafes) or A4 Use (drinking establishment. Remedial works on the building have commenced and maintenance improvements are ongoing.
- 4.63 The council hopes that by publishing an annual 'Heritage at Risk' register, vulnerable heritage assets across the borough will be highlighted and this could raise their profile and potentially introduce them to a new audience who may be able to resolve the problems individual heritage assets are suffering from. As part of the document case studies will be provided where buildings are removed from the list to provide examples of heritage assets where successful solutions have been found to provide inspiration to other owners in a similar situation.
- 4.64 Conservation area appraisals and management plans were formally agreed for the Headland and Seaton Carew in March 2025.

D. SUSTAINABLE TRANSPORT AND ACCESS TO THE COUNTRYSIDE

4.65 This section analyses policies related to the provision and improvement of the transport network in the borough. Such a network can offer access to employment opportunities as well as leisure and community facilities and can have the added benefit of reducing congestion and carbon emissions through reducing car usage. The transport and connectivity policies in the local plan have been prepared within the context of the transport challenges and ambitions of the wider Tees Valley sub-region.

Local Plan Spatial Objectives 16, 17: To ensure the provision of a safe, efficient and sustainable transport network, accessible to all and to strengthen transport links with the Tees Valley sub-region, region and beyond.

Related Policies

- Working with key partners, stakeholders and other local authorities to deliver an effective, efficient and sustainable transport network, within the overall context of aiming to reduce the need to travel (**INF1**).
- Delivering sustainable transport in Hartlepool by maximising the level of sustainable access to areas of development, particularly through good quality public transport services, safe and attractive well-lit pedestrian and cycle routes and by developing further opportunities for sustainable modes of transport to serve existing communities throughout the borough (**INF2**).
- Protecting and enhancing the countryside and coastal areas and to make them more accessible for the benefit of the residents and visitors to the borough (**INF2**).

Core Output Indicator **T1**: Number & lengths of roads created, improved to reduce congestion.

Core Output Indicator **T2**: Number & lengths of cycleways created, improved or lost.

Core Output Indicator **T3**: Number, Types & lengths of public walkways and coastal routes created, diverted improved or lost.

Transport Policies Assessment: Cycleways and Roads

4.66 The local plan makes provision for the continued improvement of roads and development of a comprehensive network of cycle routes linking the main areas of the borough. The programme of cycleway improvements supported under the Tees Valley Combined Authority (TVCA) Local Growth Fund (LGF) 'Sustainable Access to Employment' programme continues to plan the construction of cycleways across the borough in accordance to the Hartlepool Cycling Development Plan which is a constantly evolving document (www.hartlepool.gov.uk/cycleplan).

4.67 Following the successful LGF programme the council is currently working with TVCA on the Tees Valley Local Cycling and Walking Infrastructure Plan (LCWIP).

This is a Tees Valley wide plan which includes Hartlepool. Key projects will be the development of cycling corridors.

4.68 Hartlepool schemes currently being explored are as follows:

- Transport Interchange to Headland (LCWIP scheme via City Region Sustainable Travel Settlement funding).
- Transport Interchange to Wolviston (LCWIP via CRSTS).
- A689 to Summerhill (LUF).
- A689/Victoria Road to Mill House area (LUF/MDC funding)
- Town Centre – Waterfront (LUF)

4.69 The above schemes are in various stages of development with 3 having been submitted to an Active Travel England Design Review Panel, and it was anticipated that all will be on site by autumn 2025. However, these schemes are not on site yet and start date has been pushed back to Winter / spring 2026.

4.70 In addition to these schemes the transport department is looking at carrying out improvements to the existing cycleway along the full length of Catcote Road (LUF). There is a proposal to implement a footway cycleway replacing the existing footway at the A689 southwest development access to Dalton back Lane (section 106 works) and an informal crossing across the A689 to Greatham back Lane.

4.71 This year one new cycleway has been constructed at the A689 Sappers Corner to southwest development access (section 278 works). It replaces the existing footway and is 472 metres long (table 27). At the A179 / A1086 / west view Road roundabout there has been a provision of 2 toucan crossings with pedestrian / cycleway links to existing pedestrian/ Cycleway routes. (section 278 works).

Table 27: Roads and cycleways 2024/25

			Created/New	Diverted	Extinguished	Improved
2024/25	Cycleway	Name	A689 Sappers Corner to southwest development access	None	None	None
		Length(m)	472	None	None	None
	Roads	Name Length (m)	None	None	None	None
2023/24	Cycleway	Name	Merlin Way Implementation of Footway / Cycleway connecting Upper Warren with Local Centre	None	None	None
		Length (m)	253	None	None	None
		Name	Marina Way to Seaton Promenade	None	None	None
	Length (m)	381	None	None	None	
	Roads	Name Length (m)	None	None	None	None

2022/23	Cycleway	Name Length (m)	None	None	None	None
	Roads	Name Length (km)	None	None	None	None
2021/22	Cycleway	Name Length (m)	None	None	None	None
	Roads	Name Length (km)	None	None	None	None
2020/21	Cycleway	Name	A689 Cycleway/Walkway (east side) from Brenda Road roundabout to Windermere Road	None	None	A689 Cycleway/Walkway (west side) from Brenda Road roundabout to Burn Road roundabout
		Length (m)	300	None	None	1,180
	Roads	None	None	None	None	None
2019/2020	Cycleway	Name	None	None	None	1) A689 to Brenda Road cycleway/walkwayupgrade 2) Bishop Cuthbert Access Improvements
		Length (m)	None	None	None	1) 1020 2) 2950
	Roads	Name Length (km)	None	None	None	None
2018/2019	Cycleway	Name	Brenda Road cycle lanes	None	None	NCN14 upgrade North Burn to Cowpen Bewley
		Length (m)	6 900	None	None	670
		Name	Queens Meadow	None	None	None
		Length (m)	475	None	None	285
		Name	Oakesway	None	None	None
	Length (m)	50	None	None	None	
Roads		None	None	None	None	

Source: Hartlepool Borough Council, 2025.

Transport Policies Assessment: Walkways and Coastal Paths

- 4.72 The council will continue to improve access to the countryside and furniture within the countryside so that a more inclusive network will be available to a broader user base. This entails creation of new footpaths and improvement works to the network of existing footpaths. The enacted Marine and Coastal Access Path Act 2009 placed a duty for a coastal path to be created along the whole of the English coastline. The first section of the England Coastal Path is in place between the North Gare car park at Seaton Carew and Sunderland. The council continues to support initiatives to extend the England Coastal Path southwards from its current terminus at North Gare car park. Table 28 shows developments in relation to Indicator T3.
- 4.73 Table 28 shows that 1.08 km of public rights of ways and 0.55 km of permissive paths have been created this year. Policy INF2 continues being implemented and moving in the right direction hence there is no need for the policy to be reviewed. The council will continue to ensure the provision of a safe, efficient and

sustainable transport network, accessible to all and to strengthen transport links with the Tees Valley sub-region, northeast region and beyond.

Table 28: Walkways and Coastal Paths 2024/25

Core Output Indicator T3						
	Type of Walkway	Created (km)	Diverted (km)	Extinguished	Improved (km)	Locations of paths changed throughout the year
2024/25	Public Rights of Way	1.08	0.63	0.08	0.21	Hartlepool 25 and 7, Hartlepool 3, Hartlepool 43, Greatham 22, Elwick 7
	Permissive Paths	0.55	0.00	0.00	0.50	Pentagon north, Wynyard Park Merlin Way to Kingfisher Close
	Permissive Paths	0.00	0.00	0.00	1.2	Graythorpe Industrial Estate to Venator Hartlepool Golf Club
2023/24	Public Rights of Way	0	0	0	2.4	Dalton Piercy 3, Elwick 7, Hartlepool 8
	Permissive Paths	0	0	0	0.76	Seaton Walkway, Springwell Community woodland
	England Coast Paths	0	0	0	1.68	Greatham Creek to Conoco Phillips, Seaton golf Club
2022/23	Public Rights of Way	0.45	0	0	1.26	Thorn Tree Lane, Greatham Public Bridleway No 1a, Seaton Parish Public Footpath No.16, Hartlepool
	Permissive Paths	0	0	0	0.69	Brierton Lane to Summerhill Permissive Bridleway
	England Coast Paths	0	0	0	2.8	Hartlepool Golf Club, Marine Point Spion Kop Cemetery to Marine Drive, Yacht Club Seaton Common
2021/22	Public Rights of Way	0	0.28	0	0.36	Public Footpath No.3,Dalton Piercy Public Byway No.30, Bilingham Parish Public Byway No.3, Seaton Public Byway No.5, Seaton Public Footpath No.31, Hartlepool Public Byway No.1a, Seaton
	Permissive Paths	0	0	0	0.96	Seaton Walkway, Tees Road
	England Coast Paths	0	0	0	1.28	Old Cemetery Rd, Yacht Club
2020/21	Public Rights of Way	0.4	0.75	0	0.4	Public Footpath No.23 Greatham Public Footpath No.31 Hartlepool Public Footpath No.11Seaton Public Footpath No.3 Dalton Piercy
	Permissive Paths	0	0	0	0	

	England Coast Paths	0	0	0	0.65	Greatham Creek, Brenda Road and Graythorp Industrial Estate
2019/20	Public Rights of Way	0	0	0	0	
	Permissive Paths	0	0	0	0.75	Springwell Community Woodland, Clavering
	England Coast Paths	5.17	0	0	0.3	Tees Road/A178 – Graythorp Industrial Estate, Conoco Phillips, Greatham Creek

Source: Hartlepool Borough Council, 2025.

5. CONCLUSION AND RECOMMENDATIONS

- 5.1 This year, there is a net delivery of 470 dwellings (i.e. 467 new builds and 3 change of use). The net delivery is lower than last year but is above both the baseline housing target of 400 and the revised target of 464 dwellings/annum. It is also above the Objectively Assessed Need (OAN) of 287 dwellings/annum, and this is notably positive development again this year, like last year. The cumulative under delivery continues to decline and this year it is -36 compared to last year at -106.
- 5.2 There is a total of 166 new starts mainly from Greenfield sites such as Wynyard housing development sites, Upper Warren, Marine Point, Quarry Farm 2, Elwick Park, Antler Park and Woodside Meadows in Seaton. Completions from these will add on to next year's net completions. It is anticipated that some of the strategic housing sites allocated in the local plan may obtain planning permission. It is therefore expected that housing delivery will continue to increase next year and continue to meet the annual delivery target.
- 5.3 The overall averaged annual target of 410 dwellings is still considered to be an achievable, therefore there is currently no need to revise it or any of the housing policies.
- 5.4 A total of 93 completed affordable homes (all new builds) was achieved this year and it remains higher than the local plan target of 74 affordable homes. The completions were mainly from existing large housing sites such as the Marine Point, Upper Warren, Woodside Meadows and Antler Park. Thirteen Group own over 50% of the completed affordable homes. A total of £363,000.00 S106 funds was received for offsite affordable housing this year (appendix 5) and £59 000 was spent on affordable housing schemes. Through the year, signed S106 agreements have secured a total of 67 onsite units (this includes 42 DMVs) and £9,641.00 in money (appendix 6). It is anticipated there will be more delivery of affordable homes throughout next year from large housing sites as they continue to build out.
- 5.5 The council will continue to support the delivery of additional affordable housing through building on council-owned land, partnership working with Registered Providers in the borough and through securing affordable housing contributions as part of private residential developments.
- 5.6 Housing policies are continually being implemented in accordance with the local plan and the housing targets are still considered to be achievable. There is therefore currently no need to revise any of the housing policies or any of the housing target figures.
- 5.7 For the third year running, there is no net employment land uptake. However, there is 30.7 ha loss of allocated employment land to housing at Wynyard (application ref H/2022/0181). Available employment land has thus reduced to 164.5ha from 195.2ha. There is no major negative, unjustified diversion of employment policies since precedence has been set for housing at Wynyard in

previous years. There is therefore no need to revise and amend any of the employment policies.

- 5.8 For the past 5 years the vacancy rate in terms of floorspace has been increasing in the town centre. This year it is 24.8%, up from 19.6% last year. Lack of high-quality shops and vacancy rates in the town centre remains a challenge. Several shops closed, and this has significantly increased the vacancy rate this year. At the time of writing this report, Boyes departmental store opened doors to the public, but the additional floor space will be reported next year, and this will reduce vacancy rates next year provided existing open shops do not close.
- 5.9 Although the vacancy rate is higher this year, the retail and commercial policies are still considered to be performing as expected since the town centre continues to develop as a retail hub with E and F1 uses accounting for more than 50% of all uses in the town centre, followed by F2 uses in accordance with the Local Plan. This indicates that appropriate planning permissions are being granted for town centre uses within the town centre and there is therefore no evident policy diversion. There is currently no need to amend any of the town centre retail and commercial policies as they continue being implemented.
- 5.10 Tourism policies within the local plan identify the Town Centre, the Marina, the Headland and Seaton Carew as main tourism destinations and the policies encourage appropriate developments within these areas. However, rural tourism through recreation and leisure developments could be allowed under certain circumstances. Same as last year, there are no approved tourist-related planning applications this year. Building at the former Jackson's Landing progressing well at the Marina (i.e. erection of a leisure and community building including swimming pools, fitness suites, studios, cafe and ancillary spaces, external landscaping including public events space, car parking, vehicle drop off/collection and infrastructure improvements). Tourism policies continue to be implemented and there is currently no need for an update.
- 5.11 This year there has been available data with regard to loss or creation of biodiversity habitat. There were no losses or gains in the total area of habitat in international or national designated sites. It is still recommended that housing applications are more closely monitored to ensure compensation/mitigation measures are appropriately put in place to avoid losses of wildlife habitat as this is detrimental to the natural environment and biodiversity gain. Notwithstanding the above the natural environment policies are being implemented and there is currently no need to update them.
- 5.12 The total amount of waste arising this year increased in comparison to the previous year and recycled/composted waste continued to decrease from 27.71% last year to 27.2% this year. The increase in waste could be attributed to more properties being serviced because of more house completions and families moving in during the year. The council continues to communicate with residents and the wider community via Hartbeat magazine, the council's own website and social media, to help them to recycle more of their waste. Messages centre on what can and cannot be recycled and bins do not get collected if waste is mixed up.

- 5.13 Like last year, there were no permitted unjustified residential developments in the countryside outside limits to development hence the rural development policies continue to be implemented.
- 5.14 No new roads have been constructed this year, however, there is one new cycleway constructed at the A689 Sappers Corner to southwest development access (section 278 works). It replaces the existing footway and is 472 metres long. At the A179 / A1086 / west view Road roundabout there has been a provision of 2 toucan crossings with pedestrian / cycleway links to existing pedestrian/ Cycleway routes. (section 278 works).
- 5.15 There has been 1.08 km of public rights of ways and 0.55 km of permissive paths created this year. Policy INF1,2 continue being implemented and moving in the right direction hence there is no need for the policies to be reviewed. The council will continue to ensure the provision of a safe, efficient and sustainable transport network, accessible to all and to strengthen transport links with the Tees Valley sub-region, northeast region and beyond.
- 5.16 Overall as illustrated by the assessment of the local plan policies, all policies are performing in accordance with the Local Plan, there are no evident policy diversions although employment land take up remains static and available employment remains the same year on year. It is recommended to consider de-allocating some of the employment land in the next local plan. Policies will continue to be monitored annually throughout the local plan period to ensure they are being implemented and that targets are also being met as planned and any poor performing or diverting policies will be flagged up and recommendations given.

Appendix 1: Adopted Local Plan Policies

Theme	Policy	Policy Code
The Locational Strategy	Locational Strategy	LS1
Minimising and Adapting to Climate Change	Minimising and adapting to Climate Change Reducing and Mitigating Flood Risk Renewable and Low Carbon Energy Generation Strategic Wind Turbine Developments Large Scale Solar Photovoltaic Developments	CC1 CC2 CC3 CC4 CC5
Infrastructure	Sustainable Transport Network Improving Connectivity in Hartlepool University Hospital of Hartlepool Community Facilities Telecommunications	INF1 INF2 INF3 INF4 INF5
Quality of Place	Planning Obligations Compulsory Purchase Orders Location, Accessibility, Highway Safety and Parking Layout and Design of Development Safety and Security Technical Matters Energy Efficiency Advertisements	QP1 QP2 QP3 QP4 QP5 QP6 QP7 QP8
Housing	New Housing Provision Ensuring a Sufficient Supply of Housing Land Overall Housing Mix Urban Local Plan Sites The South West Extension Strategic Housing Site High Tunstall Strategic Housing Site Quarry Farm Housing Site Wynyard Housing Developments Elwick Village Housing Development Hart Village Housing Developments Affordable Housing Housing Market Renewal Extensions to Existing Dwellings Residential annexes Gypsy and Traveller Provision	HSG1 HSG1a HSG2 HSG3 HSG4 HSG5 HSG5a HSG6 HSG7 HSG8 HSG9 HSG10 HSG11 HSG12 HSG13
Strengthening the Local Economy	Prestige Employment Site Wynyard Business Park Queen's Meadow Business Park General Employment Land Specialist Industries Safeguarded land for new Nuclear Power Station Underground Storage	EMP1 EMP2 EMP3 EMP4 EMP5 EMP6
Protecting, Managing and Enhancing the Rural Area	Development in the Rural Area New Dwellings Outside of Development Limits Farm Diversification Equestrian Development Rural Tourism Rural Services	RUR1 RUR2 RUR3 RUR4 RUR5 RUR6

Retail and Commercial Development	Retail and Commercial centre Hierarchy The Town Centre Innovation and Skills Quarter Avenue Road / Raby Road Edge of Town Centre Area The Brewery and Stranton Edge of Town Centre Area East of Stranton Edge of Town Centre Area Lynn Street Edge of Town Centre Area Mill House Edge of Town Centre Area Park Road West Edge of Town Centre Area West Victoria Road Edge of Town Centre Area York Road South Edge of Town Centre Area The Marina Retail and Leisure Park West of Marina Way Retail and Leisure Park Trincomalee Wharf Retail and Leisure Park Tees Bay Retail and Leisure Park The Local Centres Late Night Uses Area Hot Food Takeaway Policy Main Town Centre Uses on Employment Land Business Uses in the Home Commercial Uses in Residential Areas	RC1 RC2 RC3 RC4 RC5 RC6 RC7 RC8 RC9 RC10 RC11 RC12 RC13 RC14 RC15 RC16 RC17 RC18 RC19 RC20 RC21
Leisure & Tourism Development	Leisure and Tourism Tourism Development in the Marina Development of Seaton Carew Tourism Accommodation Caravan Sites and Touring Caravan Sites Business Tourism, Events and Conferencing	LT1 LT2 LT3 LT4 LT5 LT6
Historic Environment	Heritage Assets Archaeology Conservation Areas Listed Buildings and Structures Locally Listed Buildings and Structures Historic Shopping Parades Heritage at Risk	HE1 HE2 HE3 HE4 HE5 HE6 HE7
Natural Environment and Green Networks	Natural Environment Green Infrastructure Green Wedges Ecological Networks Playing Fields Protection of Incidental Open Space : Landscaping along main transport corridors	NE1 NE2 NE3 NE4 NE5 NE6 NE7

Appendix 2: Neighbourhood Development Orders and Neighbourhood Development Plans

Hartlepool Rural Plan

The Hartlepool Rural Plan was adopted in December 2018 and now forms part of the Development Plan for Hartlepool. It can be accessed at the council website using the following link:

https://www.hartlepool.gov.uk/downloads/download/589/03_hartlepool_rural_neighbourhood_plan

The Headland Neighbourhood Plan

The Headland Neighbourhood Planning Group secured a grant through the Supporting Communities in Neighbourhood Planning Programme to assist them with delivering events and to raise awareness about Neighbourhood Planning but also to commission some consultancy support to develop their Neighbourhood Planning policies. A first draft of the plan has been prepared and the group has reviewed it. This process involved consultation with the local community on current issues and priorities to bring the document up-to-date. The group was successful in obtaining a grant via Locality to undertake the consultation and to commission the council to produce a proposals map. The works were complete prior to 2020 but development of the plan has ceased.

Wynyard Neighbourhood Plan

The Wynyard Neighbourhood Plan Working Group, a sub-committee of the Wynyard Residents Association accessed funding from the Supporting Communities in Neighbourhood Planning Programme to support the initial stages of plan development. The group commissioned the services of a consultant to assist with the preparation of a first draft of the plan. They also secured the support of consultants Aecom (via Locality) to prepare a masterplan for the Wynyard Neighbourhood Plan area.

The policies of the plan have been drafted and the group previously undertook a consultation exercise with residents in the plan area prior to completing the first draft of the plan. Consultation in relation to the early stages of plan development included a household survey conducted within the Neighbourhood Plan boundary and through a number of local community events and workshops. Work on the plan has not progressed for several years.

Appendix 3: Duty to Cooperate

This section reflects the requirements of section 33A of the Planning and Compulsory Purchase Act 2004⁸ (Duty to co-operate in relation to planning of sustainable development) in relation to the time period covered by this report.

The Duty to co-operate requires:

- councils and public bodies to ‘engage constructively, actively and on an ongoing basis’ to develop strategic policy
- councils to have regard to the activities of the other bodies; and
- councils to consider joint approaches to plan making.

The bodies that the council must cooperate with are set out in the Town and Country Planning (Local Planning) (England) Regulations 2012, part 2, duty to cooperate, (4) (1).

As a small local authority we recognise the benefits of working collectively with our partners both across Hartlepool and within the wider Tees Valley and North East region.

We play a key role in a number of cross sector partnerships within Hartlepool including:

- Health and Wellbeing Board
- Safer Hartlepool Partnership
- Children’s Strategic Partnership
- Economic Regeneration and Tourism Forum
- Town Deal Board

We also play an active role within the Tees Valley Combined Authority, the Cleveland Local Resilience Forum and the Teeswide Safeguarding Adults Board.

The council have been involved in the establishment of the largest Integrated Care Partnership (ICP) in the Country, covering 13 local authorities. We have influenced the governance structure so that there are 4 Integrated Care Boards (ICBs) including one for the Tees Valley and are currently working with ICB officers regarding how it will operate at a Place level. The Leader of the council will sit on the ICP representing elected members.

Officers and elected members also work closely with colleagues across the region through a number of theme based networks including those related to adult social care, children’s services, finance, transport, nature, planning, equality and health scrutiny.

Preparation of the Hartlepool Local Plan

⁸ PACA as updated by section 110 of the 2011 Localism Act

The Inspectors final report was received on the 13th April 2018 and the local plan was adopted by full council on the 22nd May 2018.

During preparation of the local plan, a series of meetings were held with Stockton on Tees Borough Council to discuss key cross boarder issues that arose regarding housing, employment and transport at Wynyard. A statement of common ground was established and this was referred to during the examination stage of the local plan. Officers will continue to hold cross border meetings and targeted sessions with many stakeholders of a strategic nature during the implementation of the local plan should strategic issues arise.

Co-operation Relating to the Evidence Base

Hartlepool Borough Council commissioned various reports from consultants as well as produced its own work which fed into the evidence base for the local plan. A number of these evidence studies have been carried out jointly or in liaison with the other Tees Valley local authorities where the issue was strategic and crossed the administrative boundary. All Tees Valley local authorities were consulted in the production of all our evidence base work.

Summary of Co-operation in Relation to the Local Plan

Organisation	Nature of Co-operation
Environment Agency	Formally consulted at Issues and Options, Preferred Options, Publication stages of the plan production and at the Main Modifications Stage. Were partners in the development of key evidence base documents including the SHLAA, Water Cycle Study, SFRA Level 1 and Level 2, the SFRA update and the Strategic Sequential Test and the Local Infrastructure Plan.
English Heritage	Were formally consulted at the Issues and Options, Preferred Options, Publication stages of the plan production and at the Main Modifications Stage. Were consulted on key evidence base documents including the SHLAA and the Strategy for the Historic Environment.
Natural England	Were formally consulted at Issues and Options, Preferred Options, Publication stages of the plan production and at the Main Modifications Stage. Were consulted in the production of many of the evidence base documents including the SHLAA, SFRA Level 1 and 2, Tees Valley Green Infrastructure Plan. Key partner in the development of the English Coastal Path.
Civil Aviation Authority	Were formally consulted at Issues and Options, Preferred Options and Publication stages of the plan production.
Homes and Communities Agency (and more latterly Homes England)	Were formally consulted at Issues and Options, Preferred Options, Publication stages of the plan production and at the Main Modifications Stage.
Highways England	Were formally consulted at Issues and Options, Preferred Options, Publication stages of the plan production and at the Main Modifications Stage. Regular meetings have been held with regional representatives of the HA. Highways England has been instrumental in the production of many of the evidence base documents including the Local Infrastructure Plan, the SHLAA etc. Key partner in the management of development at Wynyard. Involved in the preparation and a signatory in a number of Statements of Common Ground.

North Tees & Hartlepool Primary Care Trust	Were formally consulted at Issues and Options, Preferred Options, Publication stages of the plan production and at Main Modifications Stage and for many of the evidence base documents.
The Office of Rail Regulation	Were formally consulted at Issues and Options, Preferred Options, Publication stages of the plan production and at Main Modifications Stage and for many of the evidence base documents
Marine Management Organisation	Were formally consulted at Issues and Options, Preferred Options, Publication stages of the plan production and at Main Modifications Stage and for many of the evidence base documents
Tees Valley LEP (and the Combined Authority)	Were formally consulted at Issues and Options, Preferred Options, Publication stages of the plan production and at Main Modifications Stage and for many of the evidence base documents
Tees Valley Local Authorities	Continuous Informal Engagement and statutory consultations. Formal and informal Engagement at DPO, Planning Managers and Directors of Place Meetings. Engagement at Tees Valley Infrastructure Group. Production of joint evidence base documents. Cross border liaison meeting held with Stock-on Tees Borough Council On-going co-operation with Stockton-on-Tees Borough Council regarding the management of development at Wynyard.
Durham County Council	Were formally consulted at Issues and Options, Preferred Options, Publication stages of the plan production and at Main Modifications Stage A cross border liaison meeting was held with Durham County Council on 12th April, 2016.
Parish Councils	Were formally consulted at Issues and Options, Preferred Options, Publication stages of the plan production and at Main Modifications Stage.
Cleveland Police	Were formally consulted at Issues and Options, Preferred Options, Publication stages of the plan production and at Main Modifications Stage.
The Coal Authority	Were formally consulted at Issues and Options, Preferred Options, Publication stages of the plan production and at Main Modifications Stage.
National Grid	Were formally consulted at Issues and Options, Preferred Options, Publication stages of the plan production and at Main Modifications Stage.
Northern Gas Networks	Were formally consulted at Issues and Options, Preferred Options, Publication stages of the plan production and at Main Modifications Stage.
Anglian Water	Were formally consulted at Issues and Options, Preferred Options, Publication stages of the plan production and at Main Modifications Stage.
Sport England	Were formally consulted at Issues and Options, Preferred Options, Publication stages of the plan production and at Main Modifications Stage.

Appendix 4: Community Infrastructure Levy (CIL)

The main reason for introducing a CIL is that it would provide a means of securing developer contributions from all qualifying developments to ensure funds are available to cover the cost of new infrastructure required to enable development and to help give clarity to developers on what they will be required to contribute as part of a development. However, paying the compulsory CIL levy would be subject to viability of the development and will be charged on a scale of rates.

Whilst the council will keep the situation under review, the present stance is that CIL will not be implemented within the borough. The Local Authority, as part of the adoption of the Planning Obligations SPD (November 2015), undertook an assessment of viability on different size development types across the borough, using evidence from viability assessments which have taken place over the past couple of years. The assessments built in the requested developer contributions and looked at varying levels of affordable housing to ascertain a deliverable affordable housing target for the SPD and Local Plan.

The assessments illustrated that the affordable housing need of 44% left developments unviable. A range of scenarios were looked at which identified that a target for affordable housing of 18% should be set.

In undertaking the work and in assessing viability of developments over the past couple of years, it has become apparent that there is very little viability on Brownfield sites within the urban area and to apply CIL to those would render them unviable and therefore prohibit development in the borough. Even on Greenfield sites both within and on the edge of the urban area viability has had to be considered, and has differed, on a site-by-site basis. As such it is not considered that the adoption of CIL in the current market conditions would be viable and would likely constrain future housing growth within the borough due to concerns over viability of developments.

This position was highlighted at the local plan examination and it was confirmed to the Planning Inspector that CIL would not be taken forward as part of the local plan and that s106 agreements would continue to be used to secure planning obligations where viable. A Deliverability Risk Assessment was produced to support the local plan and illustrate the deliverability of certain types of development when contributions were factored in. This was agreed by developers at the examination.

Appendix 5: Developer Contributions S106 received funds 2024/25

Amount of money in £000s
(thousands)

Type of contribution	*Opening balance as of 1st April 2024	Amounts received in 2024/25	Adjusted Total	Actual Expenditure	*Actual committed balance remaining as of 31st March 2025
**** Affordable Housing	1,131	363	1,494	59	1,435
Bus Stop	0	0	0	0	0
Coastal	132	26	158	0	158
Cycleways & Interchange	1,305	246	1,551	76	1,475
*****Ecological Mitigation	207	58	265	0	265
Education	3,324	1,777	5,101	0	5,101
Green Infrastructure	113	70	183	59	124
Health	159	103	262	0	262
Highways	2,640	0	2,640	0	2,640
Highways General	37	504	541	0	541
Maintenance	0	0	0	0	0
**Offsite Recreational	66	0	66	6	60
Play	118	6	124	3	121
Public Art	5	0	5	0	5
***Restoration-security bond	100	0	100	0	100
Sports	241	87	328	137	191
Traffic Calming	9	0	9	0	9
TOTAL	9,587	3240	12,827	340	12,487

Source Hartlepool Borough Council, 2025

*Balances relate to amounts received and earmarked for purposes specified in developer agreements but not yet transferred to a budget/scheme. However further expenditure is in the process of being planned and committed.

** To be used in vicinity of the Britmag site including but not limited to Central Park.

*** Held as security in case of any remedial action required during the operational life of the Wind Turbine.

**** All affordable housing contributions are earmarked for additional HRA houses.

***** This includes £27k for 'Dog Control orders' however if the measures are not needed then the funds are repayable.

Off Site - part of these contributions are included in the Capital NIP scheme Budget Project 7440

Sports also includes contributions for Playing Pitches, Tennis Courts and Bowling Greens

Highways S106 balance of £2,640m is included on the CIP appendix for Elwick scheme in the rephased forecast.

Appendix 6: Developer Contributions S106 signed agreements 2024/25

Application No	Date of Agreement	Contribution (£s)												
		AH	BG	BS	CF	EC	EDP	EDS	EMP	GI	HW	Play	PP	TC
H/2022/0405	25/03/2025									7,000.00				
H/2015/0283	27/02/2025	5 onsite	248.50	12,500.00		5,000.00	295,132.50	96,739.72		12,500.00		12,500.00	11,664.50	2,851.00
H/2022/0181	25/02/2025				482.50 NHS				150,000.00					
H/2022/0470	07/02/2025													
H/2023/0315	27/01/2025													
H/2023/0012	20/12/2024		44.73	2,250.00						2,250.00			2,099.61	513.18
HMDC/2023/0019	30/09/2024							11,591.84		8,000.00				
H/2021/0096	11/09/2024												1,399.74	342.12
H/2022/0046	11/09/2024	2 DMVs												
H/2014/0405	20/06/2024	61 onsite		200,000.00			3,726,300.00	2,434,287.24			1,200,000.00	190,547.60		
H/2021/0572	25/04/2024									564,000.00				
H/2019/0491	23/04/2024									13,850.00				
H/2021/0204	22/04/2024	9,641.00						1,931.97	2,888.55	250.00		250.00	233.29	57.02
TOTAL (£s) Secured		67 onsite £9,641.00	293.23	214,750.00	482.50 NHS	5,000.00	4,021,432.50	2,544,550.77	152,888.55	607,850.00	1,200,000.00	203,297.60	15,397.14	3,763.32

Source Hartlepool Borough Council, 2024/25.

Key:

- GI - Green Infrastructure
- EC - Ecology
- BS – Built Sport facilities
- BG - Bowling Greens
- PP – Play Pitches
- TC- Tennis Courts
- AH - Affordable Housing
- HW - Highways
- EDP - Education Primary
- EDS – Education Secondary
- EMP - Employment



Hartlepool
Borough Council

Neighbourhoods and Regulatory Services Committee

2 March 2026

Report of: Assistant Director (Regulatory Services)

Subject: SELECTIVE LICENSING UPDATE

Decision Type: For information

1. Council Plan Priority

Hartlepool will be a place:
- where people live healthier, safe and independent lives. (People)
- that is connected, sustainable, clean and green. (Place)

2. Purpose of Report

- 2.1 The purpose of this report is to update Members on progress in assessing the viability of introducing a new selective licensing scheme in Hartlepool.

3. Background

- 3.1 Section 80 of the Housing Act 2004 (the Act) introduced a discretionary power for Local Authorities to introduce Selective Licensing (SL) schemes for a five-year period in areas which are experiencing one or more of the following conditions:

- Low housing demand (or is likely to become such an area)
- A significant and persistent problem caused by anti-social behaviour
- Poor housing conditions
- High levels of deprivation
- High levels of crime
- High levels of migration

The purpose of such a scheme is to improve standards of property management in the private rented sector, and when combined with other

measures, this should lead to improved physical, social and economic conditions.

- 3.2 Where a selective licensing designation is made, it applies to privately rented housing in the area. Subject to certain exemptions, all properties in the private rented sector which are let or occupied under a licence, are required to be licensed by the local housing authority, unless the property is a in Multiple Occupation and is required to be licensed under part 2 of the Act.
- 3.3 Under the Act, all private landlords operating within the designated area are required to pay a fee and obtain a licence from the Council for each rented property. If a property is let without being licensed, or there is a failure to comply with licensing conditions, the authority can take enforcement action. The conditions of the licence ensure that the property is managed effectively, and licence holders have to demonstrate their compliance. The fees must be ring- fenced to fund the resources for the delivery of the scheme.
- 3.4 When considering whether to make a selective licensing designation a local housing authority must first identify the objective or objectives that a designation will help it achieve. The local housing authority must also consider whether there are any other courses of action available to it that would achieve the same objective or objectives as the proposed scheme without the need for the designation to be made.
- 3.5 Only where there is no practical and beneficial alternative to a designation should a scheme be made. If the local housing authority decides there is no practical and beneficial alternative to the scheme, it must only make the designation if it is satisfied that the scheme will significantly assist it in achieving its objective or objectives together with other actions the local housing authority may be taking.
- 3.6 There have been two previous selective licensing designations in Hartlepool. The first, which was in operation from 2009-2014, included six small areas of the Borough situated within the Burn Valley, Dyke House, Foggy Furze, Grange and Stranton wards. The second designation introduced in 2015 included 13 streets, some of which were included in the first designation: Cornwall Street, Kimberley Street, Richmond Street, Rydal Street, Borrowdale Street, Sydenham Road, Burbank Street, St Oswald's Street, Dent Street, Furness Street, Sheriff Street, Straker Street and Stephen Street.
- 3.7 In order to introduce the previous two schemes, it was a requirement to demonstrate that there was robust evidence to satisfy one or both of the prescribed general conditions relating to low housing demand and anti-social behaviour.
- 3.8 The Selective Licensing of Houses (Additional Conditions) (England) Order 2015 expanded the conditions to include poor property conditions; current

or recent experience of large amounts of inward migration; areas which have a high level of deprivation; or high levels of crime.

- 3.9 In December 2024, the approval process changed to allow local authorities to implement selective licensing schemes without needing prior approval from the Secretary of State if the scheme covered 20% or less of the private rented sector of geographical area. This new process was introduced to reduce bureaucracy allowing local authorities to more effectively address housing issues.
- 3.10 The Audit and Governance Committee completed an investigation into ‘Derelict land and buildings in Hartlepool’ in 2023/24. During this investigation, the disproportionately high levels of unoccupied properties in Hartlepool was a significant concern linked with the health and wellbeing of residents, increased risks of crime and antisocial behaviour and damage to community cohesion,
- 3.11 The final report of that committee recognised the impact of empty private accommodation on communities and the potential benefits of licensing on housing quality and the number of empty homes and it was recommended that the (then) Neighbourhood Services Committee explore the viability of introducing a new selective licensing scheme.
- 3.12 The Selective Licensing of Houses (Additional Conditions) (England) Order 2015 outlines the conditions that must be met for a designation to be made.
- 3.13 Any new designation may be subject to legal challenge by Judicial Review and it is therefore essential that the correct procedures are followed, including failure to comply with the requirement to produce robust evidence to support the designation or failure to properly consult on the proposal.

4. Progress

- 4.1 In order to ensure that we have a robust evidence base and to understand the current state of Hartlepool’s housing stock and enable an in-depth analysis of issues which would determine whether and where selective licensing would be a viable option, Metastreet Ltd have been commissioned to undertake an independent detailed review across the Borough and assess housing stressors relating to key tenures, particularly the Private Rented Sector (PRS). Metastreet use a stock-modelling approach based on metadata and machine learning to provide predictive insights about the:
- Current levels of PRS properties in Hartlepool
 - Tenure change over time
 - Levels of serious housing hazards in the PRS (Category 1/high category 2 hazards)
 - Other housing related stressors including crime, anti-social behaviour (ASB), service demand, population and deprivation linked to the PRS.

- 4.2 This approach has been used by a wide range of housing authorities to understand their housing stock and relationships with key social, environmental and economic stressors. The detailed housing stock information provided will help facilitate the development and delivery of Hartlepool's housing strategy, inform the selective licensing process and enable a targeted approach to tackling poor housing in the Borough.
- 4.3 A Data Protection Impact Assessment (DPIA) was undertaken prior to the appointment of Metastreet Ltd, which addressed data sharing, the use of data, data security and handling. The DPIA has now been signed off and data collection and mapping commenced.
- 4.4 The Housing Standards team has been expanded to undertake the feasibility work with the appointment of a temporary 0.5FTE Senior Environmental Health Officer (Housing) in October 2025. A further 0.5FTE Technical Officer/Housing Standards Officer post has been advertised with interviews due to be held in February 2026.
- 4.5 Data collection has commenced within and external to the Council. For more sensitive information, and some external services, it has been necessary to produce and secure approval of data sharing agreements. The relevant services to date are:
- Council Tax
 - Benefits
 - Cleveland Fire Brigade
 - Cleveland Police (in progress)
- 4.6 The benefits information required is available to the Council via the Department for Work and Pensions (DWP). A data re-use agreement from the DWP was necessary and has now been secured.
- 4.7 This process has been complex and time consuming but was essential to securing the data required to identify appropriate area/s for potential designation.
- 4.8 Data has now been collected and is being securely mapped from the following services/data:
- Electoral Register
 - Closure Orders
 - Car parking
 - Houses in Multiple Occupation (HMOs)
 - Registered Social Landlords (part)
 - Private Sector Housing
 - Planning Enforcement
 - ASB
 - Council Tax
- 4.9 To gain a better understanding of how selective licensing is operating in other Local Authorities, meetings have been held with colleagues from

other Local Authorities in the region and work has started to understand the nature of other schemes, the scale of such schemes, resources required and fee levels.

The key issues identified to date by Local Authorities who have (or are in the process of) introducing selective licensing are:

- Determining the most appropriate area for designation.
- Ensuring adequate resources to cover staffing, training and administration of the scheme.
- Difficulty in recruiting enough suitably skilled staff to operate the scheme effectively.
- Recognising the need for a diverse skill set within the team, including transferable skills from roles such as estate agents, community wardens etc
- Providing comprehensive and intense training for the majority of staff and the ability to provide that training in a timely fashion with associated cost and time implications.
- Accurately assessing scheme costs and setting an appropriate level of licence fee. The fee should ideally cover the cost of the scheme and not include discounted rates.
- The benefits of having access to specialist legal support as successful enforcement and (ultimately) prosecution are key to the success of the scheme's credibility.
- Partnership working particularly with ASB and Police Officers.
- Managing Landlord (and Tenant) resistance including unwillingness to engage in the scheme, legal challenges and negative publicity.

Further details will be provided in a future report.

5. Future Action

- 5.1 An action plan has been developed outlining key actions, expected outputs with associated timelines:
- Interview for a 0.5FTE Technical Officer/Housing Standards Officer post to be held in February 2026.
 - Single Impact Assessment to be produced by end February 2026
 - Mapping of data and analysis to be completed by end March 2026
 - Further liaison with other Local Authority Selective Licensing services with a view to sharing good practice and gaining intelligence on elements of various schemes by end April 2026
 - Consideration to be given to other courses of action available that would achieve the same objective or objectives as selective licensing without the need for the designation to be made. To be completed by end March 2026
 - Consider the findings from Metastreet report and other evidence. June 2026
 - Report to members on options and approval to go to consultation by June/July 2026
 - Consultation with key stakeholders using a variety of methods for 12 weeks during August – December 2026

- Report back findings from consultation in February-March 2027 with final recommendations
- 5.2 A further detailed report to be taken Neighbourhoods and Regulatory Services Committee in June/July 2026 which will set out the selective licensing designation process and include the key findings from the Metastreet Ltd report and data analysis. It will also set out alternative options.
- 5.3 The report will seek approval for the next steps including further exploration of alternative options and/or whether to undertake consultation on a proposed selective licensing scheme.

To proceed to consultation, the following:

- a. Summary of evidence
- b. Proposed Licence conditions
- c. Proposed fee structure
- d. Map(s) of proposed area(s)
- e. Details of the strategic fit
- f. Evidence that we have looked at alternative options

6. Other Considerations/Implications

RISK IMPLICATIONS	No relevant issues at this stage
FINANCIAL CONSIDERATIONS	No relevant issues at this stage
SUBSIDY CONTROL	No relevant issues at this stage
LEGAL CONSIDERATIONS	No relevant issues at this stage
SINGLE IMPACT ASSESSMENT	SIA to be completed
STAFF CONSIDERATIONS	No relevant issues at this stage
ASSET MANAGEMENT CONSIDERATIONS	No relevant issues at this stage
ENVIRONMENT, SUSTAINABILITY AND CLIMATE	No relevant issues at this stage

CHANGE CONSIDERATIONS	
CONSULTATION	No relevant issues at this stage

7. Recommendations

7.1 That members note the report.

8. Reasons for Recommendations

8.1 This is an update report and does not require action at this stage.

9. Background Papers

9.1 There are no background papers for this report.

10. Contact Officers

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Sign Off:-

Chief Executive	Date: 09/02/2026
Director of Finance, IT and Digital	Date: 09/02/2026
Director of Legal, Governance and HR	Date: 08/02/2026



Neighbourhoods and Regulatory Services Committee

2 March 2026

Report of: Director for Neighbourhood and Regulatory Services

Subject: PLANNING OBLIGATIONS SUPPLEMENTARY PLANNING DOCUMENT (SPD) AND HOUSES IN MULTIPLE OCCUPATION (HMO) ARTICLE 4 DIRECTION AND SPD UPDATE.

Decision Type: For information

1. Council Plan Priority

Hartlepool will be a place:
where people live healthier, safe and independent lives. (People)
that is connected, sustainable, clean and green. (Place)
that is welcoming with an inclusive and growing economy providing opportunities for all. (Potential)
with a Council that is ambitious, fit for purpose and reflects the diversity of its community. (Organisation)

2. Purpose of Report

2.1. The purpose of this report is to update members on the progress of three key consultations undertaken by the Land Use Policy Team in recent months. The consultations focused on:

- A borough wide Houses in Multiple Occupation (HMO) Article 4 Direction.
- A HMO Supplementary Planning Document (SPD)
- A Planning Obligations Supplementary Planning Document (SPD).

2.2. The report also seeks permission to update the two draft SPDs and submit to Full Council for Adoption in May 2026.

3. Background

3.1. In summer 2025 the Land Use Policy Teams began preparing for the process of consulting upon a borough wide HMO Article 4 Direction, a HMO SPD and an update to the 2015 Planning Obligations SPD. This process involved liaison with key HBC officers such as the legal team, highways team, licensing team, the drafting of documents and then formal consultation.

3.2. On 30th June 2025 a report was taken to Neighbourhood Services Committee to inform members of a proposed statutory consultation for potentially introducing a HMO Article 4 Direction within Hartlepool. The report was in response to concerns raised by members of the public and elected members in relation to HMOs across the borough.

3.3. On 20th October 2025 two reports were taken to Neighbourhood Services Committee for permission to consult upon a draft HMO SPD and the updated Planning Obligations SPD. The reports detailed that there is a need to introduce HMO guidance to ensure that applicants are aware of what will be expected should they seek planning permission for a HMO in Hartlepool. It was also set out that the 2015 Planning Obligations SPD required an update due to inflation, to provide more clarity to residents and stakeholders and to set out the parameters and information relating to Discount Market Value Affordable Homes.

3.4. With regard to the Article 4 Direction, a 12 month statutory consultation commenced on 24th September 2025, this consultation is necessary to ensure the Article 4 Direction can be implemented lawfully. In addition to the statutory 12 months consultation, the council undertook a 17 week (24th September 2025 to 23rd January 2026) information seeking consultation to gain residents and stakeholders views.

3.5. With regard to the two SPDs covering HMO Standards and Planning Obligations, information seeking consultations were undertaken for 11 weeks (3rd November 2025 to 16th January 2026) in a bid to obtain residents and stakeholders views.

3.6. For all of the consultations the HBC platform Your Say was utilised and consultation notifications were sent to those signed up to the platform. Key stakeholders and those within the Land Use Policy consultation database who have requested to be informed about changing HBC policy and guidance were sent e mails with all the key information and survey links contained. The consultations were advertised in Hartbeat magazine, a press release was issued, and documents were available online and in Hartlepool Civic Centre. Officers received a number of phone calls in which the consultations were discussed.

3.7. The consultations have now closed, but, as stated in Section 3.4, the statutory HMO Article 4 Direction consultation remains in place until 24th September 2026.

4. Proposals

4.1. The council is proposing to adopt the two SPDs in June 2026 and may bring into force the HMO Article 4 Direction on 1st December 2026 subject to Member agreement. A decision on adopting the SPDs will be taken by Full Council, a decision on how to proceed with the HMO Article 4 Direction will be brought to Neighbourhoods and Regulatory Services Committee and then Full Council in due course.

4.2. Officers are in the process of addressing the comments received to draw out key points. A comprehensive assessment and response to all comments will be included within the legally required consultations statements that will be provided alongside future Council reports to formally adopt the SPDs.

4.3. The paragraphs below set out information relating to the engagement undertaken to date and a summary of the key issues raised.

Houses in Multiple Occupation Article 4 Direction

4.4. 331 online responses were received; no letters were received to date, consultation is still open.

4.5. The key issues raised were as follows:

- 269 respondents were in support of introducing the HMO Article 4 Direction (81%).
- 62 respondents were not in support of introducing the HMO Article 4 Direction (18%).
- HMOs give rise to anti-social behaviour.

- HMOs take up car parking spaces and put pressure on the network.
- HMOs reduce community cohesion as people come and go.
- HMO Article 4 Direction will allow the council better control.
- HMO Article 4 Direction will give residents a voice and the chance to have their say.
- HMOs are needed for those who can't afford private rent.
- More affordable homes are needed across the borough.

Houses in Multiple Occupation Supplementary Planning Document

4.6. 30 online responses were received along with 1 letter from Historic England.

4.7. The key issues raised were as follows:

- Broad support for the draft SPD to be used as a tool to better manage and maintain HMOs.
- Broad support for avoiding the sandwich effect of HMOs with non HMO residential properties.
- Broad support that there should be no more than 3 HMOs in a row.
- Those disagreeing with limiting to no more than 3 in a row mostly think there should be fewer than this.
- Uncertainty about whether HMOs above commercial units is appropriate.
- General preference for there to be specific parking requirements for HMOs rather than flexibility.
- Concerns raised regarding parking pressures, antisocial behaviour and that there is already perceived to be too many HMOs in the town.

Planning Obligations SPD Supplementary Planning Document

4.8. 14 online responses were received and 2 letters were submitted. Letters submitted were from Sport England and Lichfileds (planning agent) on behalf of Taylor Wimpey.

4.9. The key issues raised were as follows:

- Should prioritise brownfield sites.
- There is a need for updated guidance from the council.
- Priorities for s106 spending –affordable housing, green infrastructure and community facilities.
- Broad support for play facilities.
- Broad support for outdoor sport (tennis courts and bowling greens).

- Broad support for the council's approach to green infrastructure provision.
- Broad support for contributions towards preserving heritage assets.

5. Other Considerations/Implications

Risk Implications	There are identifiable risks associated with: (a) the Article 4 confirmation and scope (including potential Secretary of State modification/cancellation and Judicial Review risk), (b) the adoption of SPDs (procedural Judicial review risk if statutory steps are missed), and (c) operational resourcing due to an increase in planning applications once the Article 4 takes effect. Mitigations include adherence to Schedule 3 GPDO procedure, robust evidence justifying geography (smallest area necessary), legal review of notices. (d) weight given to the SPDs if they are not adopted by the Government's deadline of 30th June 2026.
Financial Considerations	There are limited financial implications, including statutory advertising and site notice costs for the Article 4 making/confirmation, potential additional casework costs due to increased planning applications following confirmation, and publication costs for SPD adoption. These will be managed within existing budgets.
Subsidy Control	None
Legal Considerations	<p>The work relating to the HMO Article 4 Direction has and will continue to be in accordance with HMO Article 4(1) of the Town and Country Planning (General Permitted Development) (England) Order 2015 (as amended).</p> <p>The work relating to the SPDs has been and will continue to be in accordance with the Planning and Compulsory Purchase Act (2004) (as amended) and The Town and Country Planning (Local Planning) (England) Regulations 2012.</p> <p>The Leveling up and Regeneration Act (2023) allows for the provision of regulations that will alter the way that Local Development Framework Documents (LDF) are produced. On 27th November 2025 the Government issued guidance on what to expect within the regulations when they are brought into force in spring 2026. Of note is that the Government</p>

	intends to set out that SPDs cannot be adopted after the 30th June 2026. If this deadline is not achieved then it could be argued that the SPDs do not hold weight and cannot be relied upon in decision making.
Single Impact Assessment	During the preparation of the HMO Article 4 Direction and the draft SPDs along with during the consultation process, officers had regard to the protected characteristics within the Equality Act (2010) and the additional protected characteristics that HBC deem appropriate (care leavers, armed forces and poverty and disadvantage). Officers are of the view that the drafts, consultation and thus update reports have no negative impact upon protected characteristics but could in fact have a positive impact upon some protected characteristics by virtue of creating a better environment for all, including those with protected characteristics.
Staff Considerations	None
Asset Management Considerations	None
Environment, Sustainability and Climate Change Considerations	The report provides an update following the closure of the consultations and provides a time frame on when members are likely to be asked to progress the HMO Article 4 Direction and/or adopt the SPDs. This report has no environmental, sustainability and climate change impact however any such will be discussed when the HMO Article 4 Direction and SPDs are discussed at a future committee.
Consultation	Statutory consultation on the HMO Article 4 Direction is ongoing but will cease on 24th September 2026. The information seeking consultation has now closed. Consultation on the two SPDs ran from 3rd November to 16th January 2026. Engagement with residents and key stakeholders will continue in an informal and ad hoc manner up until the adoption of the SPDs and implementation of the HMO Article 4 Direction.

6. Recommendations

6.1. Members note the content of the report.

- 6.2. Members agree to delegate to the Director of Neighbourhood and Regulatory Services, to consider representations and make modifications to the SPDs, in consultation with the Committee Chair.
- 6.3. That Members agree to permit the modified SPDs to be presented to Full Council for adoption In May/June 2026.

7. Reasons for Recommendations

- 7.1. To ensure members are kept informed of SPD progress, to ensure that the correct permission is in place prior to consultation and to ensure that the SPD can be adopted in June 2026.

8. Background Papers

- 8.1. Neighbourhood Services Committee report 30th June 2025 (Article 4 Direction).
- 8.2. Neighbourhood Services Committee report 20th October 2025 (Two SPDs).

9. Contact Officers

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Sign Off:-

Chief Executive	Date: 10/02/2026
Director of Finance, IT and Digital	Date: 06/02/2026
Director of Legal, Governance and HR	Date: 09/02/2026