



Hartlepool
Borough Council



Hartlepool Borough Council Residential Design Code

Supplementary Planning Document
May 2026

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INTRODUCTION

Overview

The Residential Design Code ('This code') sets out a vision for the design of new residential development within Hartlepool.

This code provides design parameters (codes) and guidelines (guidance) that when followed will ensure that new residential development is high quality, positively enhances its location and meets the aspirations of the council and residents.

This code will serve as a tool to aid in the delivery of new high quality residential areas within the borough in accordance with the adopted Local Plan (2018). The codes and guidance within this document are intended to inform every stage of the development process, from site analysis to detailed design, and when followed will deliver well-designed neighbourhoods.

Benefits of this code:

- Provide clear and transparent design parameters and guidance for new residential development.
- Provide certainty on the council aspirations for residential development.
- Speed up the planning application process for proposals which meet the design codes and guidance.
- Bespoke to Hartlepool and sets different parameters to meet the different characteristics of the borough.
- Forms a material consideration in the determination of planning applications.

What is a Design Code?

The National Model Design Code defines a design code as a set of simple and concise design requirements that provide specific, detailed parameters for the development of a site or area.

Who is this code for?

This code is intended to be used by a wide audience.

The key users of this code will be:

- Council officers: to assess residential planning application proposals and inform pre-application discussions. This code will also

PLANNING POLICY

National Policy and guidance

The National Planning Policy Framework (NPPF)

The NPPF sets out the Government's planning policies for England and how these are expected to be applied. The overriding message from the NPPF is that planning authorities should plan positively for new development, and approve all proposals wherever possible. It defines the role of planning in achieving sustainable development under three topic headings – economic, social and environmental, each mutually dependent.

There is a presumption in favour of sustainable development, which requires Local Planning Authorities to approach development management decisions positively, utilising twelve 'core principles' that should underpin both plan-making and decision taking, these being; empowering local people to shape their surroundings, pro-actively drive and support economic development, ensure a high standard of design, respect existing roles and character, support a low carbon future, conserve the natural environment, encourage re-use of previously developed land, promote mixed use developments, conserve heritage assets, manage future patterns of growth and take account of and support local strategies relating to health, social and cultural well-being.

The creation of high quality, beautiful and sustainable buildings and places is fundamental to what the planning and development process should achieve.

The NPPF sets out that to provide maximum clarity about design expectations at an early stage, all local planning authorities should prepare design guides or codes consistent with the principles set out in the National Design Guide and National Model Design Code, and which reflect local character and design preferences. Design guides and codes provide a local framework for creating beautiful and distinctive places with a consistent and high quality standard of design.

Their geographic coverage, level of detail and degree of prescription should be tailored to the circumstances and scale of change in each place, and should allow a suitable degree of variety.

The NPPF further states that design guides and codes can be prepared at an area-wide, neighbourhood or site-specific scale, and to carry weight in decision-making should be produced either as part of a plan or as supplementary planning documents. Whoever prepares them, all guides and codes should be based on effective community engagement and reflect local aspirations for the development of their area, taking into account the guidance contained in the National Design Guide and the National Model Design Code.

- enable the provision of consistent advice.
- Applicants: To give applicants for new residential development including designers, architects, developers and members of the public, clear parameters and guidance on what is expected in the design of proposals.
- Planning Committee: to guide Councillors during their decision making at planning committee and ensure consistency in decision making.

Purpose of this design code and guidance

This Design Code will serve as a valuable tool, providing a set of specific and measurable parameters for new residential developments within Hartlepool.

By following the design codes and guidance within this document, new residential proposals will create well-designed neighbourhoods that enhance the quality of life for residents, while respecting the unique characteristics of the borough and will create residential areas, which meet the expectations of the council and its residents.

This code and planning applications

This code will be a Supplementary Planning Document (SPD) and as such should be referenced in applications made to the council. It will be a material planning consideration that is consistent with existing national and local planning policy.

This Design Code SPD builds upon and provides more detailed advice or guidance to policies in the adopted Local Plan, the NPPF and the National Design Guide.

As this SPD does not form part of the Development Plan, it cannot introduce new planning policies, therefore any reference to requirements including 'will' or 'must' in the Design Code are to be treated as guidance providing a strong steer to stakeholders setting out what the council expects from new development in the borough.

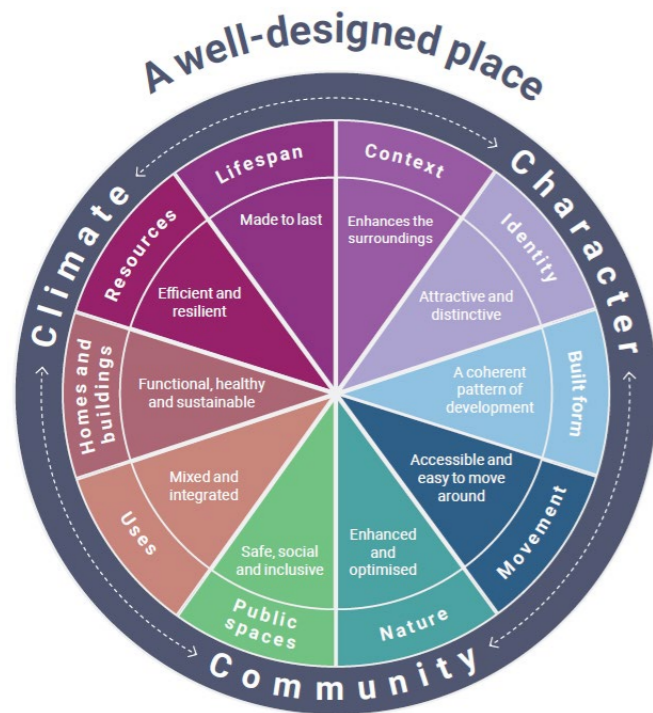
PLANNING POLICY

This National Design Guide, and the National Model Design Code and Guidance Notes for Design Codes illustrate how well-designed places that are beautiful, healthy, greener, enduring and successful can be achieved in practice. It forms part of the Government's collection of planning practice guidance and should be read alongside the separate planning practice guidance on design process and tools.

The National Design Guide (NDG) 2021

The National Design Guide sets out the characteristics of well-designed places and demonstrates what good design means in practice. It forms part of the government's collection of planning practice guidance.

The National Design Guide addresses the question of how we recognise well-designed places, by outlining and illustrating the Government's priorities for well-designed places in the form of ten characteristics. The NDG sets out that in well-designed places, an integrated design process brings the ten characteristics together in a mutually supporting way.



The National Model Design Code (NMDC) 2021

Provides detailed guidance and a framework for local authorities to create their own specific design codes, aiming to ensure high-quality, beautiful, and sustainable new developments that reflect local character, expanding on the 10 characteristics of good design in the National Design Guide.

The NMDC provides tools, examples (such as area types, parking, built form), and methods for community engagement to help create clear, illustrated requirements for developments.

The NMDC sets a baseline standard of quality and practice, which local planning authorities are expected to take into account when developing local design codes and guides.

Pathfinder Programme

The council has prepared this code with the support of the Ministry of Homes, Communities and Local Government (MHCLG) Pathfinder Programme. This code has also been subject to a Design Review Panel through the Pathfinder Programme.



PLANNING POLICY

Local Policy

The Hartlepool Local Development Framework consists of the Local Plan (2018), Minerals and Waste Development Plan Document's (DPD's), Rural Neighbourhood Plan (2018) and a suite of Supplementary Planning Documents (SPD's).

This code will be adopted as a Supplementary Planning Document to support policies within the Hartlepool Local Plan (2018) and replace the existing Residential Design Guide SPD (2019).

Hartlepool Local Plan (2018)

This code relates to several policies within the Local Plan, in particular Policy QP3 and Policy QP4:

Policy QP3: Location, accessibility, highway safety and parking.

The council will seek to ensure that development is safe and accessible along with being in a sustainable location or has the potential to be well connected with opportunities for sustainable travel.

When considering the design of development developers will be expected to have regard to the following matters:

1. The proximity of the development to nearby shops and services and how shops and services can be accessed along with how access can be improved particularly via the green infrastructure network.
2. Ensuring all residents and visitors can move with ease and safety and that features such as doors, entrance ways and parking are conveniently located.
3. Servicing arrangements and highway safety provisions are in line with the relevant local guidance and the requirements to maintain such provisions over the lifetime of the development.
4. Creating multi-purpose environments, managing the conflict between highways users specifically by distinguishing between paths and roads, whilst recognising the need to limit and/or create sympathetic highway safety measures.
5. The provision of adequate, safe, secure and

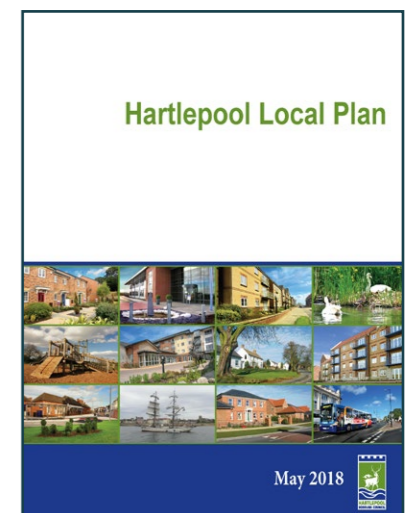
- conveniently located car and cycle parking, having regard to the possible movement of residents and visitors.
6. Ensure new car parking areas and proposals that include car parking should, where viable, use permeable paving solutions and/or introduce a SuDS scheme to mitigate for any increase in surface water run-off.
 7. Recognise the wider benefits that safety improvements at level crossings can bring about.

Policy QP4: Layout and Design of Development.

The policy seeks to ensure all developments are designed to a high quality and positively enhance their local setting.

Development should:

1. Be of an appropriate layout, scale and form that positively contributes to the borough and reflects and enhances the distinctive features, character and history of the local area.
2. Respect the surrounding buildings, structures and environment.
3. Have adequate, well located and planned public space/s.
4. Be aesthetically pleasing, using a variety of design elements relevant to the location and type of development.
5. Use an appropriate mix of materials and colour.
6. Sustain and/or enhance the historic environment and heritage assets including archaeological remains, and their settings.



PLANNING POLICY

7. Have appropriately designed and well located street furniture.
8. Be flexible to changing needs of users and consider the needs of an ageing population.
9. Should not negatively impact upon the relationship with existing and proposed neighbouring land uses and the amenity of occupiers of adjoining or nearby properties by way of general disturbance, overlooking and loss of privacy, overshadowing and visual intrusion particularly relating to poor outlook.
10. Ensure that the provision of private amenity space should be commensurate to the size of the development.
11. Use well-designed and placed obstacles, such as rocks or vegetation, to prevent vehicles parking on areas not designated for vehicles such as green spaces and pavements.

Hartlepool Rural Neighbourhood Plan (2018)

The Hartlepool Rural Neighbourhood Plan was made in 2018, the plan covers the rural parishes within Hartlepool that lie to the west of and adjoining the main built up area of Hartlepool, these include:

- Brierton,
- Claxton,
- Dalton Piercy,
- Elwick,
- Greatham,
- Hart, and
- Newton Bewley.

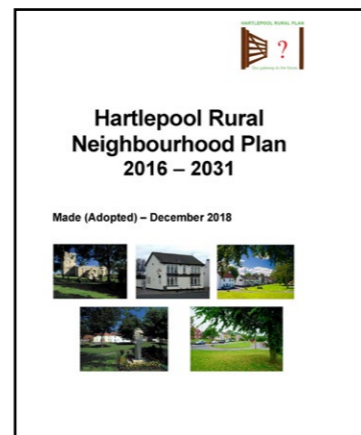
The Neighbourhood Plan sets out that all development should be well designed and take account of the design principles set out in Policy GEN 2.

Policy GEN 2: Design Principles.

The design of new development should demonstrate, where appropriate:

1. How relevant village design statements and conservation area appraisals have been taken into account;
2. How the design of new housing scores against the Hartlepool Rural Plan Working Group's Checklist as set out in appendix 4;
3. How the design helps to create a sense of place and reinforces the character of the village or rural area by being individual, respecting the local vernacular building character, safeguarding and enhancing the heritage assets of the area, landscape and biodiversity features;
4. How the design helps to reinforce the existing streetscape or green public spaces by facing onto them;
5. How the design preserves and enhances significant views and vistas;
6. How the design demonstrates that it can be accessed safely from the highway and incorporates sufficient parking spaces;
7. How the design uses sustainable surface water management solutions in new developments to reduce all water disposal in public sewers and manage the release of surface water into fluvial water and;
8. How the design ensures that homes are flexible to meet the changing needs of future generations.

In addition to those policies listed above a full list of applicable policies within the Local Plan and Rural Neighbourhood Plan are provided in Appendix 1.



PUBLIC CONSULTATION

The National Design Guide and National Model Design Code set out that a Design Code will be 'based upon effective community engagement and reflect local aspirations.' The council's Land Use Policy Team undertook an eight-week public consultation between September and November 2024 to gain the views of residents on what they valued about where they live and how future residential developments should be designed.

Stage One Consultation

The stage one consultation consisted of a series of public drop in events throughout the borough, engagement with a local youth group and an on-line survey and mapping tool.

The findings of the consultation event are summarised below and have informed the vision, aims, design codes and guidance within this document.

Character of Place

Residents valued distinctive architecture, detailing, and the overall identity of their neighbourhoods. Features such as bay windows, decorative trims, and painted accents were mentioned as contributing to a sense of place and making areas easier to navigate and remember. Homes that looked different from each other were preferred over uniformity.

Younger participants echoed this, highlighting how streets with colourful features (like painted doors or lintels) felt more inviting and easier to orient around. Even small visual differences between homes were appreciated.

There was also concern about new housing areas lacking this identity, often described as repetitive or "engineered," with little sense of individuality.

Homes and Buildings

There was a strong desire for homes that feel spacious and functional. While a preference for traditional house types was observed, however, this should not be at the cost of visual variety, with varied building form being viewed as the most attractive feature for new buildings.

For homes specifically, residents appreciated large rooms, high ceilings, practical layouts (especially rectangular room shapes), and usable landings. These features allow greater flexibility and comfort for modern living.

Participants emphasised that homes should be built with attention to both internal and external character—such as alcoves, fireplaces, and traditional detailing. There was also a call for homes to offer ample built-in storage, usable third bedrooms, and multiple bathrooms to suit family needs.

A clear dislike emerged for houses perceived as bland or cheaply finished, particularly in newer developments where materials like render were said to age poorly. The concept of "a home, not just a house" resonated throughout responses, emphasising warmth, light, and character over uniformity.



September 11, 2024

What do you value about where you live - Hartlepool Design Code

Planning Policy are collecting the views and ideas of Hartlepool residents on the what people ...

[Visit hub](#)

On-line survey

PUBLIC CONSULTATION

Layout and Density

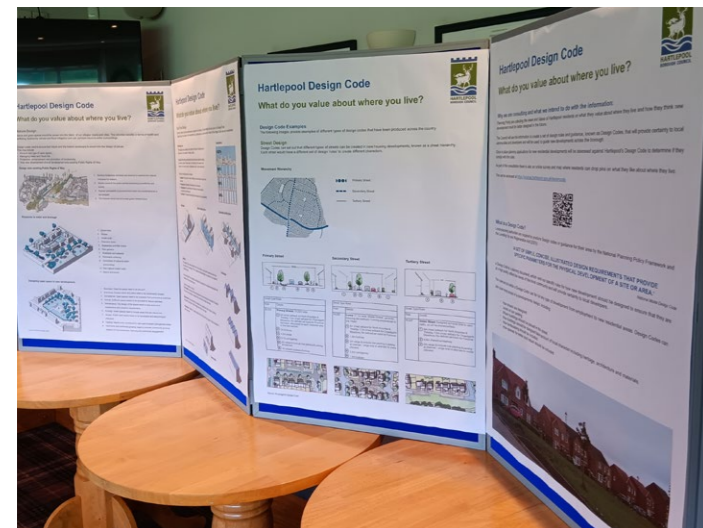
There was a preference for homes not to be located too closely together. Layouts should allow natural light into both houses and gardens, with fences and building orientation designed accordingly.

Residents emphasised the importance of privacy, quiet, and internal space in new developments. There was support for designs that allow for community interaction without sacrificing personal space, such as homes that don't open directly onto the street but have a small set-back.

Participants also wanted a variety of home sizes and styles to avoid visual monotony and to suit a range of life stages and needs.

Streets and Movement

Navigability, walkability, and green street design were key themes. Navigable street layouts were identified as being important, with cul-de-sacs noted as being disorienting. However, narrow alleyways or cut-throughs were not seen as a good alternative due to concerns over safety and appearance.



Drop in events

Pavement obstructions caused by parked cars and overgrown shrubs were frequently cited as making walking difficult. In contrast, greenery, especially street trees, were positively viewed when well-maintained and placed to avoid blocking street lighting.

Participants wanted streets to feel open and safe, ideally with front gardens, green verges, low boundaries, and good lighting. Passive surveillance was viewed as important for creating a sense of security.

Parking

Residents generally preferred off-street parking, particularly where vehicles can be seen from the home. On-street parking was considered visually disruptive and impractical, particularly when it impeded pedestrian movement.

There was some support for car-free or low-car streets, provided parking was still conveniently located nearby. People also valued streets that could be adapted for events, such as temporary closures for neighbourhood gatherings.

The importance of practical street design was raised, including space for bins, buses, and delivery vehicles. Parking at the back of homes was unpopular due to visibility and safety concerns.

PUBLIC CONSULTATION

Open Space and Nature

Green infrastructure was consistently and widely valued by a majority of respondents. Access to open spaces, street trees, and community gardens were consistently referenced as desirable features for communities. Some respondents expressed a desire for more ambitious green initiatives such as fruit trees, orchards, and space for residents to grow food.

Privacy was also cited as being important while advocating for open and welcoming frontages. There was a strong belief that personal and shared green space contributes positively to well-being and mental health. Open green links through estates and local parks were also seen as essential for encouraging walking, relaxing, and building community.

Community and Facilities

People want neighbourhoods that support everyday life. This included consistently available nearby shops, GPs, dentists, schools, and sustainable transport links. Several concerns were raised about the over-development of peripheral areas and a corresponding decline in the town centre.

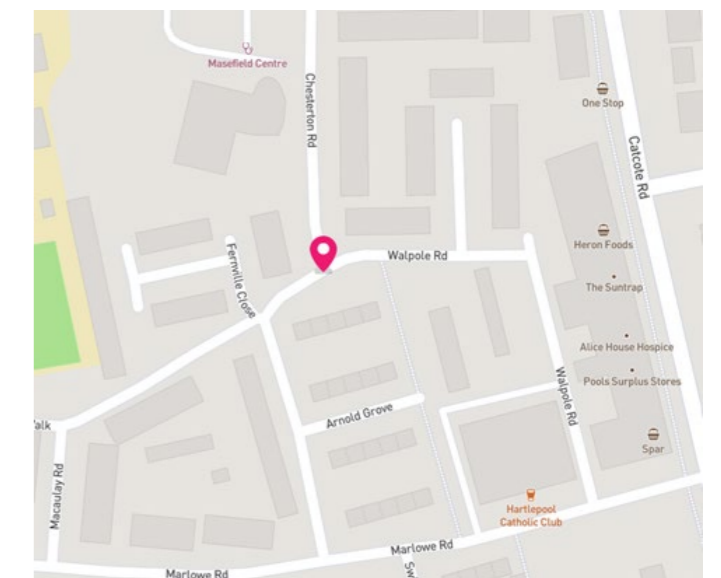
Respondents emphasised the importance of infrastructure matching the rate of housing growth, as well as including gathering spaces into developments to support community interaction. Older areas were mentioned as having potential for renewal, with reinvestment opportunities being mentioned.

Stage 2 Consultation: Draft SPD

A copy of the draft Residential Design Code SPD was consulted upon for a period of 8 weeks from the 3rd of March 2026 via an online form. Nine responses were received. The responses have been analysed and the SPD updated accordingly.



Community drop-in event



Example response to mapping tool

VISION

Following the findings of the public consultation and consultation with key internal stakeholders the following vision and aims have been produced for this code.

Vision:

To create attractive, distinctive and sustainable places to live within Hartlepool. New housing areas will provide well connected and healthy neighbourhoods with streets and green spaces that are social spaces and allow communities to thrive.

Aims:

1. Improve the quality of residential streets through encouraging them as social spaces and reducing the dominance of vehicles on the street scene.
2. Ensure new residential developments are successfully integrated into existing communities and encourage active travel through well designed movement networks.
3. Greening of residential areas through increasing the levels of landscaping, tree planting and green spaces.
4. Ensure new housing has its own identity and reflects any identified positive features of the area.
5. Promoting the use of renewable energy sources and energy efficiency in new homes.



STRUCTURE OF THIS CODE

This Code is broken down into two sections, Authority Wide and Area Types.

Section 1: Authority Wide

The first section provides Authority Wide codes and guidance which are applicable to all new residential developments throughout the borough.

Section 2: Area Types

The second section forms design codes and guidance based on Area Types. The Area Types are areas within the Borough which will see key residential growth. The three Area Types identified are:

1. Outer Neighbourhood.
2. Wynyard.
3. Villages (Core and Edge).

A character analysis of the borough has been undertaken to define the boundaries of the Area Types and identify key characteristics. This analysis along with the findings of the public consultation and assessment of planning applications has informed the Area Type codes and guidance.

Coding Plan

A Coding Plan has been produced (see opposite), which identifies the three Area Types.

The Coding Plan defines the extent of where the three Area Types codes must be applied to. It does not allocate areas for residential development. Where a site is located outside of a Area Type see Code AT1.

Should applicants be unsure which codes apply to a particular development, it is recommended that the council planning officers be contacted for pre-application advice.

- Outer Neighbourhood Area Type (ON)
- Wynyard Area Type (WY)
- Village Area Type (VL)
- Village Core
- Conservation areas

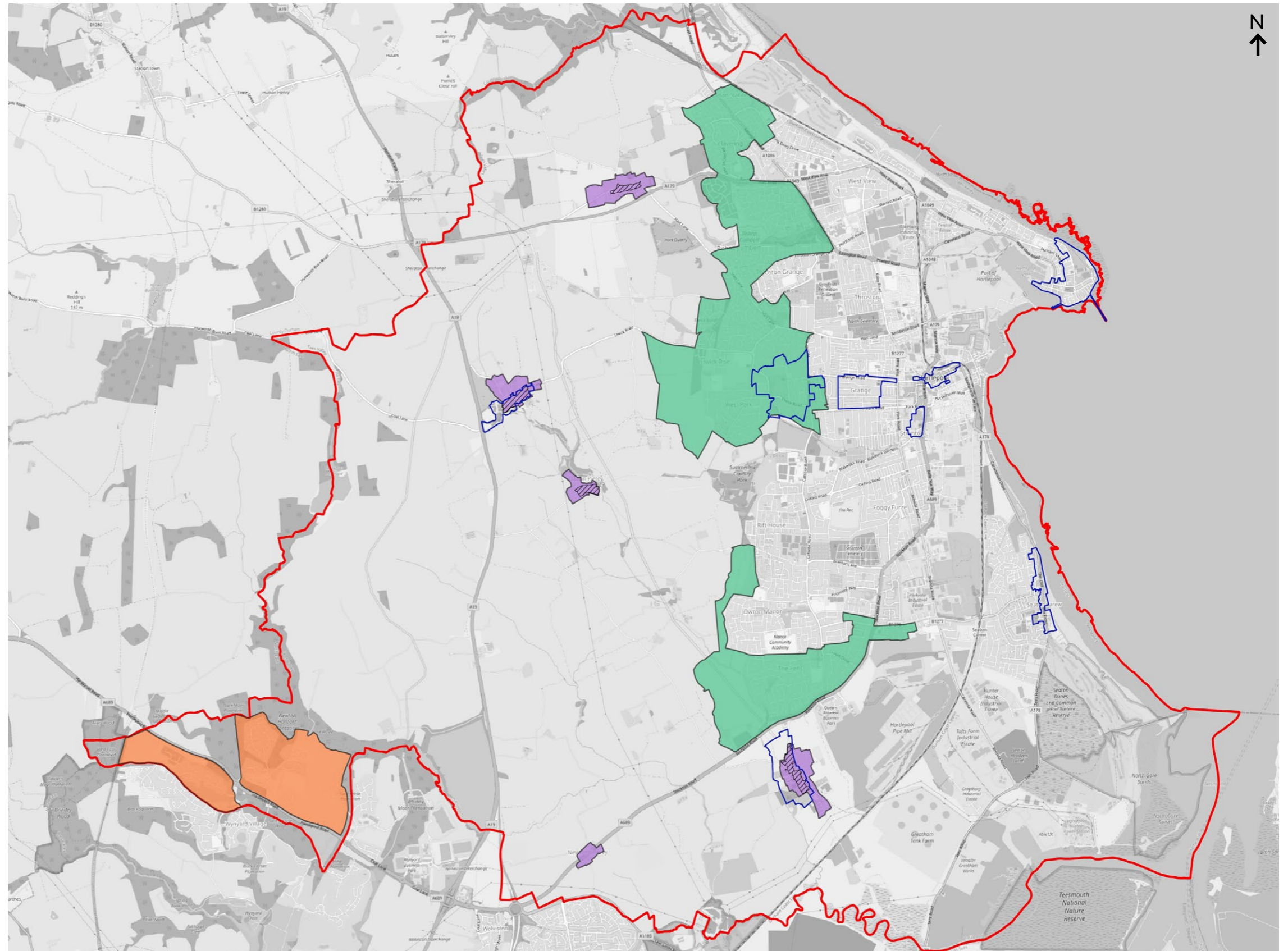


Figure 1: Coding Plan

STRUCTURE OF THIS CODE

How to use this design code

The document provides a combination of design parameters (codes) and guidance.

Codes are requirements for all new residential development, which must be followed.

Guidance is a specific set of guidelines, which may be applicable to residential development proposals and should be followed.

Justification with evidence will be required for proposals, which cannot meet or deviate from the codes and guidance.

To aid in distinguishing between what is a code and what is guidance, each code is provided with a unique reference number and is in a coloured box.

Demonstrating Compliance

Applications for new residential dwellings must show how they have complied with the applicable design codes and complete the Design Code Compliance Checklist.

The Design Code Compliance Checklist is available online and will be included within the local validation of planning applications list.

Where a departure from a code is proposed within an application, a robust justification must be provided, along with any associated evidence to enable the council to appropriately determine whether the departure is acceptable.

Section 1: Authority Wide Codes and Guidance

SITE ANALYSIS

A key element of ensuring that proposals appropriately address their site and surrounding context is to undertake a thorough baseline analysis. Each site proposed for development must undertake their own site analysis.

Code SA1: Site Analysis

- Submissions for new residential development must provide a site analysis which encompasses the following information, where applicable:
 - Topography.
 - Utilities.
 - Built heritage designations, heritage assets and their setting..
 - Existing natural features e.g. hedges, trees.
 - Neighbouring land uses.
 - Separation distances to existing properties.
 - Ecological designations.
 - Land use designations.
 - Location of closest facilities and services.
 - Key views.
 - Existing connections- vehicular, pedestrian and cycle.
 - Public Rights of Way.
 - Desire lines.
 - Public transport routes and closest bus stops to the site.
 - Flood risk and drainage.
 - Landscape character.
 - Archaeology.
 - Pollution/contamination.
- Please note this is not an exhaustive list and additional information may be required depending on the nature of the site.

Site Analysis Guidance

- The site analysis information should be provided within a Design and Access Statement in plan form.



Example of site analysis plans from the National Design Guide (2021)

MOVEMENT

You Said:

- » Within new housing areas, having safe and easy walking routes along with the ability to walk and cycle to facilities are the most desired connections.

Housing areas should benefit from easy and safe pedestrian, cycle and vehicular access. They must be safe to move around for all residents, regardless of age or physical ability. Areas that are not easy to move around can become dominated by vehicular movement and can, for some residents, have a negative impact upon confidence and lead to social isolation.

Creating well-connected and healthy neighbourhoods

It is important that any new residential development successfully integrates into established communities through providing connections to existing movement networks and creating a network of routes for all modes of transport.

Code MN1: Integrated Network

- All new development must successfully integrate into existing movement networks and street patterns.

Pedestrian and cycle connections

The provision of a high quality, attractive and safe network of footpaths and cycleways is an important element of creating sustainable communities, promoting active travel within new residential developments and encouraging the use of streets as social spaces.

Walking and cycling should be the first choice for short local journeys, particularly those of 5 miles or less.

Code MN2: Pedestrian and Cycle Connections

- New pedestrian and cycle connections must be provided within proposals.
- Connections must be made, where appropriate, to existing and/or proposed routes to:
 - a. Green infrastructure corridors.
 - b. Public Rights of Way.
 - c. Permissive routes.
 - d. Pedestrian and cycle routes.
 - e. Key locations identified within site analysis e.g. local centres, employment areas, schools and transport hubs.
 - f. Green spaces within 800m of the site.
- Where pedestrian and cycle routes are provided they must be overlooked by properties and not include features that can create hiding places.
- Lighting must be provided for well-used footpaths and avoid light spillage.
- Where a shared pedestrian and cycle route is provided, segregation between cyclists and pedestrians must be clearly marked.
- Proposals for dedicated cycle provision must be in accordance with the specifications set out within Design Guide & Specification – residential and industrial estates development (May 2021).
- Pedestrian routes must be a minimum of 2m in width.
- Shared pedestrian and cycle routes must be a minimum of 3m in width.
- Crossings for pedestrians and cyclists must be located on or close to desire lines.

Connections Guidance:

- Pedestrian and cycle only routes are encouraged in locations where vehicle access is not available, for instance to connect cul-de-sac streets.
- Ensure any future connections required to expand development are understood and factored into the proposed network and include:
 - » Way-markings or sign posts.
 - » Countryside furniture, where appropriate.
 - » Use of natural materials e.g. boulders to prevent vehicle access.



Figure 2: Example of treatment of movement connections in proposals

Natural Surveillance

New residential areas must provide safe active travel routes through ensuring they are overlooked from the surrounding housing.

Code MN3: Natural Surveillance

1. Development must be orientated to provide frontage or active side elevations onto existing or proposed pedestrian and cycle routes.
2. Activation must be provided by ground floor main windows of habitable rooms.

Treatment of Public Rights of Way (PRoW) and Permissive Routes

The Guidance for Public Rights of Way SPD (2020) provides details in relation to the treatment of PRoW and their design within Hartlepool.

The below codes set out the requirements for their treatment in relation to new residential development.

Code MN4: Treatment of PRoW and Permissive Routes

1. Where PRoW and permissive routes are located within a proposed residential development site, the route must
 - a. Be retained, either in its current alignment or diverted
 - b. Be located within a green route or open space.
 - c. Provide connections into the existing PRoW network and proposed/existing active travel routes to create an accessible neighbourhood.

Treatment of PRoW and Permissive Routes Guidance

- Where it is proposed to divert an existing PRoW the route should not form part of an adopted route or Private Drive.

Design of PRoW

There are three types of PRoW designs.

- Urban path: a PRoW which is within the developable area of the site and not located within green space.
- Urban edge path: a PRoW within the development site but located within an area of green space.
- Rural: a PRoW within the countryside.

Code MN5: PRoW Urban path

1. The design of routes within the developable area must:
 - a. Provide connections from the developable area onto the PRoW.
 - b. Be a minimum width of 2m for a footpath or 3m for a shared footpath/cycleway.
 - c. Be constructed of a tarmac surface.
 - d. Be located within a green route with a minimum of 1m of landscaping/ green space either side of the route.
 - e. Provide resting places within the associated landscaping/green space (see Code N6), where route is over 200m in length.
 - f. Not include any barriers.
 - g. Be subject to active frontage from surrounding properties.
 - h. Provide countryside furniture, where required.
 - i. Use natural materials e.g. boulders to prevent vehicle access.



Urban path example

MOVEMENT

Code MN6: PRow - Urban Edge Path

1. The design of routes located within open space of residential development must:
 - a. Provide connections between the developable area and the PRow.
 - b. Be a minimum width of 2m for a footpath or 3m for a shared footpath/cycleway.
 - c. Be constructed of a bound surface or aggregate. Tarmac will not be considered an appropriate surface.
 - d. Have a minimum of 2m landscaping / open space either side of the path.
 - e. Be a minimum of 5m from the developable area, with the exception of the point of access.
 - f. Provide resting places within the associated landscaping/green space (see Code N6), where the route is over 200m in length.
 - g. Not include any barriers.
 - h. Ensure any planting is designed to retain visibility and a sense of safety on the route.
 - i. Include way-markings or sign posts.
 - j. Provide countryside furniture, where required.
 - k. Use natural materials e.g. boulders to prevent vehicle access.



Urban edge path example

Code MN7: PRow Rural

1. The design of routes within the open countryside must:
 - a. Provide connections into the wider PRow network.
 - b. Be appropriately graded and well drained, where possible.
 - c. Be a minimum width of 2m for a footpath or 3m for a shared footpath/cycleway.
 - d. Be a minimum of 4m in width for a bridleway.
 - e. Be constructed of aggregate, if surfacing is required, otherwise natural surface e.g. grass.
 - f. Provide countryside furniture in accordance with British Standard BS5709:2018 for Gaps, Gates and Stiles.

PRow Guidance

- Where it is proposed to divert an existing PRow the route should not form part of an adopted route or Private Drive.
- The treatment of a PRow and road junction should consider including the following elements:
 - » Raised tables.
 - » Dropped kerbs.
 - » Clear markings and signage.



Rural path example

MOVEMENT

You Said:

- » Green links through housing areas and local parks are essential for encouraging walking, relaxing and community building.

Green Routes

Green routes provide off road connections between areas of green infrastructure.

Green Routes Guidance

- Where multiple areas of green space are being provided within proposals, green routes should be created to connect the spaces and form an off-road walking network.
- Green routes can utilise the Root Protection Areas requirements for retained hedges or tree belts.
- They can be in the form of:
 - » Formal footpaths with surfacing or,
 - » Informal walking routes such as mown paths within areas of landscaping or grassed areas.
- Should have at least 1m either side of the path as grassed verge or landscaping.



Green route along woodland belt

MOVEMENT

Public Transport

Access to public transport is key to providing people with choice on journeys beyond their immediate neighbourhood. The bus network remains the most important mode of public transport within Hartlepool.

Code MN8: Public Transport

1. New residential development must be accessible to bus services.
2. All new housing must be within 400m walking distance of a bus stop (this may not always be possible in the more remote parts of the borough).



Car free housing - Vaux, Sunderland

Car Free Residential Developments

The aim is to remove vehicles from residential streets thereby promoting the use of streets as social spaces, whilst also encouraging sustainable transport methods such as cycling or walking. Car parking spaces are not provided for new residential dwellings within the plot or within the street.

Code MN9: Car Free Developments

1. Where car free developments are proposed they must:
 - a. Be located in sustainable locations with access to public transport.
 - b. Provide connections into existing walking and cycling routes.
 - c. Provide new pedestrian and cycling routes in accordance with Code MN2.
 - d. Provide access for emergency services.
 - e. Include delivery bays and/ or hubs.
 - f. Incorporate communal cycle parking.
 - g. Incorporate well-designed central bin collection points.
 - h. Provide landscaping, planting and trees within streets.
 - i. Provide informal play facilities (see code N7).
2. Provide parking in a central location, where required, and must:
 - a. Be at reduced provision levels to those set out in Highways Design Guide & Specification – Residential and Industrial Estates Development (amended May 2021)
 - b. Be in the form of a courtyard (see Area Type Codes) and within 400m of the proposal.

Car Free Development Guidance

- The surface material chosen for car free streets must signify that they are vehicle free and should consist of a paved surface. The use of tarmac will not be encouraged.
- Consideration should be given to incorporating dedicated bays to car share clubs.

BUILT FORM

Hartlepool has a rich and varied built form, from historic villages in the rural area to the Headland with its unique coastal location and the urban area with Victorian and Edwardian terraced houses and villas. Most recent residential developments have been focused on the urban edge and Wynyard, creating large scale urban extensions.

When buildings and spaces are designed correctly, in most instances, they blend seamlessly with surrounding land uses, reflect local character and create a sense of place. Well-designed buildings and spaces should be aesthetically pleasing and assist in uplifting mood or heightening senses in a bid to improve the overall well-being of residents and visitors.

Code BF1: Character Analysis

1. Due to the variations in character within the borough it is important that each proposal appropriately reflects any positive features of it's surroundings. A character analysis must be undertaken and must be used to inform the design of the proposed built form. The character analysis must include:
 - a. Predominant house types e.g. apartments, bungalows, terraced, semi-detached, detached.
 - b. Predominant building heights.
 - c. Density.
 - d. Massing.
 - e. Distinctive buildings which positively influence the character of the wider area.
 - f. Predominant architectural styles
 - g. Window proportions, rhythm and treatments.
 - h. Entrance treatments e.g. canopies, porches.
 - i. Features e.g. chimneys.
 - j. Building materials and colours.
 - k. Landscaping.
2. Please note this is not an exhaustive list and additional information may be required depending on the nature of the site.
3. Where an area has limited, or no positive architectural features which contribute to character, design precedents representing the new proposed character must be provided.



Example of character analysis - National Model Design Code (2021)

BUILT FORM

You Said:

- » Homes should look different from each other. Distinctive architecture, detailing, and the overall identity of the neighbourhood is important.

Legibility

New residential areas must be easy to navigate for residents and visitors. To enable this there needs to be a clear distinction of places within proposals through the use of street hierarchy, changes in the built form and materiality, and the use of landscaping and open space.

The inclusion of corner turning buildings, landmark buildings and gateways must aid in wayfinding and legibility.

Code BF2: Corner Buildings

1. Corner turning buildings must:
 - a. Be provided where both the front and side elevations of a property present onto the public realm.
 - b. Be dual frontage properties, i.e. provide ground floor activation and detailing on the two sides which face onto the public realm.
2. To aid in legibility and way finding, corner turning house types must be differentiated throughout proposals. E.g. on major



Corner turning building - Seaton Meadows

developments where more than one corner turning building is needed, a change in house type or materiality must be provided.

3. Corner plots where rear gardens border the public realm must provide a boundary treatment of either:
 - a. Brick wall.
 - b. Brick wall and timber panelling combination.
4. A 1.8m timber fence will not be considered an appropriate response on corner plots.

Code BF3: Landmark Buildings

1. Landmark buildings must be easily identifiable, contrast from the surrounding built form and have a sense of prominence.
2. Landmark buildings must have:
 - a. Increased scale.
 - b. Change in materials.
 - c. Different elevational treatment.
3. Landmark buildings must be provided at key locations including:
 - a. Site entrance, both vehicular and pedestrian if separate.
 - b. Nodes.
 - c. Prominent corners.
 - d. Key view terminus points.



Landmark building - Lucy Cavendish

BUILT FORM

Frontage and Building Line

Successful streets are characterised by active edges and natural surveillance where the street space is overlooked from homes. This creates a sense of ownership and safety over the street. In residential areas it is important that elevations facing the street include entrance doors and windows from habitable rooms.

For new residential development to sit comfortably within its surroundings, it is important that the proposed building line respects the existing built form.

Code BF4: Active Frontages

1. To ensure the creation of safe and attractive streets, which encourage active travel and the use of streets as social spaces, new residential development must present active primary elevations on to the street with:
 - a. Front doors located onto the street.
 - b. Windows from habitable rooms must face onto the building line to provide natural surveillance over the street and public spaces.
2. Proposals must not present rear boundaries onto existing residential areas.

Code BF5: Building Line

1. New residential developments must follow the established building line where it exists and provides frontage onto the public realm.
2. Proposals within the Area Types must follow the building line guidance set out within the relevant Area Type section.

Phased Development

Where large scale residential development is proposed, which will be provided over several phases, it is important to ensure that the built form between the phases relates well to one another and that it is not clear where the boundary of the phase is.

Phased Development Guidance

- Ensure that built form of different phases of development sits comfortably and relates well with each other. This should be achieved through the use of similar:
 - » Building materials.
 - » Detailing e.g. window treatment.
 - » Proportion or pattern of fenestration.
 - » Scale of the buildings.

Conservation Areas

There are 8 conservation areas within Hartlepool. The codes and guidance within this SPD do not provide a detailed analysis of the conservation areas, these are provided within Conservation Area Management Plans (CAMPs), appraisals or visual assessments.

Code BF6: Heritage and Conservation

1. Any proposals for new residential development within a conservation area and its setting, and / or the setting of heritage assets must take into consideration the relevant CAMPs, Character Appraisals and/ or visual assessments, where applicable.

You Said:

- » Ensuring privacy, quiet, and internal space are important in new developments.
- » Attention should be paid to both internal and external character.

Separation distances

Residents must be able to enjoy their home and must enjoy a certain level of natural light, must benefit from privacy and must not be unreasonably overlooked by neighbours or passers-by. To ensure this, residential separation distances must be applied to all new developments.

A reduced distance may be considered where it can be demonstrated that this will assist in creating a variety of streets or an exemplar development.

Code HB1: Separation Distances

1. The following standards are required, as a minimum, of all new residential developments.
 - a. 20m between principle elevations.
 - b. 10m between gable and principle elevations.
 - c. 2m between blank gable elevations.
 - d. 1m between blank gable elevations will be considered where the preceding distance between gables is 3m or over.

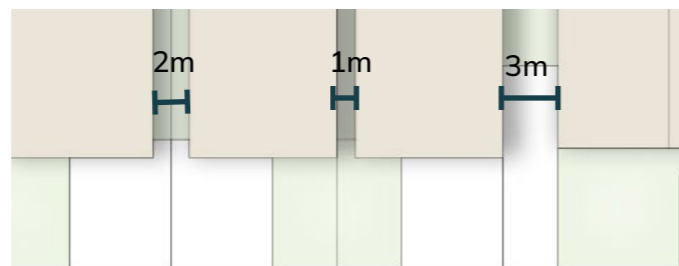


Figure 3: Separation between blank gables plan

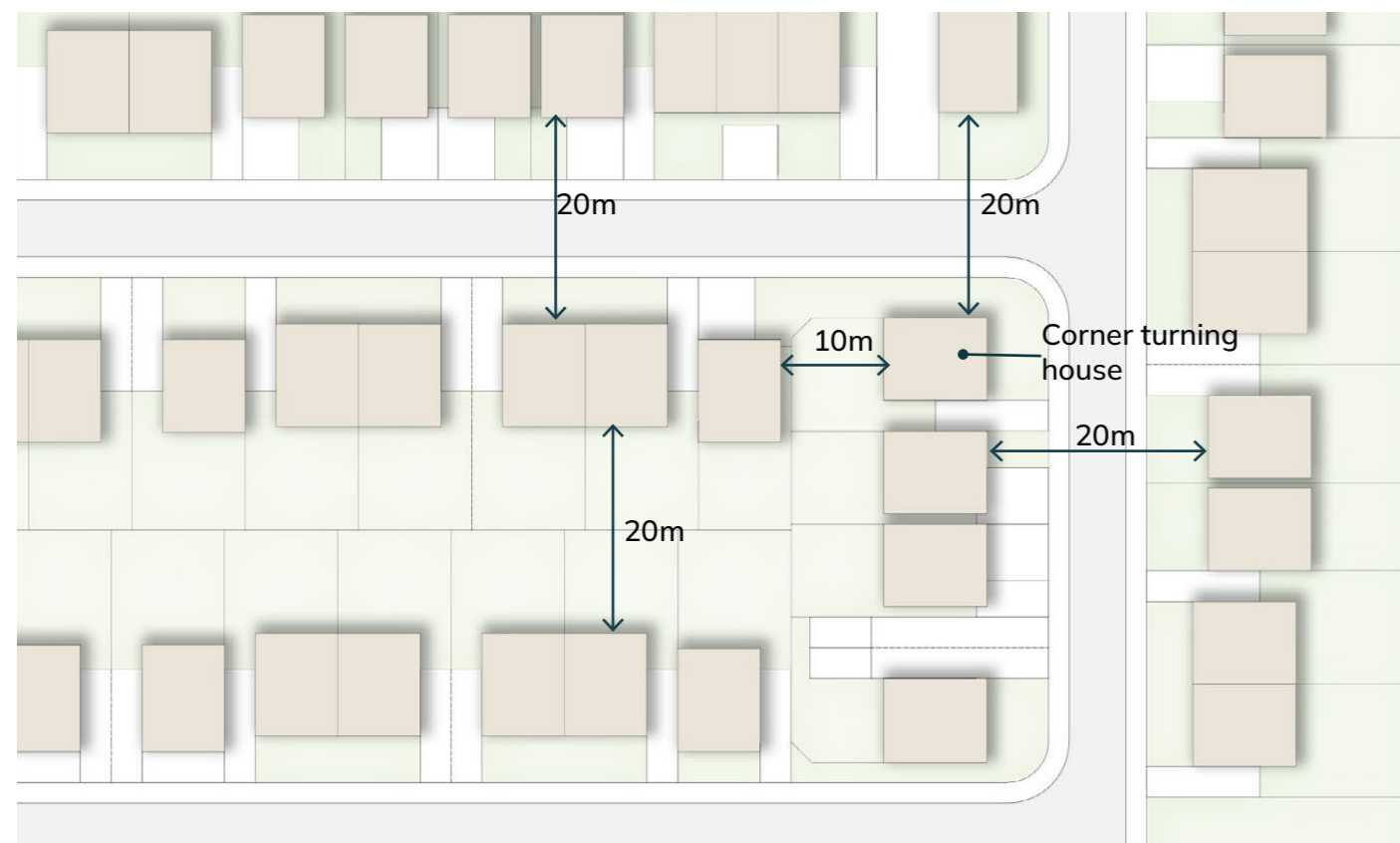


Figure 4: Separation distances plan

Internal Space Standards

Residents should have sufficient space within their homes to undertake typical day-to-day activities. Providing adequate space within dwellings allows households to socialise with family members and guests, improves storage capacity, improves space for solitary activities, provides greater flexibility in arranging rooms to meet different/changing needs, allows more opportunity for working from home, provides more space for managing waste and recycling, and improves daylight and ventilation.

The Nationally Described Space Standard (NDSS) deals with internal space within new dwellings and is suitable for application across all tenures. It sets out requirements for the Gross Internal (floor) Area of new dwellings at a defined level of occupancy,

as well as floor areas and dimensions for key parts of the home, notably bedrooms, storage and floor to ceiling height.

The Gross Internal Area of a dwelling is defined as the total floor space measured between the internal faces of perimeter walls that enclose the dwelling. This includes partitions, structural elements, cupboards, ducts, flights of stairs and voids above stairs.

Code HB2: Nationally Described Space Standards

1. All new dwellings must, as a minimum, meet the Nationally Described Space Standards (2015), as set out below.

Number of bedrooms	Number of bed spaces (persons)	1 storey dwellings	2 storey dwellings	3 storey dwellings	Built-in storage
1b	1p	39 (37)*			1.0
	2p	50	58		1.5
2b	3p	61	70		2.0
	4p	70	79		
3b	4p	74	84	90	2.5
	5p	86	93	99	
	6p	95	102	108	
4b	5p	90	97	103	3.0
	6p	99	106	112	
	7p	108	115	121	
	8p	117	124	130	
5b	6p	103	110	116	3.5
	7p	112	119	125	
	8p	121	128	134	
6p	7p	161	123	129	4.0
	8p	125	132	138	

Minimum gross internal floor areas and storage (m²)

* Where a 1b1p has a shower room instead of a bathroom, the floor area may be reduced from 39m² to 37m², as shown bracketed.

You Said:

- » Homes should have their own growing space, even balconies.

Private Amenity Space

Residents must have the space to enjoy their private amenity space and undertake day-to-day activities, like drying clothes, eating out and playing. Private amenity space must visually add to the quality of the home and the area.

Code HB3: Private Amenity Space

- All dwellings to be provided with outdoor private amenity space commensurate to the size of the dwelling.
- For dwelling houses gardens must:
 - Provide sufficient space for the anticipated day-to-day needs of the residents.
 - Be usable e.g. where topographical changes occur, terracing must be created.
 - Be orientated to benefit from good levels of daylight.
 - Include vegetation and/or landscaping to improve the overall visual quality of the dwelling and the area.
 - Enable space for food growing opportunities.
- For flatted development amenity space must:
 - Be provided for all occupants within the curtilage of the building. This may be in the form of outdoor private space for ground floor accommodation, balconies for accommodation above ground floor level, or communal amenity space.
 - Private amenity space must be a minimum of 5m² for 1-2 person apartments with an additional 1m² per additional resident and a minimum depth of 1.5m.
 - Enable space for food growing opportunities e.g. container gardens or communal growing gardens.

Refuse Storage

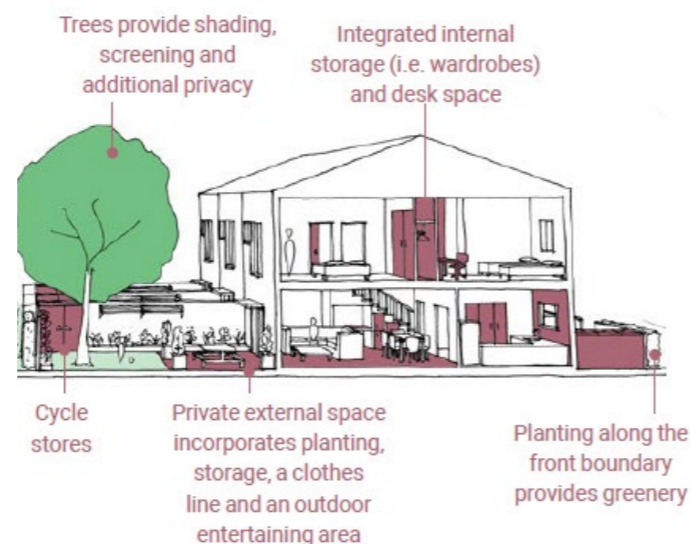
The provision of accessible bin storage must be provided for each dwelling, the provision of dedicated bin stores must avoid issues with waste receptacles cluttering the street scene or impacting on the character of the built form.

Code HB4: Refuse Storage

- Suitable refuse space of at least 1.5 square meters must be provided for each unit.
- In accordance with current building regulation bin stores must be located within 45m of a suitable collection point on the street network.

Code HB5: Bin Collection Points

- Bin collection points must be provided on Private Drive street types.
- Be located a maximum of 25m from the adopted highway.
- Have a hard standing surface.
- Be of a size to accommodate one bin per unit of the Private Drive.



Private amenity space - National Design Guide (2021)

Refuse Storage Guidance

- When located to the rear or side of the building bin storage should be marked by a hard standing area or a purpose-built store.
- When located to the front of properties these should be:
 - » Co-located with cycle parking.
 - » Screened from public view with an appropriate boundary treatment, which compliments the materials of the primary building, or
 - » Incorporated into the design of the building.
- Communal waste bins should be in accessible locations and provide screening from the street scene.

Cycle Storage

The promotion of more sustainable alternatives to the private car is a consistent theme within both national and local transport policy. For this reason, secure and conveniently located cycle parking facilities must be provided for all new developments.

Code HB6: Cycle Storage

- Every unit must be provided with cycle parking space.
 - Dwellings – 2 spaces per dwellings, in curtilage.
 - Apartments communal parking – 1 per 4 units for up to 100 units.
- Cycle storage must be in accessible locations, secure and dry.
- Where cycle storage is proposed to the front of homes this must be accommodated alongside refuse stores and not dominate the street scene. Materials used must complement the primary building material.

Utilities

Placement of meters and their housing requires careful consideration to ensure they do not impose unduly on the overall facade of properties, whilst still being accessible for reading.

Code HB7: Utilities

- Meters must be housed to the side elevation of properties.
- Where this is not feasible, for instance on terraced properties, the meter must be designed in a manner to not be over imposing on the facade. i.e the colour of the housing box used.



Meter box impacting on elevation

You Said:

» Access to open spaces, street trees, and community gardens are desirable features for communities.

To add to the overall character and quality of an area and to assist in improving physical and mental health, all residents must have access to open spaces and other green infrastructure within the vicinity of where they reside. In providing open spaces close to home and access to a green network, residents will have greater opportunities for play, sport, recreation, relaxation and local food production, which in turn will promote active healthy lifestyles close to home.

The Green Infrastructure SPD has an aim of 'providing a network of interconnected GI and spaces rich in habitat and home to a diverse range in wildlife'. New residential developments must contribute to the delivery of this aim through the provision of new green infrastructure from large open spaces to tree lined streets.



Summer Hill, Hartlepool

Code N1: Green Space

1. Green spaces must be at the heart of new residential developments, ensuring that everyone has access to green spaces where recreational and leisure activities can take place.
2. Any proposed green space must be within 800m of all proposed properties.
3. Green space must be in easily accessible locations and usable by all.
4. Green space must create nature rich spaces close to where people live.
5. The scale of any proposed green space and its features will be dependant on the level of residential development being proposed and the needs of the community.
6. Formal play space must be provided when a need is identified and in accordance with Code N8.



Ward Jackson Park, Hartlepool

Green Space Design:

The way in which spaces are designed is crucial to their success. The design considerations vary with the type of space, a formal park being very different to a large natural green space.

- 1 Ground floor activation
- 2 Convenient entrances
- 3 Attractive, clear & direct routes
- 4 Nature rich
- 5 Sports facilities in locations to reduce disturbance
- 6 Tree planting
- 7 Multi-functional spaces

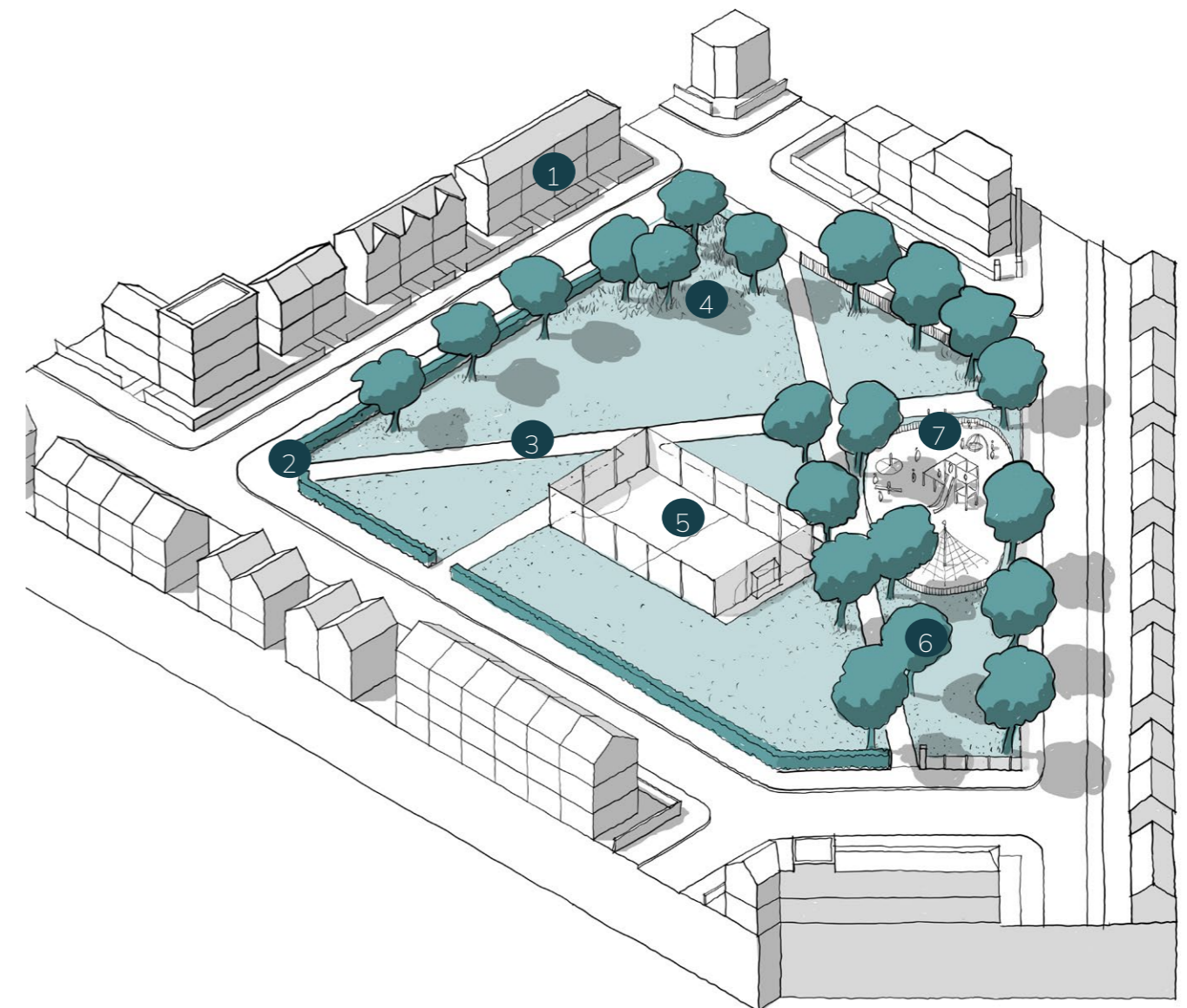


Figure 5: Example of well designed multi-functional green space

Code N2: Green Space Design

1. Proposals for green space within new residential development must:
 - a. Provide active ground floor windows onto green space.
 - b. Avoid presenting rear boundaries onto open space.
 - c. Ensure entrances and access points into green spaces are conveniently located on desire lines for walking and cycling.
 - d. Incorporate drainage solutions (see Code N11).
 - e. Include areas that are nature-rich and provide opportunities to improve biodiversity through planting.
 - f. Include tree planting for shade and shelter.
 - g. Place landscaping to discourage anti-social behaviour.
 - h. Protect spaces from unauthorised vehicular access, by using sensitively placed obstacles, such as rocks or vegetation.
 - i. Provide a space which facilitates informal play and socialising.
 - j. Provide appropriate management that is secured under a s106 agreement or other such secure arrangement.
2. Green spaces over 0.5ha must also:
 - a. Provide attractive, clear and direct pedestrian and cycle routes in accordance with Code MN2 and constructed of tarmac.
 - b. Be multifunctional spaces.
 - c. Include seating in accessible locations.
 - d. Ensure sports facilities are in locations that limits disturbance to properties and avoids conflict with other users, where required.
 - e. Ensure barriers around sport pitches are permeable and made of high quality materials (where sports pitches are required).
 - f. Provide lighting for well-used footpaths and games areas, their type and placement must avoid light spillage.

Code N3: Creating a Network

1. Proposals for green spaces must be connected into the wider green Infrastructure network.
2. Where more than one green space is provided on site, connections must be provided between them. Connection must be for pedestrians and cyclists.
3. Where green spaces sit on the edge of the site and border the rural edge/countryside, plans must be submitted outlining how access and connection into the countryside are provided.

Creating a Network Guidance

- When creating a network of green spaces, wildlife friendly movement should be considered

Code N4: Community Growing

1. A green space of over 1 hectare must provide community growing facilities such as communal allotments or orchards.



Communal food growing - Hartlepool

Landscape Pockets

Landscape pockets are small areas of landscaping to be provided within certain street types (see Area Types) and offer the opportunity to provide visually attractive areas of landscaping, increase the provision of street trees and can incorporate resting places with street furniture.

Code N5: Landscape Pockets

1. Provide landscape pockets in accordance with the street typology of the Area Type.
2. Must create high quality areas of landscaping which:
 - a. Are a minimum of 50m².
 - b. Incorporate street trees and planting.
 - c. Include resting places (see Code N6).

Resting places

Resting places invite people to sit, dwell and enjoy their surroundings. They are an integral part of well-designed active places through encouraging people to walk and cycle more and for longer distances. Resting places are particularly important for older people and people with mobility issues.

Code N6: Resting Places

1. To encourage active travel by all residents, resting places must be provided approximately every 200m.
2. Where a proposed street length does not meet the 200m requirement at least one resting place must be provided within a site.
3. Provide seating with a back and arm rest and ensure shading is created through tree planting.
4. Seating must be constructed of a low maintenance and robust material.
5. Can be incorporated into landscaped verge and landscape pockets depending on the street typology (see relevant Area Type Codes).
6. Placed in areas subject to natural surveillance.
7. Must not obstruct movement on the footway.

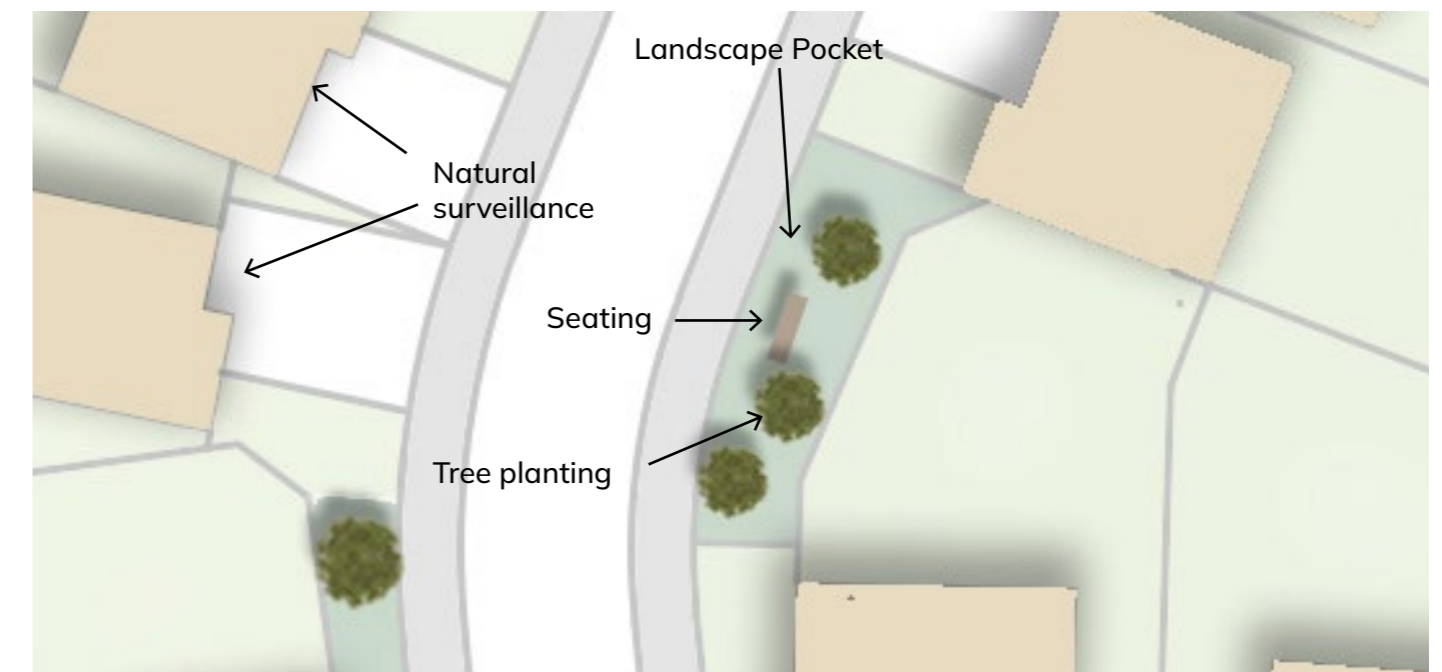


Figure 6: Example of landscape pocket with resting place

You Said:

» Having trees planted within the street was the most attractive feature, followed by the provision of open space near homes and strips of landscaping along the street.

Play Areas

Encouraging healthy and active lives is a priority for the council. Including areas for informal and formal play will be key to achieving this. Play spaces are also important in encouraging social interaction and creating cohesive communities.

Informal Play

To encourage the use of streets by families, improve health and well-being and create more child friendly places, the inclusion of informal doorstep play or 'play on the way' must be provided within new residential developments.

Code N7: Informal Play

1. Informal 'play on the way' opportunities must:
 - a. Be incorporated through utilising verges, landscape pockets, green routes and green spaces.
 - b. Use natural materials. e.g. tree stumps and rocks.



Play on the way - Farmstead Road, Lewisham

Code N8: Formal Play Areas

2. Where formal play areas are required they must:
 - a. Be easy to find and access using safe and direct active travel routes.
 - b. Predominantly use natural materials for equipment.
 - c. Be subject to natural surveillance either from active frontages from surrounding residences or other functions to prevent anti-social behaviour and to encourage a sense of safety.
 - d. Be enclosed. The boundary treatment must relate to the Area Type, be permeable in nature and up to 1.2m in height.
 - e. Provide seating areas with shade.
 - f. Be accessible and suitable for all.

Play Guidance

- Where new play facilities are being provided within a site, discussions will be required between the applicant and the council to ensure the most appropriate form of play is being provided.



Play area with natural materials - Wynyard, Hartlepool

Trees and Hedges

Incorporating trees and other landscape features in streetscapes provides habitat, shading, cooling, air quality improvements and carbon sequestration, as well as being a vital component of attractive places.

Research has proven that trees provide significant benefits for developers and property owners by increasing property values and selling prices. Hartlepool currently has a tree canopy cover of 9.4%, placing it among the lowest local authority areas nationally and substantially below the national average of 15.8%. The Environment Act 2021 establishes a statutory target for increasing tree canopy cover to 16.5% across England, reinforcing the requirement for development to safeguard existing trees and hedgerows and to incorporate meaningful new tree planting within layout proposals.

Code N9: Trees & Hedges

1. Existing category A and B trees within development sites must be retained.
2. Where there is a requirement to remove an existing tree, exceptional circumstances must be demonstrated. The tree/s must be replaced on-site.
3. There must be no net loss of potential canopy cover within the curtilage of a development.
4. Existing hedges must be retained and utilised within development proposals.
5. Replacement trees must be planted no later than the next available planting season upon completion of the relevant phase of development or before the occupation of a property, whichever comes first.
6. Trees must be suitably positioned to allow sufficient space for mature growth. Projected crown size must be used to ensure that conflict does not occur with property, infrastructure, street lighting and highway sight lines.
7. Existing hedges and their associated easement must be utilised for green routes within proposals.

8. Tree planting must be appropriate for a changing climate and suitable for the future, not just the present. The introduction of non-native species into planting schemes where suitable must be explored to ensure a resilient and biodiverse tree population of the future.



Tree planting within landscaping - Hartlepool



Tree planting within landscape pocket - Goldsmith Street



Existing trees and development - Place Fields

NATURE & PUBLIC SPACE

Street Trees

Incorporating trees within streets will aid in the greening of new residential areas of Hartlepool and bring health, biodiversity, economical and ecological benefits.

Code N10: Street Trees

1. Street trees must be provided in several locations and will be set by the street type.
2. Trees must be:
 - a. Within verges of Neighbourhood Streets to create avenues.
 - b. Within landscaping pockets within Residential Streets and Lanes.
 - c. Between parking bays where landscaping is required to break up more than 4 consecutive spaces.
 - d. Within the carriageway, in build outs, consideration must be given to ensuring visibility.
3. Underground rooting systems must be used to accommodate street trees where space is restricted and incorporate water management.
4. Tree species selection must be relevant, specific and suitable for its intended use following existing research and literature designed to inform specifiers such as - Trees & Design Action Group – Tree Species selection for Green Infrastructure, a guide for specifiers and The Essential Tree Selection Guide from RBG Kew.
5. Tree planting and landscaping within verges must be supported by a management and maintenance plan to ensure successful establishment and long-term retention.

Street Trees Guidance

- Coordinating tree planting with utilities providers and service ducts early in the lifetime of a scheme can ensure that trees do not interfere with underground services.
- Consideration should be given to the location of street trees and proximity to lighting columns.
- Tree planting should be carefully selected and positioned to allow space for the mature tree without causing obstruction or interfering with property, infrastructure, street lighting or junction sightlines.

Trees in Gardens Guidance

- Tree planting within front gardens is encouraged, however, this should not be the sole provision of street trees due to the potential for their removal by future occupiers.
- Where back to back development is proposed trees should be introduced to 2-3 rear gardens to provide a more attractive view and reduce the dominance of fences for residents.



Street trees - Ashmere, Kent

NATURE & PUBLIC SPACE

Sustainable Drainage Systems (SuDS)

SuDS are designed to manage storm water locally (as close to sources as possible), and mimic natural drainage to reduce the effect on the quality and quantity of surface water run off from developments. When designed and managed appropriately SuDS can positively contribute to place making and provide multiple benefits including visual amenity, recreational, and ecological.

The codes and guidance below are in relation to the design of the SuDS and should be considered alongside any technical specifications. Consultation with the Lead Local Flood Authority (LLFA) at the earliest opportunity is advised.

Code N11: SuDS

1. The design of SuDS must be in accordance with:
 - a. National Standards (2025) or any future versions.
 - b. Industry recognised guidance including C753 The SuDS Manual.
2. SuDS within new residential proposals must:
 - a. Be integrated into on-site green infrastructure provision in accordance with the principles and standards of Natural England's Green Infrastructure Framework.
 - b. Not negatively impact upon the level of usable green space for leisure and amenity.
 - c. Support Biodiversity Net Gain.
 - d. Provide recreational routes on their perimeter.
 - e. Be natural in appearance, forming shapes which mimic natural ponds.
 - f. Incorporate features such as rocks and pebbles.
 - g. Include appropriate planting.
 - h. Minimise any visual impact of large pre-cast concrete outfalls with key-clamp railings.
 - i. Complement the landscape character of the proposal.

SuDS Guidance

- SuDS designed into highway provision are encouraged and can be provided within verges in the following forms:
 - » Swales.
 - » Rain gardens.
 - » Tree pits.



Suds within open space - Brooklands, Milton Keynes



Suds within street - St Chads, Tilbury

RESOURCES

Renewable Energy

The council is committed to taking a leading role and working with everyone across the borough to act now on the causes and impacts of climate change.

Residential developments have a key role to play in helping to address climate change and tackle fuel poverty through the use of sustainable design and construction.

The government has set a target to reduce greenhouse gas emissions to net zero by 2050 with an interim carbon budget requiring a 78% reduction in emissions by 2037. Incorporating renewable, decentralised or low carbon energy in new residential developments is an important component of meeting these targets.

Renewable Energy Guidance

- To reduce the carbon footprint of new homes the following renewable technologies should be incorporated:
 - » Solar panels.
 - » Heat pumps.
 - » Rainwater harvesting

Fabric First

Much of the focus of improving the energy efficiency of new buildings is on the building fabric itself and energy technologies. Fabric First is an approach to design which maximises the efficiency of the components and materials that make up the fabric of the building.

Fabric First Guidance

- Fabric First should be considered alongside the use of renewable technologies.

Solar Gain

Solar gain refers to the increase in temperature in a space, object or structure that is a result of energy from the sun. Energy from the sun is free; therefore solar gain can reduce heating costs as well as having significant environmental benefits.

The principles of solar gain use the design and positioning of buildings to maximise the amount of natural heat and light that is obtained. There should however be a balance with the need for energy efficiency and the provision of natural surveillance and other elements of this code.

Solar Gain Guidance

- The main elevation should face within 30 degrees of due south.
- The main long axis of the building i.e. the ridge line should run east-west.
- Taller buildings should, where possible, be located to the north of the site to avoid restricting sunlight.
- The largest part of the roof's surface should be south facing or SSE/SSW for solar panel placement.
- The incorporation of a overhang on the southern elevation should be used to prevent overheating.
- Glazing should be maximised on the south to maximise energy from the sun and minimised on the north elevation.
- Use buildings, trees and landscaping to provide protection from prevailing winds.



Solar panels on roof

Section 2: Area Type's

AREA TYPES

An analysis of the general characteristics of the borough has been undertaken to define 3 distinct Area Types.

- Outer Neighbourhood (ON)
- Wynyard (WY)
- Village (VL)

Within each Area Type there are variations within the street network and built form however, there are overall similarities in characteristics of the areas. These characteristics along with the views expressed during the public consultation exercise, views of key stakeholders, and aspirations for the areas have been used to inform the following codes and guidance for each Area Type.

Proposals Outside of Area Types

The Authority Wide section does not provide codes for the street network, parking and built form detail, as these are covered by the Area Types. For proposals located outside of the Area Types the detailed design for street network, parking and built form detail will be based upon the following code:

Code AT1: Proposals Outside of Area Types

1. Where a proposal is located outside of an area type, the following should be applied for the Street Network, Parking and Built Form detail:
 - a. Located within the existing urban area - applications will be determined on a case-by-case basis.
 - b. Located outside of existing urban area and adjacent an Area Type -Proposals must use the codes within that Area Type adjacent to the site.
 - c. Located outside of the existing urban area - applications will be determined on a case-by-case basis.

Outer Neighbourhood Area Type

OUTER NEIGHBOURHOOD CODING PLAN

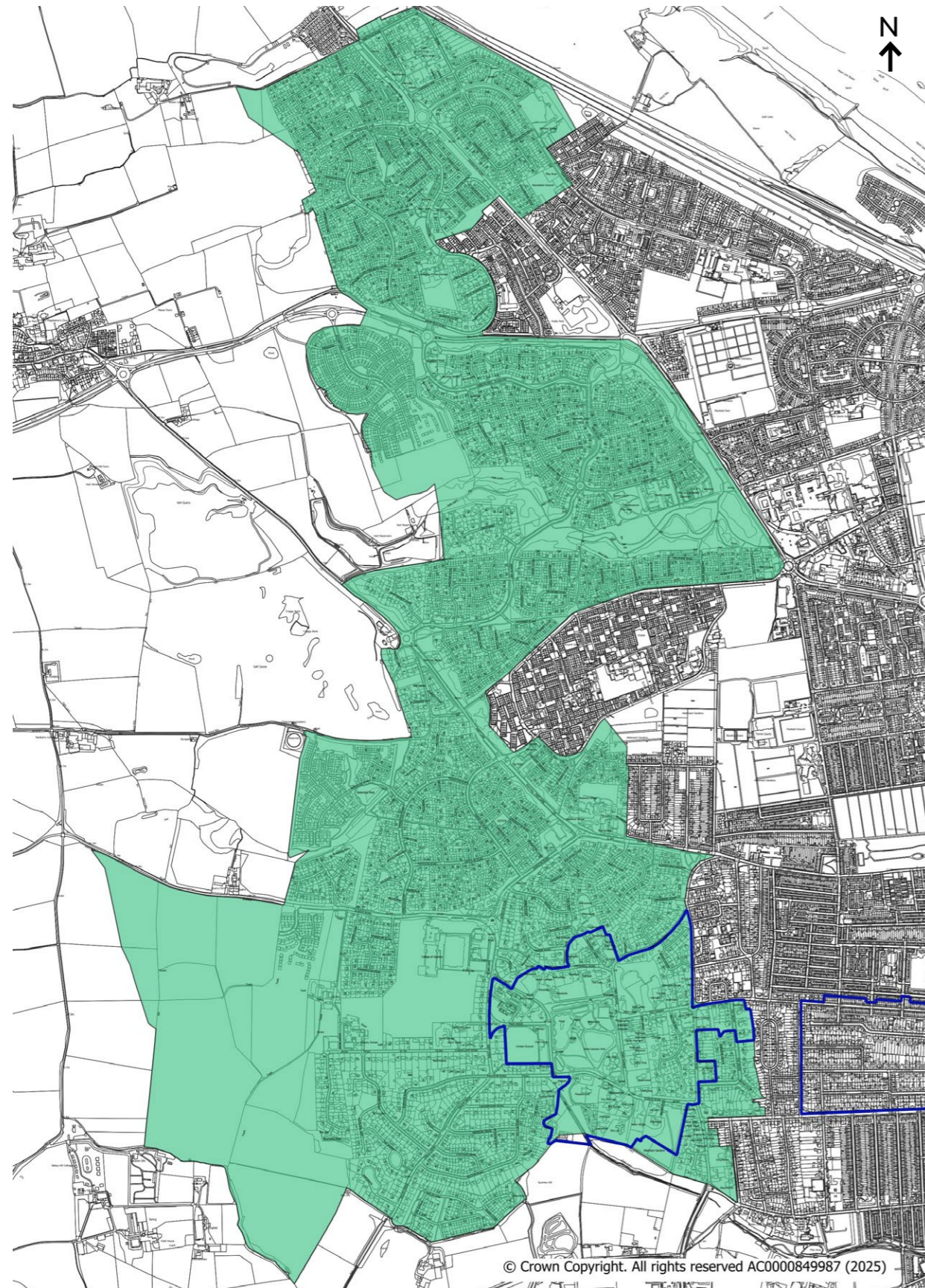


Figure 7: Outer Neighbourhoods Coding Plan (a)

OUTER NEIGHBOURHOOD CODING PLAN

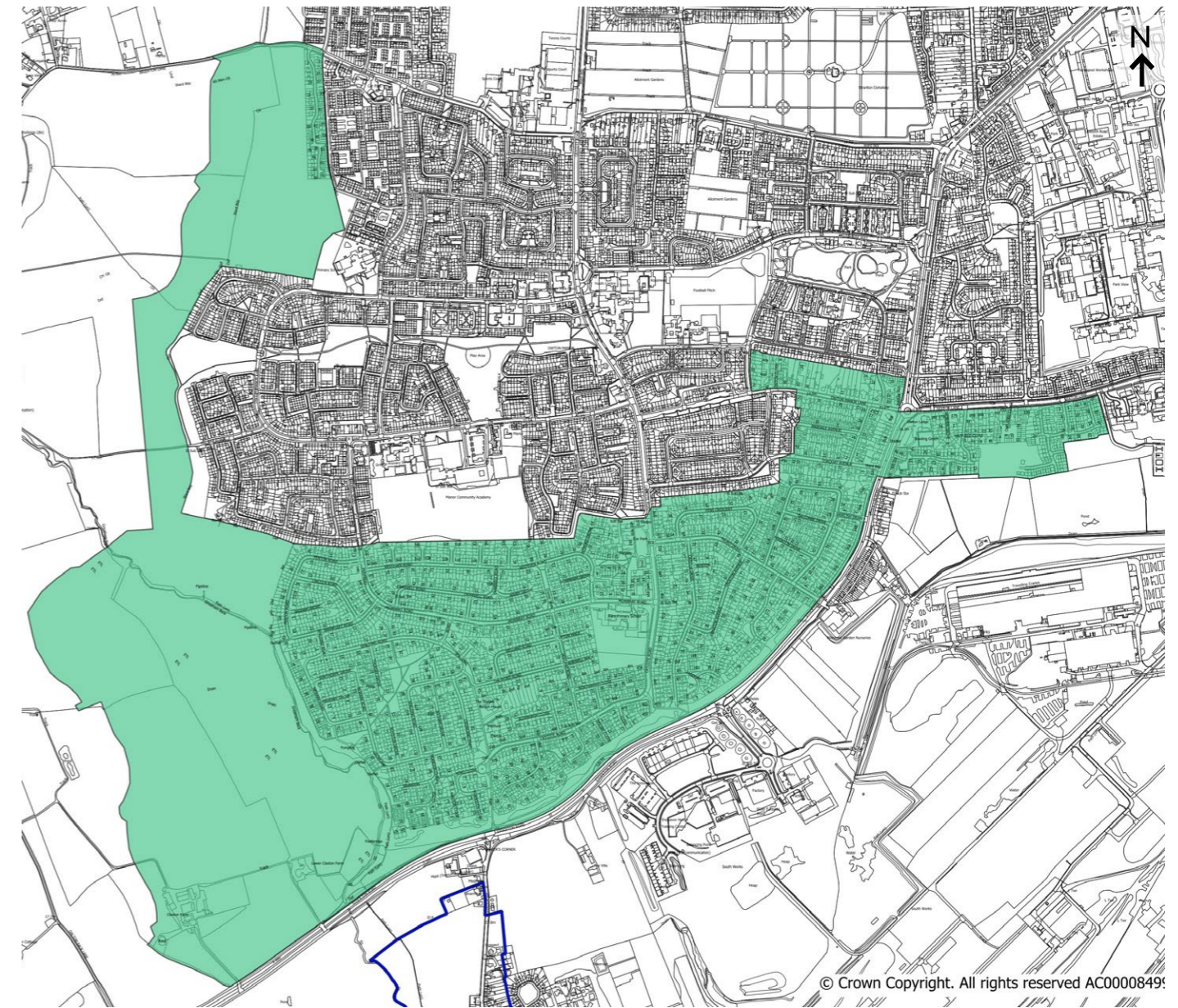




Figure 8: Outer Neighbourhoods Coding Plan (b)

-  Outer Neighbourhood Area Type (ON)
-  Conservation areas

OUTER NEIGHBOURHOOD AREA TYPE (ON)

The Outer Neighbourhood Area Type is characterised by suburban housing on the edges of Hartlepool and the Local Plan housing allocations on the western edge of the urban area. The expansion of Hartlepool over time within this location has resulted in variation between the existing neighbourhoods.

The analysis of the areas has highlighted key characteristics which are present within the neighbourhoods and will be used to inform the codes and guidance for future development within the area.

Outer Neighbourhood Summary of Analysis.

Streets and movement

- Street hierarchies are evident in most areas.
- Wider avenue streets with landscaped verges forming the primary street. Public transport is possible through carriageway width although inappropriate parking can impede this.
- Streets with lower traffics have a greater sense of enclosure.
- Streets have a predominance of front of plot parking where this is not accompanied by front gardens or landscaping the street scene is dominated by parking.
- Inappropriate parking within the street can negatively impact upon the street scene.
- Streets are predominately cul-de-sac's reducing pedestrian permeability and impacting upon way finding.



Existing residential areas within Outer neighbourhood Area Type

- Pedestrian connections between recent developments and existing communities is not always provided.

Built Form

- Properties are predominately detached and semi-detached in nature within instances of short runs of terraces in areas surrounding local centres or key facilities e.g. schools.
- Properties are mainly two storey in nature with increases in scale to 2.5 to 3 storey at key locations such as entrances or around local centres.
- Properties are usually provided with a set back of between 3 and 6m from the street to accommodate front of plot parking and front gardens.
- The building line is typically consistent with variation provided on lower density streets toward the rural edge or green spaces.
- Many of the areas form previous large scale urban extensions such as Bishop Cuthbert.

Materials

- Red brick.
- Buff brick.
- Render - white or cream.
- Tile hangings.



OUTER NEIGHBOURHOOD AREA TYPE (ON)

Architectural Detailing

- There is a mix of architectural styles and features which are typical of their era of construction.
- Common features include:
 - » Mini gables.
 - » Gabled front elevations.
 - » Head and cill detailing.
 - » Hipped roofs.
 - » Chimneys.
 - » Dormer windows.
 - » Bay windows.
 - » Canopies.
 - » Porches.

Green Space / Landscaping

- Large open spaces, with formal planting and play spaces.
- Verges provided on Neighbourhood Streets with limited planting.
- Connections provided into surrounding countryside and PRow network.
- Areas of incidental open space with planting.

Facilities

Local centres and schools have been provided to meet the needs of the community. Pedestrian connections are provided, however the location of the facilities are not always within the centre of the neighbourhood which they serve.



Existing residential areas within Outer neighbourhood Area Type

STREET NETWORK (ON)

The design of the street network plays an important role in determining how it is used. Different streets play different roles in a place depending on the movement upon them, the built form and uses around them and the design of the street space itself, including any natural features.

A key characteristic of the built form within the Outer Neighbourhood Area Type is the provision of a hierarchy of streets, from wide Neighbourhood Streets providing the main connections through the development and to key facilities, to shared surface 'Lanes,' which are more informal streets with a greater sense of enclosure.

You Said:
 » The most positive feature of residential streets is the availability of planting and public green spaces along with wide streets and pavements.



Figure 9: Example street network

- ● ● Main Distributor
- ■ ■ Neighbourhood Street
- - - Residential Street
- Lanes



Street planting - Marleigh Park Phase 2, Cambridge



Street planting - Liberty Quarter, Kent

STREET NETWORK (ON)

Neighbourhood Street

The aim of the Neighbourhood Street is to provide the main pedestrian, cycle, public transport and vehicle connections through a residential area, these routes can include connections between development parcels and local centres, schools and open spaces. Neighbourhood Streets must connect into the existing movement network surrounding the area and provide through routes.

Code ON1: Neighbourhood Street Verge

1. Minimum of 2m verge must be provided on one side of carriageway. This can alternate between sides but must be provided for the entirety of the street.
2. The verge must be provided for at least 70% of the total street length taking into consideration driveway crossings. e.g. for a Neighbourhood Street 100m in length the verge must be provided for at least 70m (See example layout opposite).
3. Verge must incorporate street tree planting (see Code N10).
4. Resting places must be provided every 200m (approx), see Code N6.
5. Where visitor parking is provided, their location within the verge corridor will only be considered acceptable as long as 70% of the total street length is verge.
6. Barriers must not be provided alongside verges or footways. This enables permeability and reduces visual clutter.

Code ON2: Neighbourhood Street Carriageway

1. Provide a carriageway width of 5.5m.
2. Where public transport is proposed the carriageway must measure a minimum of 6.75m.
3. Cul-de-sacs are not permitted.

Code ON3: Neighbourhood Street Footway and Cycleway

1. A minimum footway width of 2m must be provided on both sides of the carriageway.
2. Cycling provision to be either a 3m shared pedestrian/cycleway or a segregated 2m cycleway.
3. Cycle links must connect into nearby existing off-road cycle routes, where available.

Code ON4: Neighbourhood Street Setback

1. Whilst allowing for the separation distances set out in HB1, a set back of between 2m and 6m must be provided from the pavement.
2. Projections such as bay windows, porches and balconies are permitted.
3. Variation: the front face of buildings must not vary more than 3m from the building line.



Figure 10: Neighbourhood Street cross-section

STREET NETWORK (ON)

Neighbourhood Street Guidance

- Where direct access from a Neighbourhood Street is not provided and access is via a private drive or lane (see diagram on pg 72) the setback for the relevant street type should be followed.
- Drainage features such as swales are encouraged in verges (see Code N11).
- The route of Neighbourhood Streets should follow desire lines identified in the context analysis of the site.
- Crossings should be located on or close to desire lines so that pedestrians find them convenient and pleasant to use.

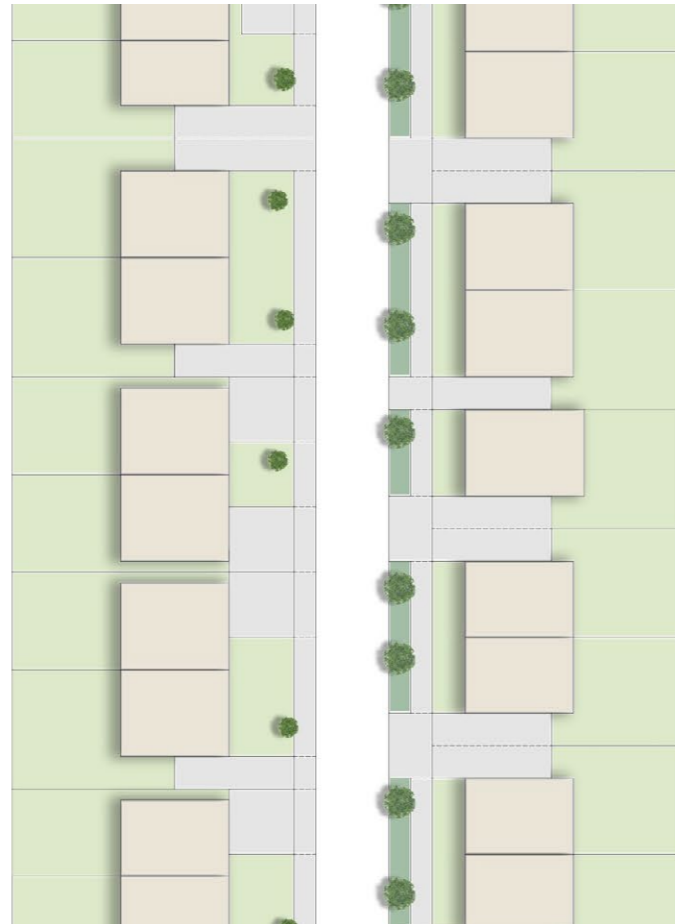


Figure 11: Example of Neighbourhood Street with verge



Verge within Neighbourhood Street - Liberty Quarter, Kent

STREET NETWORK (ON)

Residential Street

Residential Streets mainly carry local traffic and provide connections through development parcels. These streets typically see lower traffic flow and support active travel and encourage the use of streets as social spaces.

Code ON5: Residential Street Carriageway

1. Provide a carriageway width of 5.5m.
2. Cul-de-sacs are not permitted.
3. A minimum footway width of 2m must be provided on both side of carriageway.

Code ON6: Residential Street Landscaping & Planting

1. To ensure the creation of green streets throughout new residential developments and the provision of street trees, which are not within private ownership, landscaping pockets of a minimum of 50m² must be provided within Residential Streets. See Code N5.
2. These pockets must form 'incidental open space' and not form part of the curtilage of any dwelling.
3. Resting places to be provided every 200m (approx) see Code N6.

Code ON7: Residential Street Setback

1. Whilst allowing for the separation distances set out in HB1, a set back of between 3m and 6m must be provided from the pavement.
2. Projections such as bay windows, porches and balconies are permitted.
3. Variation: the front face of buildings must not vary more than 3m from the building line.

Residential Street Guidance

- Cycling should be provided for within the carriageway or as a 3m shared surface pedestrian / cycleway.
- Crossings should be located on or close to desire lines so that pedestrians find them convenient and pleasant to use.



Figure 12: Residential Street example layout

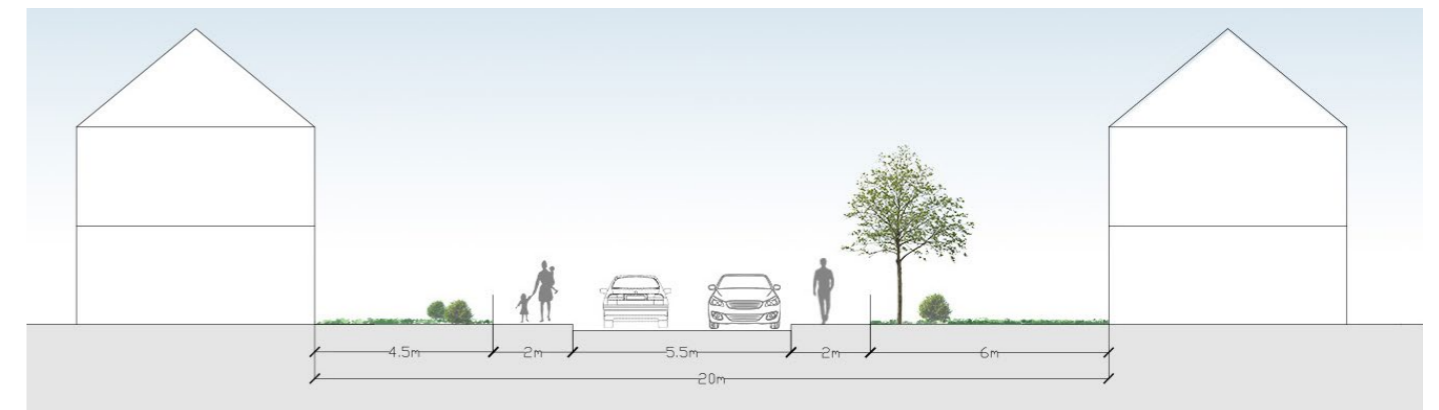


Figure 13: Residential Street cross-section

STREET NETWORK (ON)

Lanes

Lanes will consist of shared surface streets. The removal of a defined carriageway supports active travel and encourages the use of streets as social spaces.

Code ON8: Lanes

1. Serve a Maximum of 25 units.
2. Pedestrian movement has priority.
3. Cul-de-sacs permitted.
4. Provide a minimum carriageway width of 6m + 0.5m hardened maintenance margin.
5. Include a 1.8m service corridor within the carriageway.
6. Provide a change in surface material to distinguish the street type and signal to users that a change in behaviour is required.
7. Pedestrian only connections must be provided between Lanes to ensure permeability. These can include modal filters (see modal filter diagram on pg51).

Code ON9: Lanes Landscaping and Planting

1. To ensure the provision of street trees that are not within private ownership, landscaping pockets of a minimum of 50m² must be provided within Lanes. See Code N5.
2. These pockets must form 'incidental open space' and not form part of the curtilage of any dwelling.

Code ON10: Lanes Setback

1. Whilst allowing for the separation distances set out in HB1, a minimum set back of 2m to be provided from the carriageway.
2. Projections such as bay windows, porches and balconies are permitted.
3. Variation: the front face of buildings must not vary more than 3m from the building line.



Figure 14: Lanes example layout

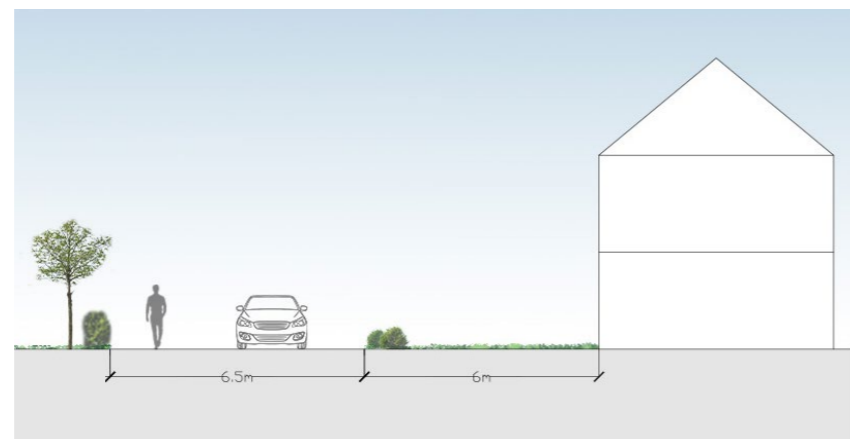


Figure 15: Lanes cross-section

STREET NETWORK (ON)

Lanes Guidance

- Cycling should take place within the carriageway.
- The design of the street should ensure that parking is controlled and only taking place in designated areas.
- Tree planting should be utilised within the street to break up front of plot parking. This can be achieved through:
 - » Landscaping pockets or
 - » In landscaped areas between parking bays.
- Tree planting can be used as a means of traffic calming through diverting the carriageway.
- Changes in material can include block paving or coloured tarmac for the whole of the street or as a 2m transition strip.
- Block paving and coloured tarmac will be subject to a commuted sum for maintenance.



Modal filter - Ashmere Phase 1, Kent



Figure 16: Lanes connectivity - modal filters

STREET NETWORK (ON)

Private Drives

Provide connections for up to five properties and are in private ownership. The streets are encouraged as private social spaces for the properties they serve and have no through traffic.

Private Drive Guidance:

- Providing a change in surface material is encouraged to distinguish the street type whilst also enhancing the character of the street.

Code ON11: Private Drives

1. Must serve no more than 5 properties.
2. A maximum of 25m in length.
3. Minimum of 3.7m in width.
4. Bin collection points to be located a maximum of 25m from the adopted highway (See Code HB5).

Code ON12: Private Drive Landscaping

1. Each property on a Private Drive must be provided with a landscaped front garden following the house type plot ratio in Code ON16.
2. Tree planting must be provided within front gardens for at least 2 properties per Private Drive.

Code ON13: Private Drive Setback

1. Whilst allowing for the separation distances set out in HB1, a minimum set back of 3m from the carriageway.
2. Projections such as bay windows, porches and balconies are permitted.
3. Variation: the front face of buildings must not vary more than 6m from the building line.

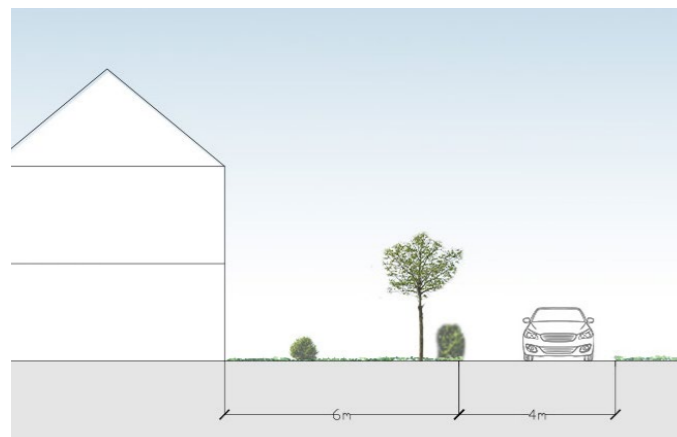


Figure 17: Private Drive cross-section

PARKING (ON)

You Said:

- » The design of parking in new housing could be better.
- » Inappropriate parking should be discouraged.

Parking Provision

A balance is to be made within residential areas on the provision of car parking and the creation of attractive streets.

Parking within new residential developments must sit comfortably within the street scene and not detract from the overall visual aesthetic of the residential area. To ensure this is achieved within new residential developments the following codes must be adhered to.

Code ON14: Parking Provision Standards

1. Development proposals must provide as a minimum the parking standards as set out below.

Unit Size	Number of spaces
1 bedroom	2
2 bedroom	2
3 bedroom	2
4 bedroom	3
5 bedroom+	3
Flatted development	1.5

2. The above parking standards adhere to the Highways Design Guide & Specification – Residential and Industrial Estates Development (amended May 2021).

Parking Methods

There are a range of parking solutions which are considered appropriate for the Outer Neighbourhood, these include:

- In curtilage to the front of properties.

- In curtilage to the side of properties.
- Garage.
- On street allocated.
- On street unallocated.
- Courtyard.

The parking dimensions specified within the following codes align with the Highways Design Guide & Specification – Residential and Industrial Estates Development (amended May 2021).

Code ON15: Parking Methods

1. A maximum of 60% of parking proposed can be to the front of properties. This includes in curtilage (driveway) and allocated on-street parking bays.

Parking to plot ratio

Car parking has a negative impact on the street scene when there is also a lack of landscaping and greenery within the street.

Code ON16: Parking to Plot Ratio

1. The following minimum requirements of front gardens must be adhered to:
 - a. Detached: A minimum of 35 % of the front of plot must be provided as landscaped garden.
 - b. Semi detached: A minimum of 25% of the front of plot must be provided as landscaped garden.

Visitor Parking

The provision for visitor parking is encouraged within proposals to prevent inappropriate parking.

Visitor Parking Guidance

- Where visitor parking is provided it should:
 - » Be spread throughout the proposal.
 - » Not reduce the provision of verge in accordance with the relevant street type codes.
 - » Be screened by planting when located adjoining areas of open space.

PARKING (ON)

In Curtilage - Front of Property

Providing parking in plot to the front or side of properties was considered the most popular during consultation. To ensure that parking does not dominate the street scene and the creation of green streets the following code and guidance are provided:

Code ON17: In Curtilage - Front of Property

1. No more than 4 consecutive front of plot spaces.
2. Where more than 4 spaces are required, an area of soft landscaping with tree and / or hedge planting measuring a minimum of 5m in length x 2m in width must be provided after every 4th space. This includes front gardens.
3. Single space dimensions: 6m in length x 3m in width.
4. Double space dimensions: 6m in length x 5.5m in width.
5. A maximum of 60% of parking proposed can be to the front of properties (see Code ON15)

Front of Property Guidance

- The inclusion of hedge planting to the side of parking bays is encouraged to act as screening.

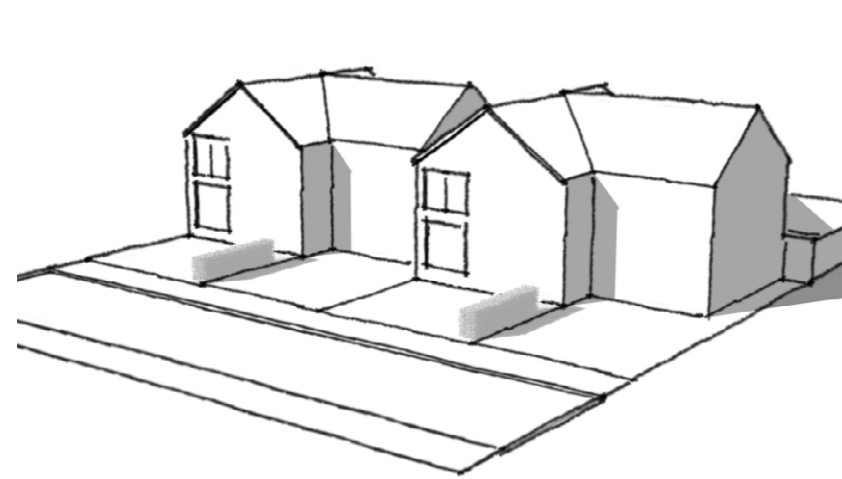


Figure 19: Front of plot parking

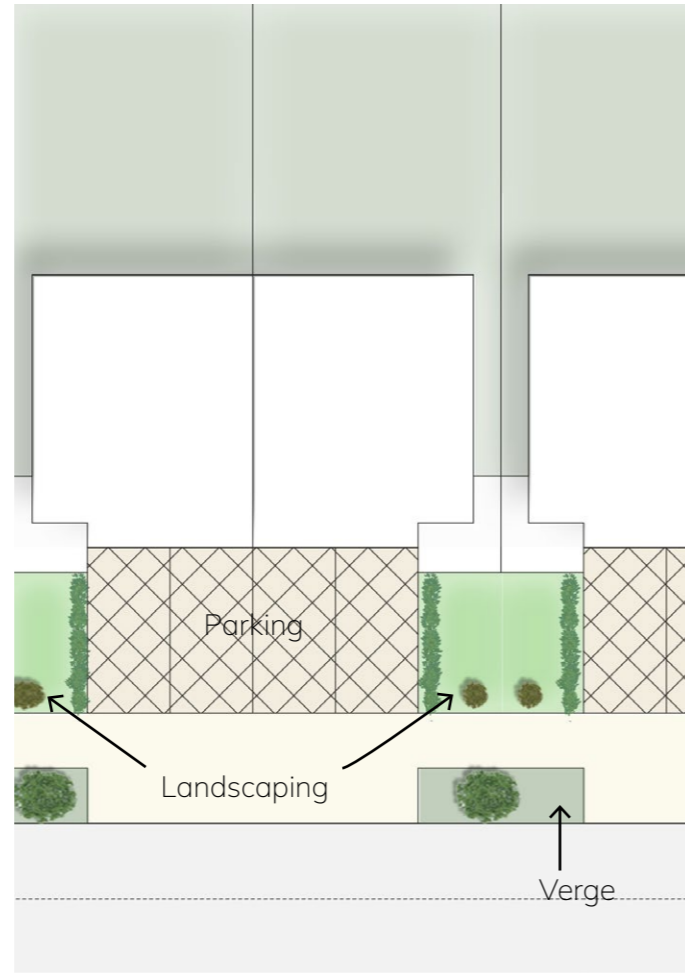


Figure 18: Front of plot parking and landscaping



Screening through planting

PARKING (ON)

In Curtilage - Side of Property

Providing parking to the side of properties enables the provision of front gardens and tree planting, and will also reduce the dominance of parking on the street scene.

Code ON18: In Curtilage - Side of Property

1. Single space dimensions: 6m in length x 3m in width.
2. Double space dimensions: 11m in length x 3m in width.

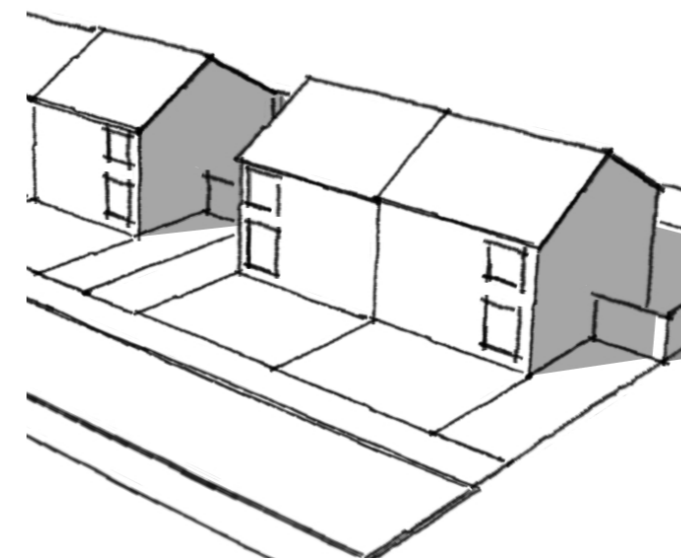


Figure 20: Side of plot parking

Garages

Public consultation indicated that garages are seen as a desirable form of parking in new residential developments. To ensure that they do not impact upon the street scene and encourage active frontages, the following code and guidance are provided:

Code ON19: Garages

1. To count toward parking provision garages must have minimum internal dimensions of 6m x 3m.
2. Detached garages must be a minimum of 2m behind the building line of the associated residential dwelling.
3. No more than 2 consecutive garages. This applies to both integrated and detached.

Garages Guidance:

- Integrated garages can impact upon the street scene through increasing the levels of inactive elevations. It is therefore important to ensure that the level of integrated garages does not dominate the street scene.
- Integrated garages should be recessed with the residential aspect of the dwelling being the primary focus of elevations.

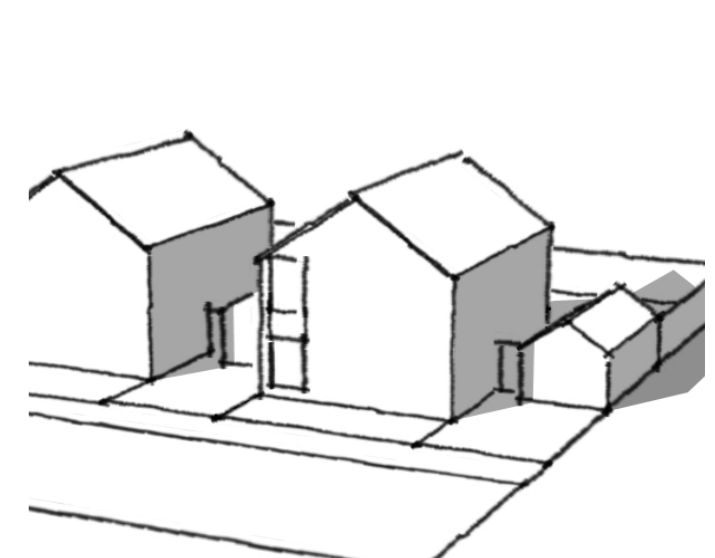


Figure 21: Detached garage

PARKING (ON)

Rear Parking

Parking to the rear of buildings enables the removal of cars from the street scene and the provision of landscaping, however, concerns are raised over their security and safety where natural surveillance is not possible. Rear parking is not considered an appropriate parking solution and should only be used when the following code can be met in full.

Code ON20: Rear Parking

- Where parking is located to the rear within a proposal it must:
 - Be subject to a level of ground floor activation from the surrounding built form.
 - Incorporate an element of permeability to boundary treatments e.g. brick wall and open boarded timber combination.
 - Provide a change in surface material to distinguish between public and private space.
 - Include landscaping with tree or hedge planting at least 2m in width and 4.8m in length after every 4th space.
 - Ensure rear access to properties is maintained.

Rear Parking Guidance

- Rear parking will only be considered in circumstances where a level of ground floor activation is provided from neighbouring properties i.e. where a property on an adjacent street provides frontage.
- Rear parking should be used sparingly within proposals.

Courtyard Parking

The provision of courtyard parking to the front of properties can be a useful parking method in providing allocated on-street parking in a positive setting.

It is important to ensure that courtyards are designed appropriately and must be in the form of small courtyards with landscaping, feature paving and street furniture.

Code ON21: Courtyard Parking

- Proposals for courtyard parking must incorporate the following:
 - Safe pedestrians and cycle movement.
 - Natural surveillance from adjoining houses, or by buildings entered from the parking area.
 - Landscaping as either; a central space with parking around or, as landscaped areas measuring 1m in width after every 4th space and include tree / hedge planting.
 - Central landscaped areas should include planting and be designed as social spaces.
 - Provide a careful balance between the desire of car owners to park as near to their homes as possible and the need to maintain the character of the overall setting.
 - Prevent inappropriate parking through careful placement of street furniture / planting.



Courtyard Parking - Hatlepool

PARKING (ON)

On-Street Parking

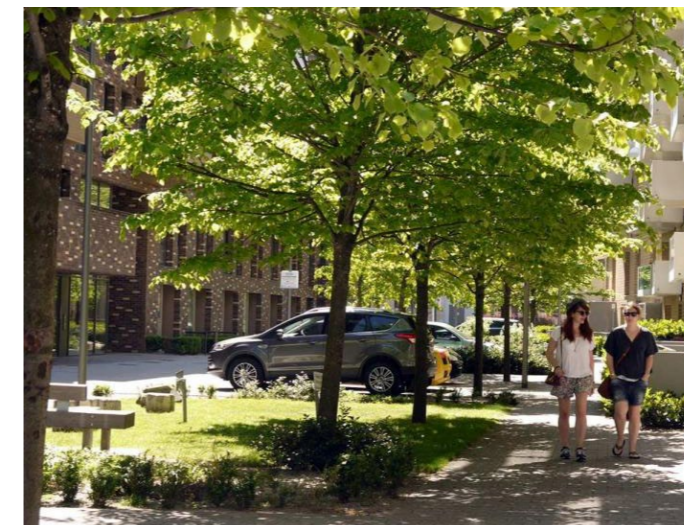
Providing parking within the street can take the form of either allocated or unallocated space.

Code ON22: On Street Allocated & Unallocated

- Proposals for on-street parking must:
 - Be 2.4m in width x 6m in length, for central spaces. End spaces must be 2.4m in width x 4.8m in length.
 - Provide a maximum of 4 consecutive spaces.
 - Provide an area of soft landscaping with tree planting measuring a minimum of 2.4m in width x 4.8m in length after every 4th space. Where more than 4 spaces are provided.
 - Clearly define the space as private for allocated bays.

Terraced properties parking

Where terraced properties are provided with front of plot parking there is a negative impact on the street scene and a reduction in the provision of front gardens.



On-street parking with landscaping - St Andrews

Code ON23: Terraced Properties parking

- Parking for short runs of terraced properties (up to 4 units) must consist of:
 - End properties provided with side of plot parking and front gardens.
 - Central unit's provided with front of plot parking (see image below).
- Parking for more than 4 terraced properties must:
 - Take the form of either courtyard parking and / or on street parking.

Parking Materials

The type of materials used for parking can impact upon the street scene and should be used to define a change from public to private space

Materials Guidance

- Parking within the curtilage of the dwellings should consist of the following:
 - » Permeable paving.
 - » Tarmac.
 - » Blocks.
- Proposals should provide a different surface material to that proposed on footways and roads to clearly distinguished between public and private space.

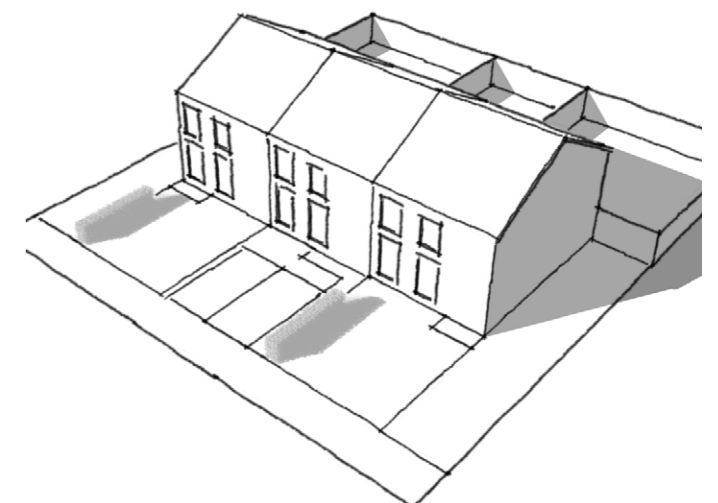


Figure 22: Terraced parking (short runs)

BUILT FORM DETAIL (ON)



Figure 23: Limited permeability through use of cul-de-sacs



Figure 24: Improvements which can be made to create a connected development

BUILT FORM DETAIL (ON)

Urban Form

Development should take the form of perimeter block development with housing facing onto streets/public realm and gardens facing inward, creating a clear distinction between public and private space.

Perimeter blocks can be provided in either formal or informal styles.

- Formal perimeter blocks consist of regular rectangular blocks with consistent building lines and street geometry.
- Informal perimeter blocks are irregular in shape with variations in building lines and curved street geometry.

Urban Form Guidance:

- Formal perimeter blocks should be used for development on Neighbourhood Streets, and near local centres or community facilities, where provided.
- A mix of formal and informal perimeter blocks should be used for development on Residential Streets, Lanes and Private Drives.
- Where development blocks are adjacent to areas of public open space or are located on the rural edge, informal perimeter blocks are encouraged to create a soft interface between urban and rural.

You Said:

- » Cul-De-Sacs are disorientating
- » Being able to navigate streets is important
- » Narrow alleyways raise concerns over safety.

Cul-De-Sacs

Proposals which are based upon an interconnected series of Cul-De-Sacs can reduce the overall permeability of areas, increase walking distances and cause disorientation for users.

Code ON24: Cul-De-Sacs

1. Only permitted on Lanes and Private Drives.
2. Pedestrian permeability must be maintained e.g using modal filters (see pg53).

Cul-Dec-Sac Guidance:

- Cul-De-Sacs within new residential development should only be considered where permeability and wayfinding are maintained for users.

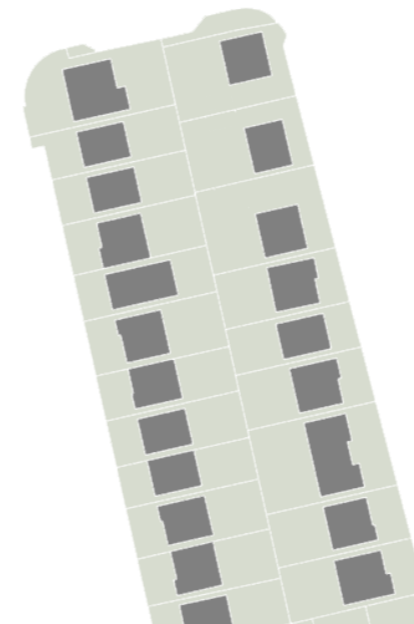


Figure 25: Formal perimeter block



Figure 26: Informal perimeter block

BUILT FORM DETAIL (ON)

Building Line

The shape of the building line will contribute to the character of the area. The Outer Neighbourhood Area Type is characterised by a mix of curved and straight building lines depending on the era of construction and the position within the development.

Frontage Guidance:

- A mix of curved and straight building lines should be created.
- Curved building lines should be on Residential Streets and Lanes with straight building lines being focused on Neighbourhood Streets.
- The building line for Private Drives will be dependant on their location within the development. Close to the site entrance and Neighbourhood Streets should be more formal in nature. Curved building lines should be provided for Private Drives on the rural edge or adjacent to green spaces.
- Houses with side parking can be placed closer to the highway, however, this should not be at the detriment of front gardens, tree planting and residential amenity.

Building Heights

Building heights are predominantly 2 storey within the Outer Neighbourhood Character Area. Changes in building heights can add interest and variety to street scenes. This can be achieved by changes in roof pitch or through changes in number of storeys provided.



Changes in scale -Alkerden Gateway, Kent

Building Heights Guidance:

- The following storey heights should be achieved:
 - » Neighbourhood Street - up to 3 storey.
 - » Residential Street - 2 storey up to 3 storey at key locations.
 - » Lanes - 2 storey up to 2.5 storey at key locations.
 - » Private Drives - 2 storey up to 3 storey at key locations.
 - » Rural edge - 2 storey.
- Key locations include:
 - » Entrances.
 - » Corner locations.
 - » View terminus points.
 - » Focal points.
 - » Neighbouring community facilities.

Housing Mix

The analysis of the Outer Neighbourhood Area Type highlighted the predominance of detached and semi-detached properties. Instances of terraced properties and apartments are also found within the Area Type, these are focused on primary streets or adjacent to local centres/facilities where increases in density are considered appropriate.

Housing Mix Guidance:

- Any new residential development within the Outer Neighbourhoods should comprise:
- Predominately of detached and semi-detached properties.
- Include terraced properties (short runs of up to 4 units) and apartments. These will be considered appropriate on Neighbourhood and Residential Streets, neighbouring key facilities such as a local centre/school and at site entrances.
- Where access alleys are required for the rear of terraced properties they should:
 - » Not negatively impact upon the rear garden sizes of adjoining properties.
 - » Be clearly defined as private space.
 - » Provide gates at their entrances.

BUILT FORM DETAIL (ON)

Gateways / Entrance Features

Proposals for new residential development within the Outer Neighbourhood Area Type should include gateways which create a clear point of arrival and strengthens the sense of place. This is of particular importance for large scale developments over 200 units.

Code ON25: Gateways

1. Where gateways are proposed they must include:
 - a. Landmark buildings (see Code BF3).
 - b. Landscaping and tree planting.
 - c. Entrance features such as feature walls or public artwork.

Materials & Elevational Detailing

It is not the intention of this code to be prescriptive in terms of setting materiality or elevational detailing of proposals or stifle innovative designs.

Materials and Elevational Detailing Guidance

- In accordance with Code BF1, any proposals should have identified the key features of the existing built form, which have a positive influence on the character of the Outer Neighbourhood Area Type.



Examples of potential elevational treatment and materials in the Outer Neighbourhood Area Type

- Proposals should create their own unique character which reflects any features identified through character analysis.
- Key elevational details identified as part of the analysis for this code include the following details:
 - » Mini gables.
 - » Gabled front elevations.
 - » Head and cill detailing.
 - » Hipped roofs.
 - » Chimneys.
 - » Dormer windows.
 - » Bay windows.
 - » Canopies.
 - » Porches.
- Standard house types should be altered to reflect the characteristics identified through Code BF1.
- Large scale proposals over 200 units should consider using character areas to distinguish between the different areas within proposals.
- The below images are provided as examples of potential elevational treatment and materials in the Outer Neighbourhood Area Type.

BUILT FORM DETAIL (ON)

Street scene

Careful consideration needs to be given to the house types proposed and how they will be read within the street.

Code ON26 : Street Scene

1. Within proposed streets there must be the provision of a variety of house types with changes in
 - a. Elevational detailing
 - b. Materiality
 - c. Roof line and/ or ridge height.
2. House types proposed must create a coherent street scene when read as a whole.

Boundary Treatment

Boundary treatments must be used to clearly define changes from public to private space as well as enhancing the character and appearance of the area.

Code ON27: Boundary Treatment

1. Boundary treatment to the front of plots must not exceed 1.2m. Ensuring that a level of natural surveillance from the associated property can be provided over the street.
2. When proposing boundary treatments to the front of plots, consideration must be given to visibility of vehicles entering and exiting the plot.
3. Plots that adjoin areas of public open space must clearly distinguish between public and private space through the provision of a boundary treatment of either:
 - a. Railings.
 - b. Knee rail.
 - c. Hedging.
 - d. Estate fencing

Boundary Treatment Guidance:

- Changes in the type of boundary treatment used on different street typologies is encouraged.
- Boundary treatments should not reduce the overall permeability of the development or reduce the level of openness of the street (see image below).
- Appropriate boundary treatments for Outer Neighbourhood include:
 - » Metal railings.
 - » Hedge.
 - » Low brick wall (brick to compliment the primary material of the residential building).
 - » Brick and metal railing combination.
 - » Knee rail.
- For the established street types the following boundary treatments should be used, where required:
 - » Neighbourhood Street - metal railing, low brick wall, brick and metal railing combination.
 - » Residential Street - metal railing, low brick wall, hedge.
 - » Lanes - metal railing, low brick wall, hedge.
 - » Private Drive - hedge.
 - » Rural edge - non encouraged, hedge if required to define private space.

URBAN EDGE (UE)

Within the Outer Neighbourhood Area Type development will adjoin the urban-rural edge of the borough. It is therefore important to ensure that future residential developments provide an appropriate response, which enables developments to graduate into the rural landscape and respond to potential views.

Code UE1: Street Types

1. Street typologies must include Lanes and Private Drives.

Code UE2: Parking

1. The appropriate parking solutions for the Urban Edge include:
 - a. Front of plot see Code ON17 with a front garden following the ratio's set within Code ON16.
 - b. Side of plot Code see Code ON18.
 - c. Garages see Code ON19.

Code UE3: Connections

1. To encourage active travel and promote health and well-being, connections by modes of active travel must be provided into the countryside, where available.
2. Pedestrian and cycle connections must connect into existing Public Rights of Way or Permissive Routes in accordance with Codes M4 - M7.

Code UE4: Frontage

1. Properties must provide frontage onto the Urban Edge.
2. Where side elevations are proposed these must be corner turning properties with dual frontages (see Code BF2).

3. Presenting rear elevations and boundary fencing onto the Urban Edge will not be considered an appropriate response.

Code UE5: Amenity Space

1. Landscaped front gardens with tree planting must be provided for all properties fronting onto the urban edge.

Code UE6 Boundary Treatments

1. To optimise the integration of the built form into the countryside boundary treatments are not encouraged.
2. Where a clear distinction between public and private space is required the following must be provided:
 - a. Low-level hedge.
 - b. Estate fencing.

Urban Edge Guidance:


- Where Private Drives are proposed on the Urban Edge these should not reduce the overall permeability of proposals. Where the predominant use of Private Drives prevents the movement of users, pedestrian connections to an adoptable standard should be placed within a verge or adjacent green space.
- Changes in surface material of streets should be provided to lessen vehicle speeds.
- Informal development blocks are encouraged on the urban edge to create a soft interface between urban and rural.
- Incorporate a tree lined landscaped edge to soften the visual impact between urban and rural.



Treatment onto Urban Edge - CP2a, Newhall

OUTER NEIGHBOURHOOD MATRIX

Street type	Neighbourhood Street	Residential Street	Lanes	Private Drives
Verge	2m on one side of carriageway	Non Mandatory	Non Mandatory	Non Mandatory
Landscaping	Planting and trees within verge.	Landscaping pockets min.50m ²	Landscaping pockets min.50m ²	Front gardens
Street Trees	Yes in verge	Yes in landscaping pockets	Yes in landscaping pockets	Front gardens, minimum of 2 trees per street.
Minimum Carriageway Width	5.5 - 6.75m	5.5m	6.5m	3.7m minimum.
Minimum Footway Width	2m (both sides of carriageway)	2m (both sides of carriageway)	Non defined in carriageway	N/A
Resting Places	Every 200m (approx)	Every 200m (approx)	In landscape pockets	No
Cycleway	Yes. 2m segregated or 3m shared.	Non defined in carriageway or 3m shared.	Non defined in carriageway	No
Block Style	Formal	Mix	Mix	Mix
Building Line	Straight	Curved	Curved	Mix
Setback	2 - 6m	3 - 6m	2m minimum	2m minimum
Variation	3m	3m	3m	6m
Cul-de-sacs Permitted?	No	No	Yes	Yes
Permitted on Urban Edge?	No	No	Yes	Yes
House Types	Detached Semi- detached Apartments Terraces	Detached Semi- detached Terraces	Detached Semi- detached	Detached Semi- detached
Boundary Treatment	Metal railing Low brick wall Brick wall and railing combination None	Metal railing Low brick wall Hedge None	Metal railing Low brick wall Hedge None	Hedge None
Building Heights	Up to 3 storey	2 storey up to 3 at key locations	2 storey up to 2.5 at key locations	2 storey up to 3 storey at key locations

 Code requirement

Wynyard Area Type

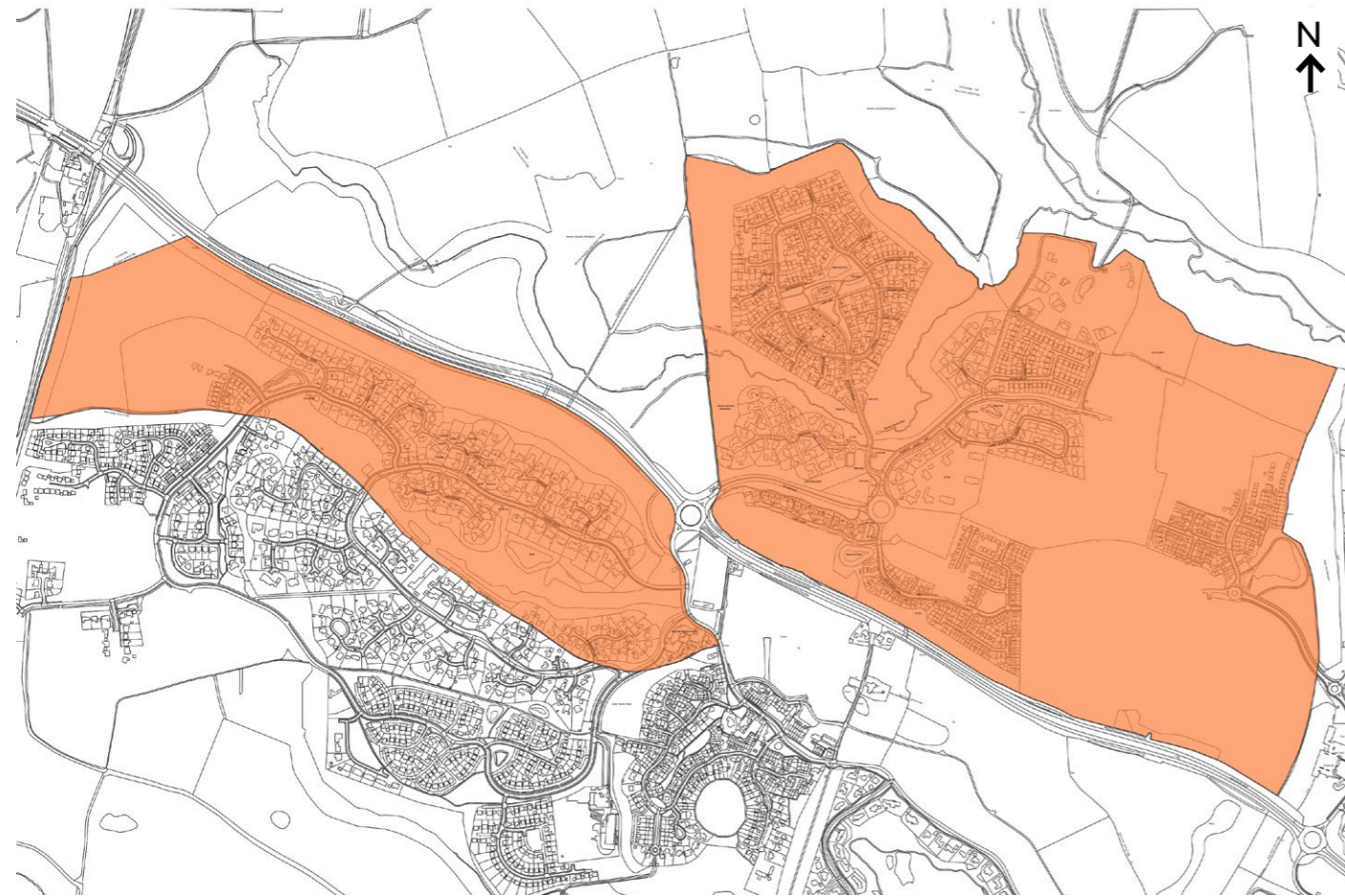



Figure 27: Wynyard Coding Plan

 Wynyard Area Type (WY)

WYNYARD AREA TYPE (WY)

The Wynyard Area Type covers the Hartlepool borough area of Wynyard. Wynyard is allocated as a housing growth area within the Local Plan (2018) and is located on the south western edge of the borough. Wynyard is characterised by modern family homes with a woodland backdrop.

The adopted Wynyard Masterplan (2019) provides a framework for the development of the Wynyard area. Subsequent planning applications have superseded many of the elements of the masterplan. The codes and guidance set out within this SPD are intended to supplement the Place Making Design Principles within the adopted Masterplan.

Wynyard Summary of Analysis.

Streets and Movement

- A wide boulevard with a grassed verge either side of the carriageway characterises the main street both north and south of the A689.
- Residential streets leading from the main boulevard into development parcels provide hierarchy of streets.
- Internal streets are predominately cul-de-sacs.
- Street trees are provided in some instances, e.g. The Pentagon and Siskin Park, the remainder of areas rely on trees within front gardens
- Streets with lower traffic levels have a greater sense of enclosure.
- Tighter building lines and set backs in more

recent development.

- Streets are curved in nature.
- Combination of front of plot and side of plot parking.
- Gated communities with reduced permeability.
- Network of walking routes providing connections provided into surrounding countryside and PRow network.

Built Form

- Properties are predominately large detached houses, more recent developments have introduced semi-detached properties and apartments.
- Properties are between 2 and 2.5 storey in nature with instances of 1 and 3 storey properties.
- Properties are provided with a set back of between 3 and 6m from the street.
- Building lines are consistent in more recent developments with limited variation. The provision of curved streets creates an informal feel.
- Properties provided with front gardens and on plot parking.

Materials

- Red brick.
- Buff brick.
- Multi brick.
- Render - white or cream.
- Tile hangings.



The Pentagon - Street with verge and street trees



Large detached homes

WYNYARD AREA TYPE (WY)

Architectural Detailing

- There is a mix of modern and traditional architectural styles.
- Custom built housing provides an eclectic mix of styles and detailing in some areas.
- Clearly defined different phases of development through changes in architectural styles.
- Some instances of repetitive house types with limited variation in places.
- Common features include:
 - » Mini gables.
 - » Gabled front elevations.
 - » Head and cill detailing.
 - » Chimneys.
 - » Brick detailing.
 - » Window surrounds.
 - » Glazed panels.
 - » Dormer windows.
 - » Bay windows.
 - » Elongated contemporary windows.
 - » Canopies.
 - » Porches.
 - » Balconies.

Boundary Treatment

- Metal railings.
- Brick pillars.
- Low brick wall.
- Open front gardens.

Green Space / Landscaping

- Large green spaces with formal planting and play spaces.
- Incremental areas of landscaping and landscaped front gardens create a 'green' feel to areas.
- Verges provided on Boulevard and Neighbourhood Streets with instances of tree planting.

Facilities

Facilities are currently limited to the south of Wynyard within Stockton Borough Council. A local centre will be provided to the north of the A689.



Existing residential areas within Wynyard Area Type

STREET NETWORK (WY)

The street network within Wynyard has been established through the existing and forthcoming residential areas however, there are inconsistencies between the street types between the different development areas. The following codes have been produced to ensure a coherent street network is provided going forward and to focus on the creation of streets, which are well connected, attractive and usable as social spaces.

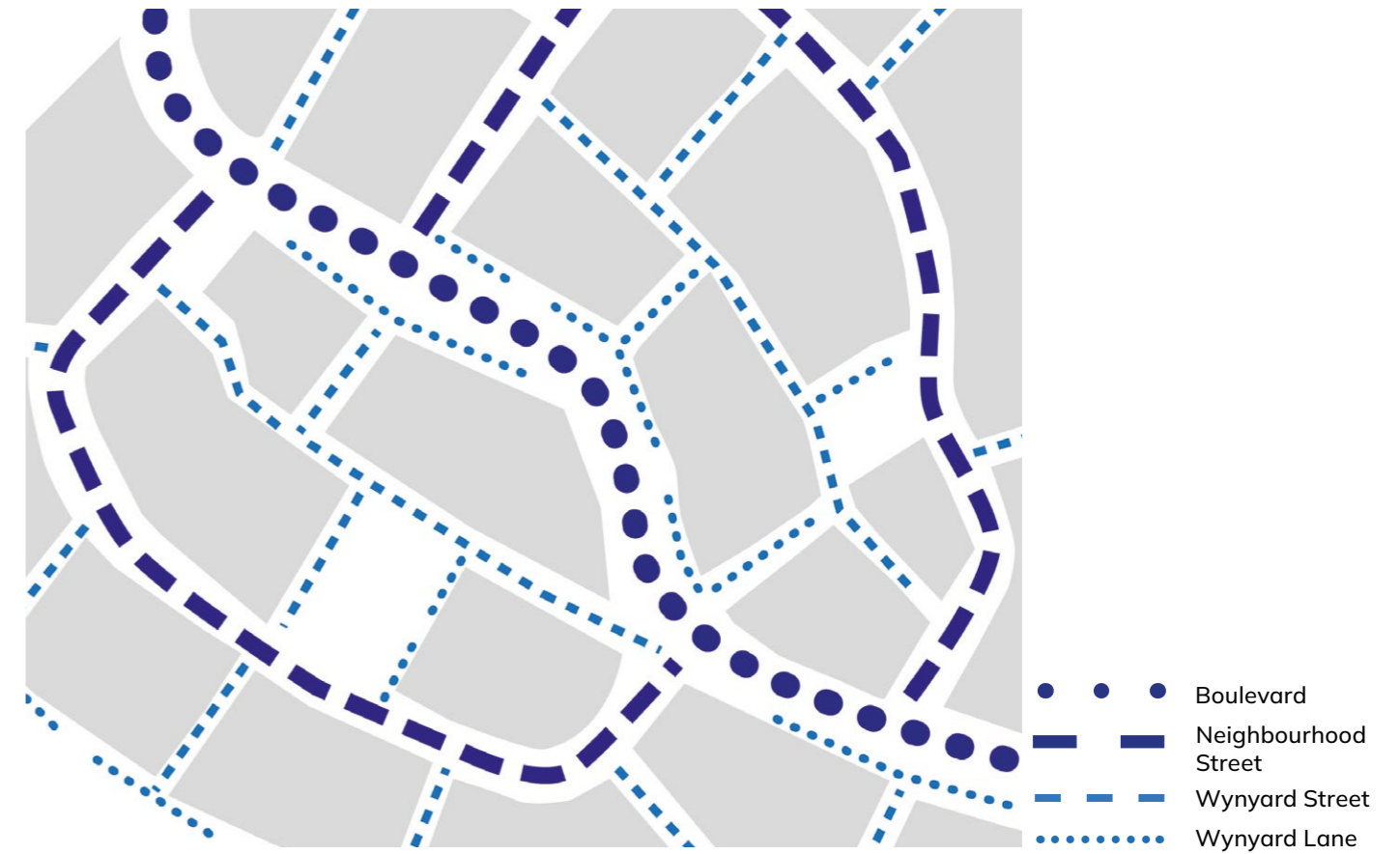


Figure 28: Wynyard example street network



Boulevard -Seaham Garden Village, Durham



Shared surface street- Alkerden Village, Kent

STREET NETWORK (WY)

Boulevard

The Boulevard provides the main pedestrian, cycle, public transport and vehicle connections through the Wynyard area, these streets will be the backbone of the street network connecting onto the A689, between development parcels and to facilities including local centres, schools and open spaces. Within the Wynyard Masterplan these are identified as Primary Arterial Streets.

Code WY1: Boulevard Verge

1. Minimum of 2m verge must be provided on both sides of the carriageway.
2. Verge must incorporate street tree planting (see Code N10).
3. Resting places must be provided every 200m (approx) see Code N6.
4. Where visitor parking is provided their location within the verge area will be considered acceptable as long as 70% of the total street length is verge.
5. Barriers must not be provided alongside verges or footways to enable permeability and reduce visual clutter.

Code WY2: Boulevard Carriageway

1. Provide a carriageway width of 6.75m to enable public transport provision.
2. Cul-de-sacs are not permitted.



Figure 29: Example of Boulevard with verge



Figure 30: Boulevard cross-section

STREET NETWORK (WY)

Code WY3: Boulevard Footway & cycleway

1. Footway must be provided on both sides of the carriageway with a minimum width of 2m.
2. Footways to provide direct connections to:
 - a. Development parcels.
 - b. Existing active travel routes.
 - c. Future active travel routes in accordance with the Wynyard Park Woodland Footpath Strategy.
 - d. Key facilities identified in the site analysis including the local centre and school (see Code SA1).
3. Cycling provision to be included as a 3m shared pedestrian/cycleway or a segregated 2m cycleway.
4. Cycle links to connect into existing off-road cycle routes.

Code WY4: Boulevard frontage

1. Frontage must be provided onto the Boulevard, presenting rear boundaries will not be considered acceptable.

Code WY5: Boulevard Setback

1. Whilst allowing for the separation distances set out in HB1, a set back of between 2m and 6m must be provided from the pavement.
2. Projections such as bay windows, porches and balconies are permitted.
3. Variation: the front face of buildings must not vary more than 3m from the building line.

Boulevard Guidance

- Where direct access from a Boulevard is not provided and access is provided via a private drive or lane (see diagram opposite), the setback for the relevant street type should be followed.
- Crossings should be located on or close to desire lines so that pedestrians find them convenient and pleasant to use.
- Drainage features such as swales are encouraged in verges (see Code N11).

STREET NETWORK (WY)

Wynyard Neighbourhood Street

The aim of the Neighbourhood Street is to provide the main pedestrian, cycle, public transport and vehicle connections through a residential area, these routes can include connections between development parcels and local centres, schools and open spaces. Neighbourhood Streets will connect into the existing movement network surrounding the area and provide through routes.

Code WY6: Wynyard Neighbourhood Street Verge

1. A 2m verge must be provided on both sides of the carriageway.
2. The verge must be provided for at least 70% of the total street length taking into consideration driveway crossings. E.g. for a Neighbourhood Street 100m in length the verge must be provided for at least 70m (see opposite).
3. Verge must incorporate street tree planting (see Code N10).
4. Resting places must be provided every 200m (approx), see Code N6.
5. Where visitor parking is provided their location within the verge is considered acceptable as long as 70% of the total street length is verge.
6. Barriers must not be provided alongside verges or footways to enable permeability and reduce visual clutter.

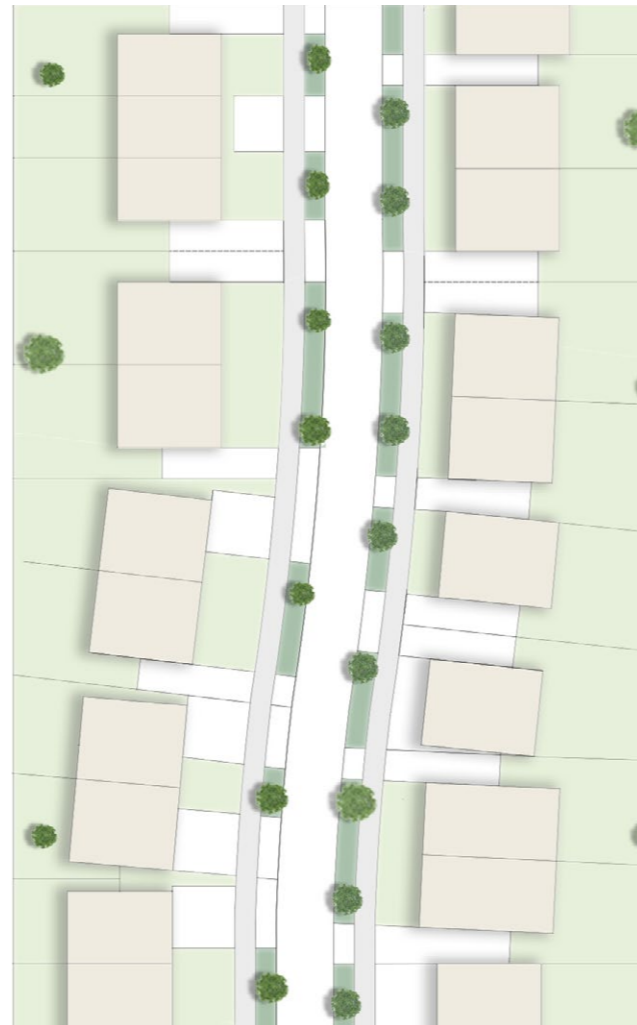


Figure 31: Example of Wynyard Neighbourhood Street



Figure 32 Neighbourhood Street cross-section

STREET NETWORK (WY)

Code WY7: Wynyard Neighbourhood Street Carriageway

1. Provide a carriageway width of 5.5m.
2. Cul-de-sacs are not permitted.

Code WY8: Wynyard Neighbourhood Street Footway & cycleway

1. A minimum footway width of 2m must be provided on both sides of the carriageway.
2. Cycling provision to be as a 3m shared pedestrian/cycleway or a segregated 2m cycleway.
3. Cycle links to connect into nearby existing off-road cycle routes, where available.

Code WY9: Wynyard Neighbourhood Street Setback

1. Whilst allowing for the separation distances set out in HB1, a set back of between 2m and 6m must be provided from the pavement.
2. Projections such as bay windows, porches and balconies are permitted.
3. Variation: the front face of buildings must not vary more than 3m from the building line.

Wynyard Neighbourhood Street Guidance

- The route of Neighbourhood Streets should follow desire lines identified in the context analysis of the site.
- Crossings should be located on or close to desire lines so that pedestrians find them convenient and pleasant to use.
- Drainage features such as swales are encouraged in verges (see Code N11)



Planting within street - The Nightingales, Andover



Existing Neighbourhood Street - Wynyard

STREET NETWORK (WY)

Wynyard Street

Wynyard Streets mainly carry local traffic and provide connections through development parcels. These streets typically see lower traffic flow and support active travel and encourage the use of streets as social spaces.

Code WY10: Wynyard Street Carriageway

1. Provide a carriageway width of 5.5m.
2. Cul-de-sacs are not permitted.
3. A minimum footway width of 2m must be provided on both sides of the carriageway.

Code WY11: Wynyard Street Landscaping & Planting

1. To ensure the creation of green streets throughout new residential developments and the provision of street trees, which are not within private ownership, landscaping pockets of a minimum of 50m² must be provided within Residential Streets. See Code N5.
2. These pockets must form 'incidental open space' and not form part of the curtilage of any dwelling.
3. Resting places to be provided every 200m (approx) see Code N6.

Code WY12: Wynyard Street Setback

1. Whilst allowing for the separation distances set out in HB1, a minimum set back of 2m must be provided from the pavement.
2. Projections such as bay windows, porches and balconies are permitted.
3. Variation: the front face of buildings must not vary more than 3m from the building line.

Wynyard Street Guidance

- Crossings should be located on or close to desire lines so that pedestrians find them convenient and pleasant to use.



Figure 33: Example of Wynyard Street

STREET NETWORK (WY)

Wynyard Lanes

Lanes will consist of a shared surface street. The removal of a defined carriageway supports active travel and encourages the use of streets as social spaces.

Code WY13: Wynyard Lanes Carriageway

1. Serve a Maximum of 25 units.
2. Pedestrian movement has priority.
3. Cul-de-sacs permitted.
4. Provide a minimum carriageway width of 6m + 0.5m hardened maintenance margin.
5. Include a 1.8m service corridor within the carriageway.
6. Provide a change in surface material to distinguish the street type and signal to users that a change in behaviour is required.
7. Pedestrian only connections must be provided between Wynyard Lanes to ensure permeability. These can include modal filters.

Code WY14: Wynyard Lanes Landscaping and Planting

1. To ensure the provision of street trees that are not within private ownership, landscaping pockets of a minimum of 50m² must be provided within Lanes. See Code N5.
2. These pockets must form 'incidental open space' and not form part of the curtilage of any dwelling.

Code WY15: Wynard Lanes Setback

1. Whilst allowing for the separation distances set out in HB1, a minimum set back of 2m must be provided from the carriageway.
2. Projections such as bay windows, porches and balconies are permitted.
3. Variation: the front face of buildings must not vary more than 3m from the building line.

Wynyard Lanes Guidance

- Cycling on Lanes should be able to take place within the carriageway.
- The design of the street should ensure that parking is clear, legible and only taking place in designated areas.
- Visitor parking should be clearly marked.
- Changes in material can include block paving or coloured tarmac for the whole of the street or as a 2m transition strip.
- Block paving and coloured tarmac will be subject to a commuted sum for maintenance.
- Tree planting should be utilised within the street to break up front of plot parking. This can be achieved through:
 - » Landscaping pockets or
 - » In landscaped areas between parking bays.
- Tree planting can be used as a means of traffic calming through diverting the carriageway.

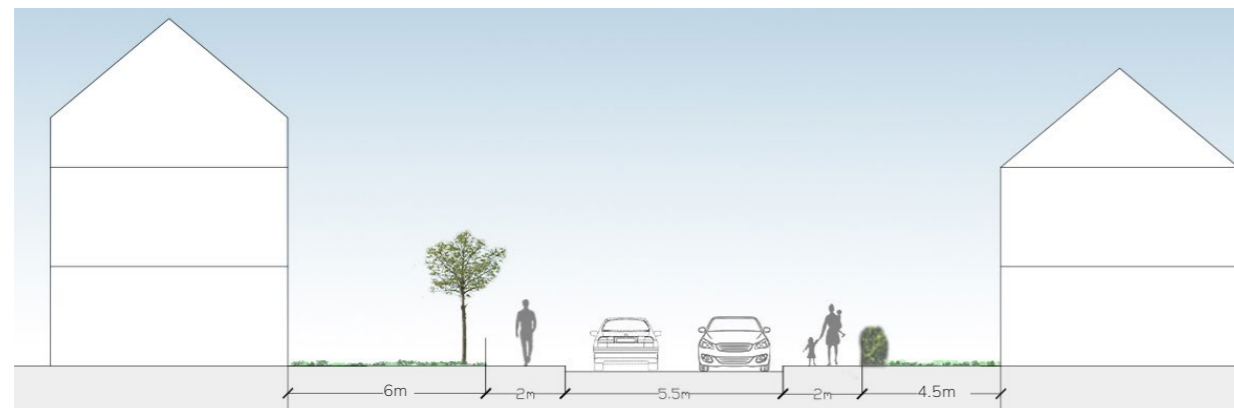


Figure 34: Wynyard Street cross-section

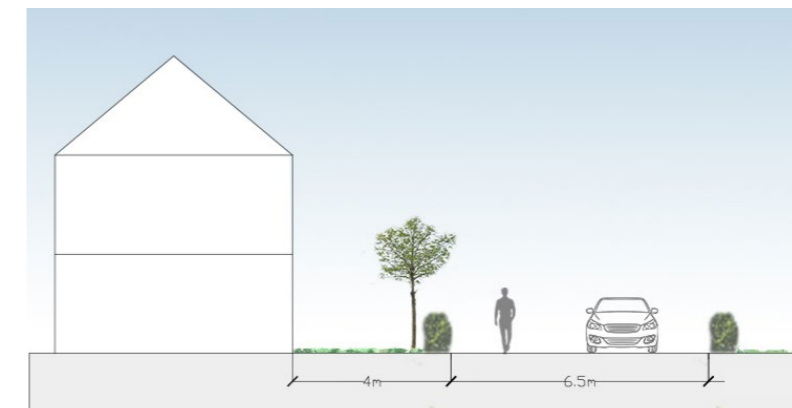


Figure 35: Wynyard Lanes cross-section

STREET NETWORK (WY)

Wynyard Private Drives

Private Drives provide connections for a small group of properties and are in private ownership. The streets are encouraged as private social spaces for the properties they serve and have no through traffic.

Code WY16: Wynyard Private Drives

1. Must serve no more than 5 properties.
2. A maximum of 25m in length.
3. Minimum of 3.7m in width.
4. Bin collection points to be located a maximum of 25m from the adopted highway.
5. Cul-de-sacs permitted

Code WY17: Wynyard Private Drive Landscaping

1. Each property on a Wynyard Private Drive must be provide with a landscaped front garden following the house type plot ratio in Code WY21.
2. Tree planting must be provided within front gardens for at least 2 properties per Private Drive.

Code WY18: Wynyard Private Drive Setback

1. Whilst allowing for the separation distances set out in HB1, a minimum set back of 2m must be provided from the pavement.
2. Projections such as bay windows, porches and balconies are permitted.
3. Variation: the front face of buildings must not vary more than 6m from the building line.

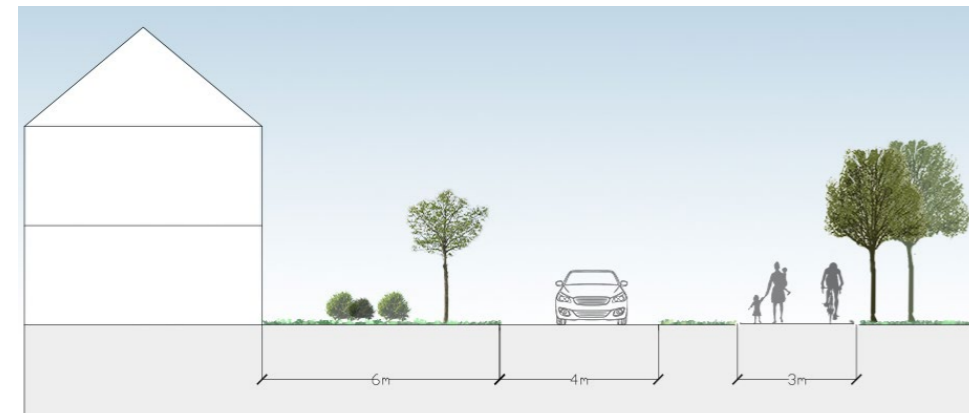


Figure 36: Wynyard Private Drive onto Green Wedge cross-section

PARKING (WY)

You Said:

- » The design of parking in new housing could be better and inappropriate parking should be discouraged.

Parking Provision

A balance is to be made within residential areas on the provision of car parking and the creation of attractive streets.

Parking within new residential developments must sit comfortably within the street scene and not detract from the overall visual aesthetic of the residential area. To ensure this is achieved within new residential developments the following codes must be adhered to.

Code WY19: Parking Provision Standards

1. Development proposals must provide as a minimum the parking standards as set out below.

Unit Size	Number of spaces
1 bedroom	2
2 bedroom	2
3 bedroom	2
4 bedroom	3
5 bedroom+	3
Flatted development	1.5

2. The above parking standards adhere to the Highways Design Guide & Specification – Residential and Industrial Estates Development (amended May 2021).

Parking Methods

There are a range of parking solutions which are considered appropriate for Wynyard, these include:

- In curtilage to the front of properties.
- In curtilage to the side of properties.

- Garage.
- On street allocated.
- On street unallocated.
- Courtyard.

The parking dimensions specified within the following codes align with the Highways Design Guide & Specification – Residential and Industrial Estates Development (amended May 2021).

Code WY20: Parking Methods

1. A maximum of 60% of parking proposed can be to the front of properties. This includes in curtilage (driveway) and allocated on-street parking bays.

Parking to plot ratio

Car parking has a negative impact on the street scene when there is also a lack of landscaping and greenery within the street.

Code WY21: Parking to Plot Ratio

1. The following minimum requirements must be adhered to:
 - a. Detached: A minimum of 35 % of the front of the plot must be provided as landscaped garden.
 - b. Semi detached: A minimum of 25% of the front of the plot must be provided as landscaped garden.

Visitor Parking

The provision for visitor parking is encouraged within proposals to prevent inappropriate parking.

Visitor Parking Guidance

- Where visitor parking is provided it should:
 - » Be spread throughout the proposal.
 - » Not reduce the provision of verge in accordance with the relevant street type codes.
 - » Be screened by planting when located adjoining areas of open space.

PARKING (WY)

In curtilage - Front of Property

Providing parking in plot to the front or side of properties was considered the most popular during consultation. To ensure that parking does not dominate the street scene and the creation of green streets, the following code and guidance are provided:

Code WY22: In Curtilage - Front of Property

1. No more than 4 consecutive front of plot spaces.
2. Where more than 4 spaces are required, an area of soft landscaping with tree and / or hedge planting measuring a minimum of 5m in length x 2m in width must be provided after every 4th space. This includes front gardens.
3. Single space dimensions: 6m in length x 3m in width.
4. Double space dimensions: 6m in length x 5.5m in width.
5. A maximum of 60% of parking proposed can be to the front of properties (see Code WY20)

Front of Property Parking Guidance

- The inclusion of hedge planting to the side of parking bays is encouraged to act as screening.

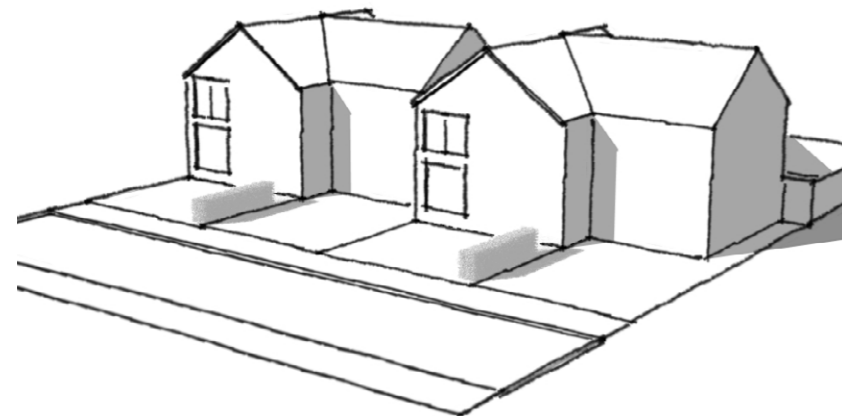


Figure 38: Front of plot parking

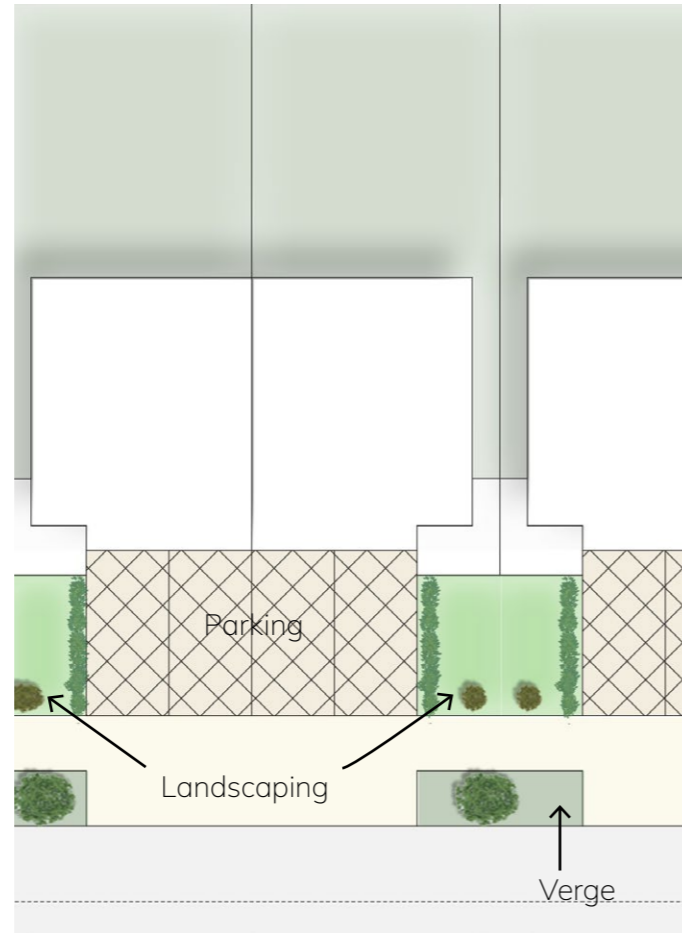


Figure 37: Front of plot parking and landscaping



Screening through planting,

PARKING (WY)

In Curtilage - Side of Property

Providing parking to the side of properties enables the provision of front gardens and tree planting and will also reduce the dominance of parking on the street scene.

Code WY23: In Curtilage - Side of Property

1. Single space dimensions: 6m in length x 3m in width.
2. Double space dimensions: 11m in length x 3m in width.

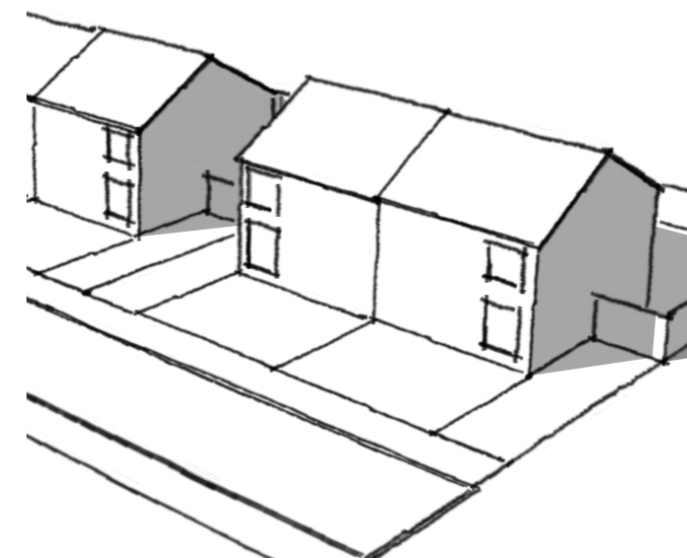


Figure 39: Side of plot parking

Garages

Public consultation indicated that garages are seen as a desirable form of parking in new residential developments. To ensure that they do not impact upon the street scene and encourage active frontages the following code and guidance are provided:

Code WY24: Garages

1. To count toward parking provision garages must have minimum internal dimensions of 6m x 3m.
2. Detached garages must be a minimum of 2m behind the building line of the associated residential dwelling.
3. No more than 2 consecutive garages. This applies to both integrated and detached.

Garages Guidance:

- Integrated garages can impact upon the street scene through increasing the levels of inactive elevations. It is therefore important to ensure that the level of integrated garages does not dominate the street scene.
- Integrated garages should be recessed with the residential aspect of the dwelling being the primary focus of elevations.

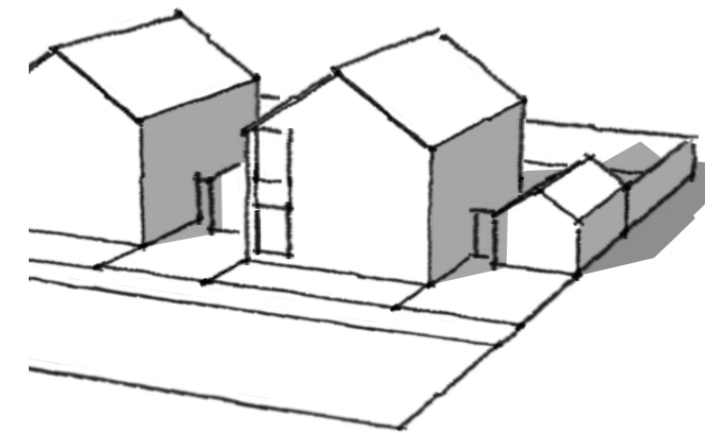


Figure 40: Detached garage

PARKING (WY)

Courtyard parking

The provision of courtyard parking can be a useful parking method in providing allocated on street parking in a positive setting.

It is important to ensure that courtyards are designed appropriately and must be in the form of small courtyards with landscaping, feature paving and street furniture.

Code WY25: Courtyard Parking

1. Proposals for courtyard parking must incorporate the following:
 - a. Safe pedestrians and cycle movement.
 - b. Natural surveillance from adjoining houses, or by buildings entered from the parking area.
 - c. Landscaping as either; a central space with parking around or, as landscaped areas measuring 1m in width after every 4th space and include tree / hedge planting.
 - d. Central landscaped areas should include planting and be designed as social spaces.
 - e. Provide a careful balance between the desire of car owners to park as near to their homes as possible and the need to maintain the character of the overall setting.
 - f. Prevent inappropriate parking through careful placement of street furniture / planting.



Courtyard Parking - Hatlepool

On Street Parking

Providing parking within the street can take the form of either allocated or unallocated space.

Code WY26: On Street Allocated & Unallocated

1. Proposals for on-street parking must:
 - a. Be 2.4m in width x 6m in length, for central spaces. End spaces must be 2.4m in width x 4.8m in length.
 - b. Provide a maximum of 4 consecutive spaces.
 - c. Provide an area of soft landscaping with tree planting measuring a minimum of 2.4m in width x 4.8m in length after every 4th space. Where more than 4 spaces are provided.
 - d. Clearly define the space as private for allocated bays.



On-street parking with landscaping - St Andrews

PARKING (WY)

Terraced properties parking

Where terraced properties are provided with front of plot parking there is a negative impact on the street scene and a reduction in the provision of front gardens.

Code WY27: Terraced Properties Parking

1. Parking for short runs of terraced properties (up to 4 units) must consist of:
 - a. End properties provided with side of plot parking and front gardens.
 - b. Central unit's provided with front of plot parking (see image below).
2. Parking for more than 4 terraced properties must:
 - a. Take the form of either courtyard parking and / or on street parking.
3. Rear parking will not be considered appropriate.

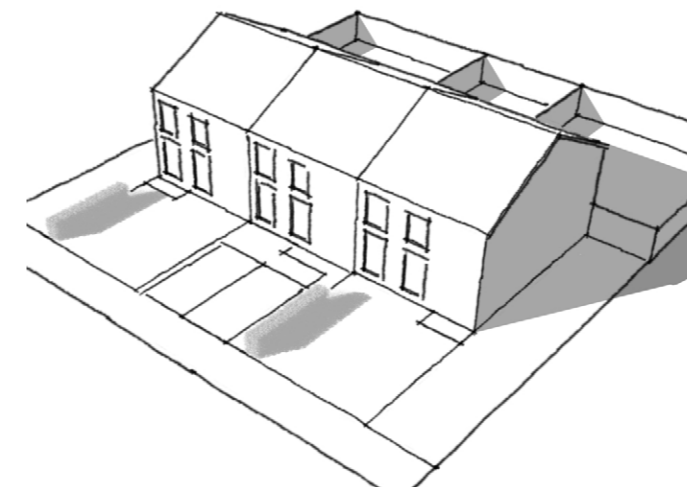


Figure 41: Terraced parking (short runs)

Parking Materials

The type of materials used for parking can impact upon the street scene and should be used to define a change from public to private space

Materials Guidance

- Parking within the curtilage of the dwellings should consist of the following:
 - » Permeable paving.
 - » Tarmac.
 - » Blocks.
- Proposals should provide a different surface material to that proposed on footways and roads to clearly distinguished between public and private space.

BUILT FORM DETAIL (WY)

Urban form

Development should take the form of perimeter block development with housing facing onto streets/public realm, and gardens facing inward creating a clear distinction between public and private space.

Perimeter blocks can be provided in either formal or informal styles.

- Formal perimeter blocks consist of regular rectangular blocks with consistent building lines and street geometry.
- Informal perimeter blocks are irregular in shape with variations in building lines and curved street geometry.

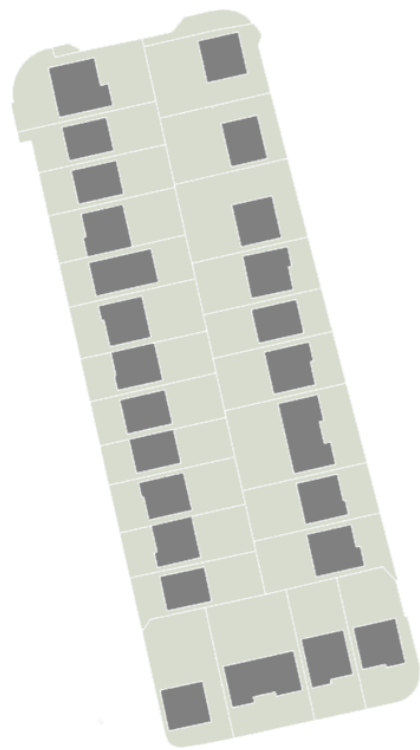


Figure 42: Formal perimeter block

Urban Form Guidance:

- Wynyard is characterised by curved streets. Informal perimeter blocks should be used for development on Boulevard's and Wynyard Neighbourhood Streets to reflect this.
- A mix of formal and informal perimeter blocks should be used for development on Wynyard Streets and Private Drives.
- Informal perimeter blocks should be used on Wynyard Lanes.
- Where development blocks are adjacent to areas of public open space, green wedge or are located on the edge of Wynyard, informal perimeter blocks are encouraged to create a soft interface between urban and rural.



Figure 43: Informal perimeter block

BUILT FORM DETAIL (WY)

Cul-De-Sacs

Proposals which are based upon an interconnected series of Cul-De-Sacs can reduce the overall permeability of areas, increase walking distances and cause disorientation for users.

Code WY28: Cul-De-Sacs

1. Only permitted on Wynyard Streets, Wynyard Lanes and Wynyard Private Drives.
2. Pedestrian permeability must be maintained, e.g. using modal filters (see pg53).

Cul-De-Sac Guidance:

- Cul-De-Sacs within new residential development will only be considered where permeability and way finding are maintained for users.

Building Heights

Within Wynyard there is a variety of building heights from 1.5 - 3 storey. The predominant building height is 2 to 2.5 storey. The changes in building heights throughout Wynyard adds interest and variety to the street scenes. Changes in roof pitch also increase the scale of properties within the area and creates varied roof lines.

Building Heights Guidance:

- The following storey heights should be achieved:
 - » Boulevard - up to 3 storey.
 - » Neighbourhood Street - up to 3 storey.
 - » Wynyard Street - up to 2.5 storey.
 - » Wynyard Lanes - up to 2.5 storey.
 - » Wynyard Private Drives - 2 storey up to 3 storey at key locations.
 - » In locations on the edge of Wynyard up to 2 storey.
- Key locations include:
 - » Entrances.
 - » Corner locations.
 - » View terminus points.
 - » Focal points.
 - » Neighbouring areas for community facilities.

You Said:

- » Cul-de-sacs are disorientating
- » Being able to navigate streets is important.

Housing Mix

The analysis of the Wynyard Area Type highlighted the predominance of detached and semi-detached properties.

Housing Mix Guidance:

- Any new residential development within Wynyard should comprise:
 - » Predominately detached and semi-detached properties.
 - » Terraced properties (short runs up to 4 units) and apartments. These will be considered appropriate on Boulevard's, Neighbourhood and Wynyard Streets, neighbouring key facilities such as a local centre/school, at site entrances and within the retirement village.
- Where access alleys are required for the rear of terraced properties they should:
 - » Not negatively impact upon the rear garden sizes of adjoining properties.
 - » Be clearly defined as private space.
 - » Provide gates at their entrances.

Gateways

Proposals for new residential development within Wynyard should include gateways when accessed directly from Boulevard Streets or the A689.

Code WY29: Gateways

1. Where gateways are proposed they must include:
 - a. Landmark buildings, see Code BF3.
 - b. Landscaping and tree planting.
 - c. Entrance features such as feature walls or public artwork.

BUILT FORM DETAIL (WY)

Materials & Elevational Detailing

It is not the intention of this code to be prescriptive in terms of setting materiality or elevational detailing of proposals or stifle innovative designs.

Materials and Elevational Detailing Guidance

- In accordance with Code BF1, any proposals should have identified the key features of the existing built form, which have a positive influence on the character of the Wynyard Area Type. Proposals should clearly set out how the identified key features and materials are reflected within the architectural detailing of proposals.
- Key elevational details identified as part of the analysis for this code include the following details:
 - » Mini gables.
 - » Gabled front elevations.
 - » Head and cill detailing.
 - » Chimneys.
 - » Brick detailing.
 - » Window surrounds.
 - » Glazed panels.
 - » Dormer windows.
 - » Bay windows.
 - » Elongated contemporary windows.
 - » Canopies.
 - » Porches.
 - » Balconies.
- A variety of building materials are present within the Wynyard area. Any proposals should compliment the existing material pallet.
- Standard house types should be altered to reflect the characteristics of Wynyard.
- The following images are provided as examples of potential elevational treatment and materials in the Wynyard Area Type.



Examples of potential elevational treatment and materials in the Wynyard Area Type.

BUILT FORM DETAIL (WY)

Street scene

Careful consideration needs to be given to the house types proposed and how they will be read within the street.

Code WY30 : Street Scene

1. Within proposed streets there must be the provision of a variety of house types with changes in:
 - a. Elevational detailing
 - b. Materiality
 - c. Massing
 - d. Roof line and/ or ridge height.
2. House types proposed must create a coherent street scene when read as a whole and in relation to the existing housing of Wynyard

Boundary Treatment

Boundary treatments must be used to clearly define changes from public to private space as well as enhancing the character and appearance of the area.

Code WY31: Boundary Treatment


1. Boundary treatments to the front of plots must not exceed 1.2m to ensure that the level of natural surveillance from the associated property can be provided over the street.
2. When proposing boundary treatments to the front of plots, consideration must be given to visibility of vehicles entering and exiting the plot.
3. Plots which adjoin areas of public open space must clearly distinguish between public and private space through the provision of a boundary treatment of either:
 - a. Railings.
 - b. Knee rail.
 - c. Hedging.

Boundary Treatment Guidance:

- Changes in the type of boundary treatment used on different street typologies is encouraged.
- Boundary treatments should not reduce the overall permeability of the development or reduce the level of openness of the street (see image below).
- Appropriate boundary treatments for Wynyard include:
 - » Metal railings.
 - » Hedge.
 - » Low brick wall (brick to compliment the primary material of the residential building).
 - » Brick and metal railing combination.
 - » Knee rail.
- For the established street types the following boundary treatments should be used, where required:
 - » Boulevard - metal railing, low brick wall, brick pillar and railing combination.
 - » Neighbourhood Street - metal railing, low brick wall, brick and metal railing combination.
 - » Wynyard Street - low brick wall, hedge.
 - » Wynyard Lanes - low brick wall, hedge.
 - » Wynyard Private Drive - hedge.
 - » Open Space - Knee rail, metal railing.

WYNYARD MATRIX

Street Type	Boulevard	Wynyard Neighbourhood Street	Wynyard Street	Wynyard Lanes	Wynyard Private Drives
Verge	2m on both sides of carriageway	2m on one side of carriageway or 1m either side	Non Mandatory	Non Mandatory	Non Mandatory
Landscaping	Planting and trees within verge.	Planting and trees within verge.	Landscaping pockets min.50m ²	Landscaping pockets min.50m ²	Front gardens
Street Trees	Yes in verge	Yes in verge	Yes in landscaping pockets	Yes in landscaping pockets	Front gardens, minimum of 2 trees per street.
Minimum Carriageway Width	6.75m	5.5m	5.5m	6.5m	3.7m minimum.
Minimum Footway Width	2m (both sides of carriageway)	2m (both sides of carriageway)	2m (both sides of carriageway)	Non defined in carriageway	N/A
Resting Places	Every 200m (approx)	Every 200m (approx)	Every 200m (approx)	In landscape pockets	No
Cycleway	Yes. 2m segregated or 3m shared.	Non defined in carriageway or 3m shared.	Non defined in carriageway or 3m shared.	Non defined in carriageway	No
Block Style	Formal	Formal	Mix	Mix	Mix
Building Line	Straight	Straight	Curved	Curved	Mix
Setback	2 - 6m	2 - 6m	2m minimum	2m minimum	2m minimum
Variation	3m	3m	3m	3m	6m
Cul-De-Sacs Permitted?	No	No	No	Yes	Yes
House Types	Detached Semi- detached Apartments Terraces	Detached Semi- detached Apartments Terraces	Detached Semi- detached Terraces	Detached Semi- detached	Detached Semi- detached
Boundary Treatment	Metal railing low brick wall. brick pillar and railing combination	Metal railing Low brick wall Brick wall and railing combination None	Low brick wall Hedge None	Low brick wall Hedge None	Hedge None
Building Heights	Up to 3 storey	Up to 3 storey	2 storey up to 3 at key locations	2 storey up to 2.5 at key locations	2 storey up to 3 storey at key locations

 Code requirement

Village Area Type

VILLAGE CODING PLAN

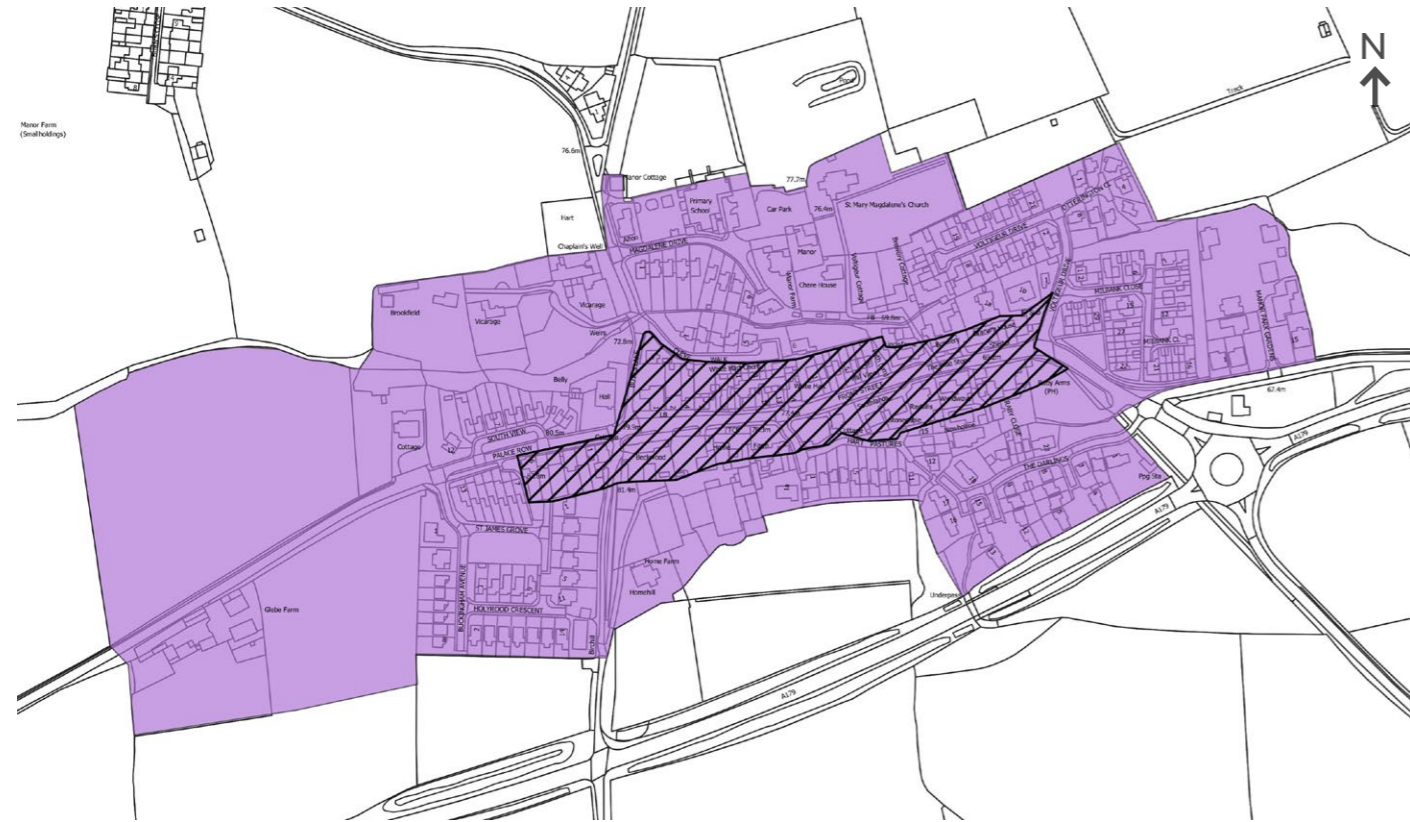


Figure 44: Hart Coding Plan

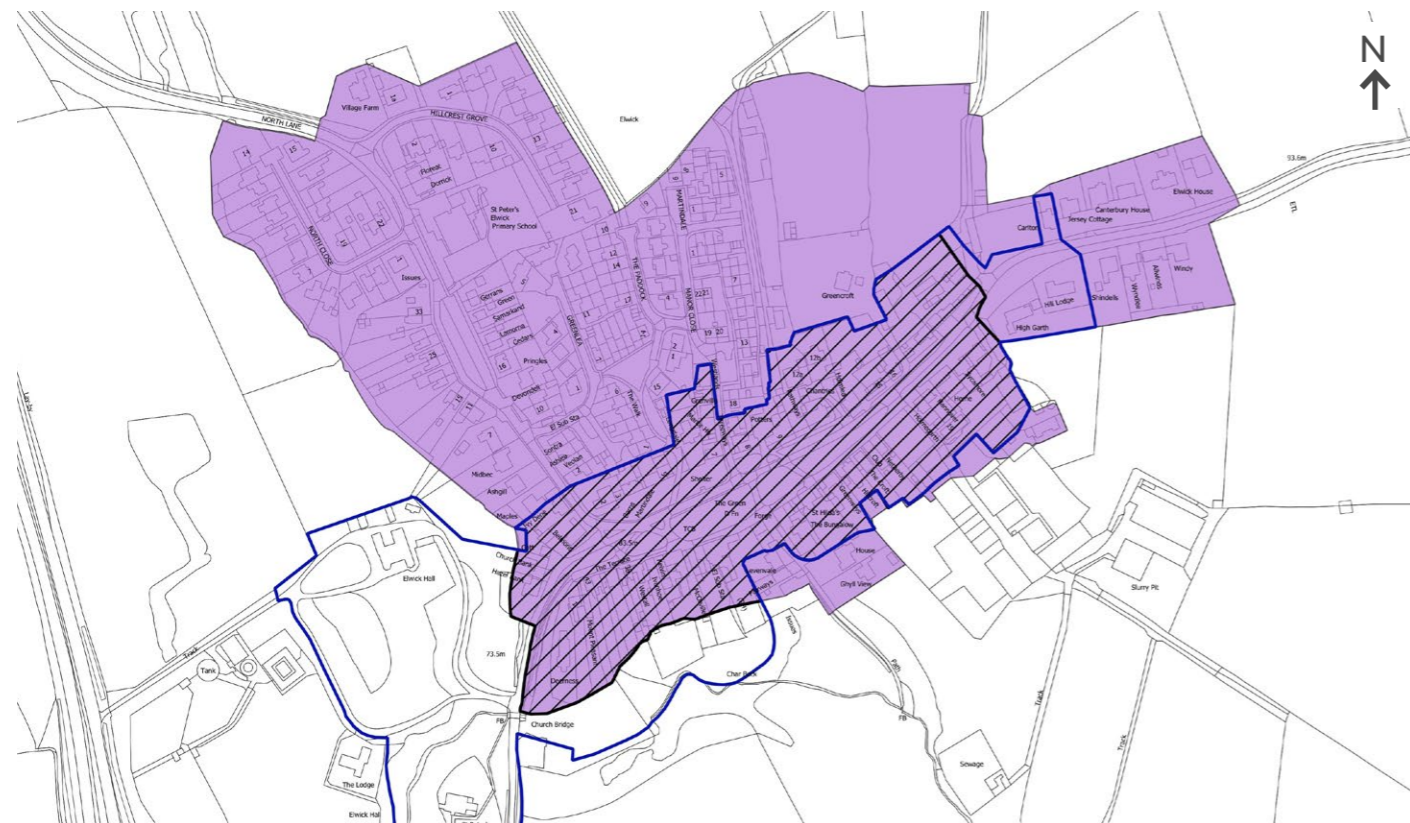


Figure 45: Elwick Coding Plan

VILLAGE CODING PLAN



Figure 46: Newton Bewley Coding Plan

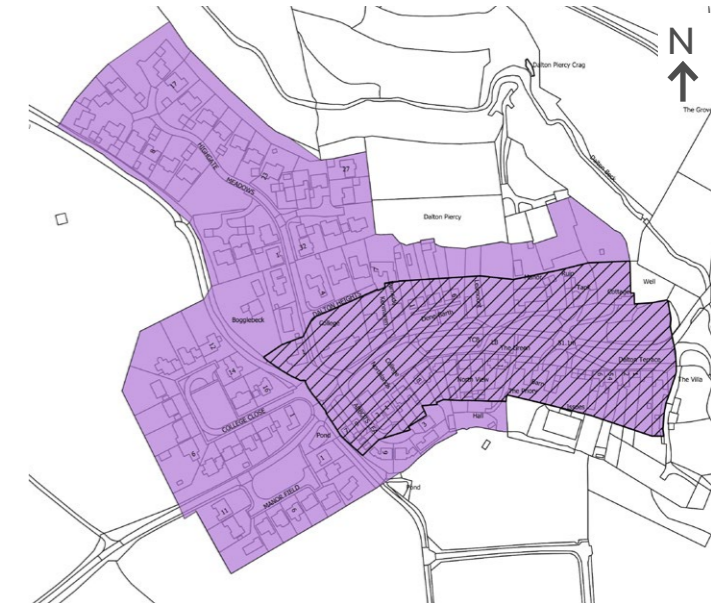


Figure 48: Dalton Piercy Coding Plan

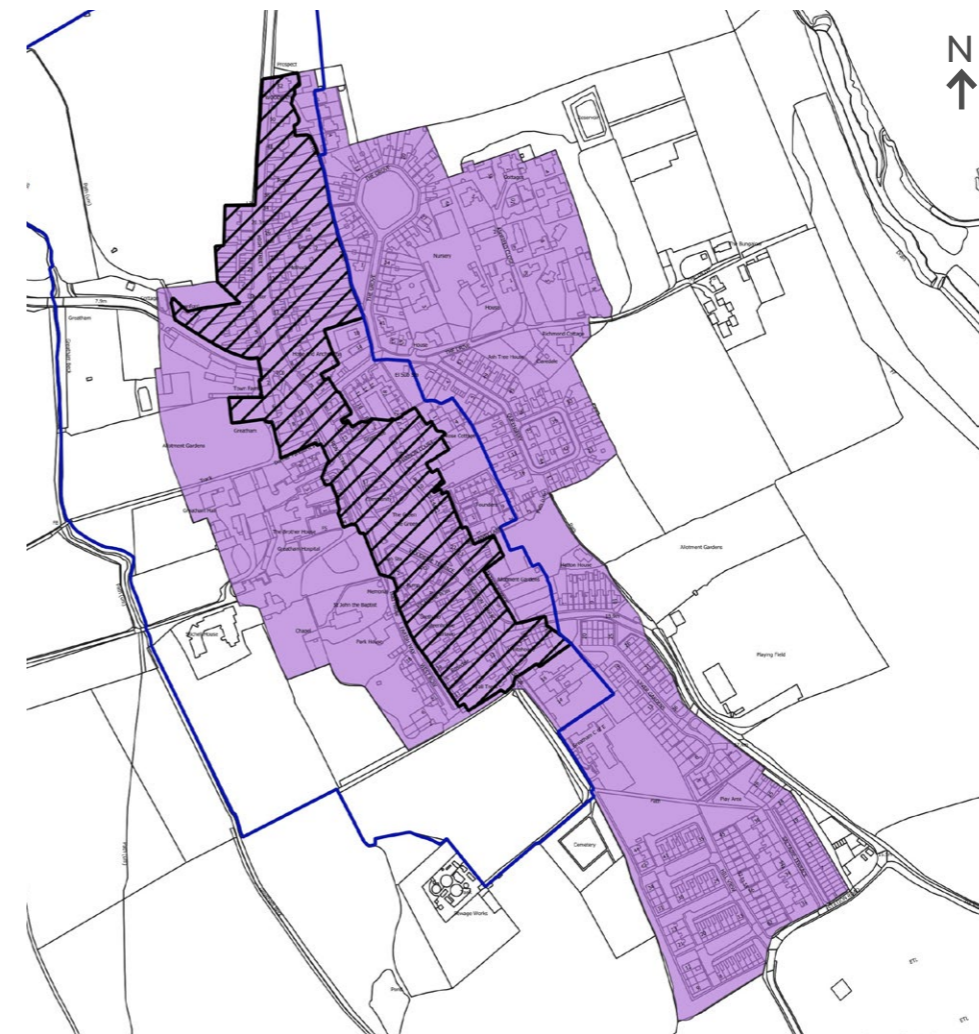


Figure 47: Greatham Coding Plan

- Village Area Type
- Village Core
- Conservation areas

VILLAGE AREA TYPE

There are a number of villages located within the rural area of Hartlepool, these include:

- Dalton Piercy.
- Elwick.
- Greatham.
- Hart.
- Newton Bewley.

Should residential development come forward within these locations, applicants are expected to have regard to this code.

Summary of Design Analysis

The villages have all developed over time and as such, have a variety of styles and characters. Although each village has its own identity there are similar characteristics between the villages.

Street Network

- Central historic linear street with variations in carriageway width along the street length reflecting their historic nature.
- Bus movement accommodated along central street.
- On street unallocated parking along central street, occasionally de marked through change in surface.
- Residential streets lead from the historic centre of the villages, these are typically 5.5m in width and have a footway on either side.
- Residential streets lead from the historic centre of the villages, these are typically 5.5m in width and have a footway on either side.
- Parking at the edge of Villages is predominately in curtilage to the front of the plot for detached / semi-detached properties. For terraced properties parking is provided in courtyards or on-street unallocated.

Built Form

- The historic core of each village is characterised by terraced properties and detached houses.
- Properties are predominantly 2 storey nature, however changes in the era of construction and the original purpose of the building result in variations in scale and ridge height.
- Properties are mainly accessed directly from the pavement with limited or no setback.
- Recessed doors provide a small area of defensible space.
- Higher density areas with enclosed streets due to predominance of terraces and limited set backs.
- At the edge of the villages, properties are typically:
 - » Detached and semi-detached with landscaped front gardens. There are some instances of terracing with parking courts or unallocated on street parking.
 - » Have a varied setback from 1- 6+m with a predominance of over 6m.
 - » 2 storey in nature with limited variety in ridge height.
 - » Medium to low density areas, predominately suburban in nature, with instances of large detached dwellings in grounds.



Village Core street - Elwick

VILLAGE AREA TYPE

Materials

- Predominant building materials include:
 - » Red brick.
 - » Render – Full property cover in various colours.
 - » Stone.
 - » Red roof tiles / clay.
 - » Grey roof tiles / slates.
- Later additions on the edge of the villages have introduced:
 - » Buff brick.
 - » Partial render.

Architectural Detailing

Each village has their own features which make them unique.

- Greatham and Elwick are within conservation areas and have detailed visual assessments.
- Properties at the edge of the villages are typical of the era in which they were built with some features reflecting back to the village core.
- Predominant architectural features include:
 - » Chimneys.
 - » Door surrounds.
 - » Window surrounds.
 - » Head and cill detailing.
 - » Recessed doors.
 - » Sash windows.
 - » Bay windows (edge of village).
 - » Mini gable protrusions (edge of village).
 - » Canopies (edge of village).
 - » Porch (edge of village).



Village Core images

VILLAGE AREA TYPE

Landscaping

- Typically a central village green with tree planting. The surrounding built form fronts onto the village green creating communal spaces.
- Incidental open space with tree and shrub planting along the core street.
- Occasional verge with planting on the edge of the villages.
- Landscaped front gardens (edge of village).



Village Edge images

STREET NETWORK (VL)

You Said:

- » The village identity needs to be retained
- » More trees should be planted.

To reflect the distinct variation between the traditional village core and the later expansion at the edge of the villages, the Village Area Type codes will reference Village Core or Village Edge. These areas are defined on the Coding Plan.

The Village Core relates to the historic centres of the villages and the Village Edge to the post-war developments leading away from the Village Cores.



Tree planting within street - Derwenthorpe, York

Street Network

The street network within the village core consist of existing linear streets leading to residential streets within the Village Edge Area Types. The existing built form has a strongly established building line with a limited set back.

Code VL1: Village Core Street

1. New residential development, which is located fully or partially, within the Village Core must:
 - a. Provide vehicle access directly from the Core Street, where required.
 - b. Provide a pedestrian path, where one does not currently exist. The new footpath must connect into the existing pedestrian movement network along the Core Street.
 - c. Provide a verge with tree planting where the Character Analysis (see Code BF1) has identified this is a characteristic of the Village Core.

Code VL2: Core Street Building Line and set back

1. New residential development must:
 - a. Follow the existing building line.
 - b. Provide frontage onto the street.
 - c. Be accessed directly from the footway.

Village Core Street Guidance

- Parking within the Village Core should follow the prevailing method and will be considered on a case-by-case basis.
- Where the existing building line does not provide a set back:
 - » A variation of up to 0.5m from the existing building line should be considered to enable an area of defensible space or;
 - » Where a set back is not achievable a recessed doorway should be provided to create an area of defensible space.

STREET NETWORK (VL)

It is anticipated that residential development will be located at edge of existing villages and will take the form of small scale extensions, as is the case for the housing allocations at the Villages of Elwick and Hart.

Code VL3: Street Hierarchy

1. Any new residential development which consists of more than one street must include a street hierarchy in accordance with Codes VL4 - VL11.

Village Street

The aim of a Village Street is to provide the main pedestrian, cycle and vehicle connections through a development proposal, whilst also providing the main connection into the existing village movement network.

Code VL4: Village Street

1. Village Streets must:
 - a. Provide a 2m verge as a minimum, on one side of the carriageway. This can alternate between sides but must be provided for the entirety of the street.
 - b. Incorporate street tree planting (see Code N10).
 - c. Provide a verge for at least 70% of the total street length taking into consideration driveway crossings. E.g. for a street 100m in length the verge must be provided for at least 70m.
 - d. Provide a resting place in accordance with Code N6.
 - e. Ensure where visitor parking is provided that a verge is retained for 70% of the total street length.
 - f. Not provide barriers alongside verges or footways. This enables permeability and reduces visual clutter.
 - g. Provide a carriageway width of 5.5m.
 - h. Provide a minimum footway width of 2m on both sides of the carriageway.
2. Cul-de-sacs are not encouraged on Village Streets, however, where these are required due to the sites location or limited availability of access points, pedestrian permeability must be maintained.

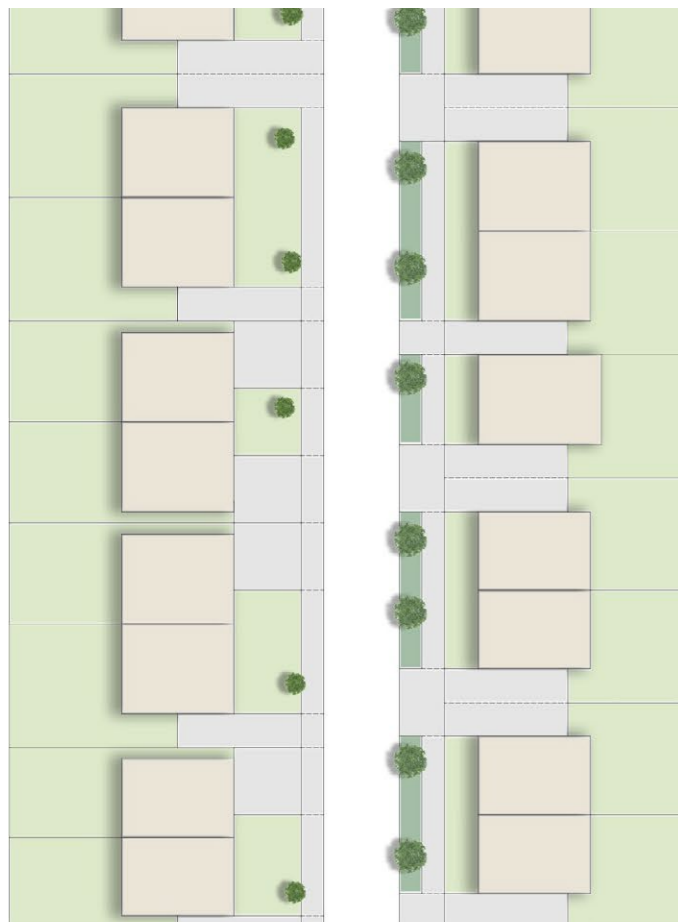


Figure 49: Example of Village Street with verge

STREET NETWORK (VL)

Code VL5: Village Street Connections

1. To ensure the creation of sustainable developments within the rural area pedestrian connections must be provided:
 - a. Into the Village Core.
 - b. To public transport routes, where available.
2. Pedestrian and cycle connections must connect into existing Public Rights of Way or Permissive Routes in accordance with Codes M2 - M7.

Code VL6: Village Street Setback

1. Whilst allowing for the separation distances set out in HB1, a minimum setback of 2m must be provided from the pavement.
2. Variation: the front face of buildings must not vary more than 3m from the building line.

Village Street Guidance

- A 1m verge will be considered where evidence can be provided that site restrictions do not allow for a 2m verge. In these instances, tree planting will still be required and underground rooting systems should be utilised.
- Drainage features such as rain gardens are encouraged in verges (see Code N11).
- The route of Village Streets should follow desire lines identified in the Site Analysis of the site (see Code SA1).
- Crossings should be located on or close to desire lines so that pedestrians find them convenient and pleasant to use.
- Cycle routes should be provided through sites, preferably through green spaces and connect into existing cycle networks.
- The level of setback provided should be consistent with the character analysis undertaken through Code CA1 and Code BF6.
- Where the proposal is within view of the Village Core, a minimal setback should be provided to correspond with the village characteristics.

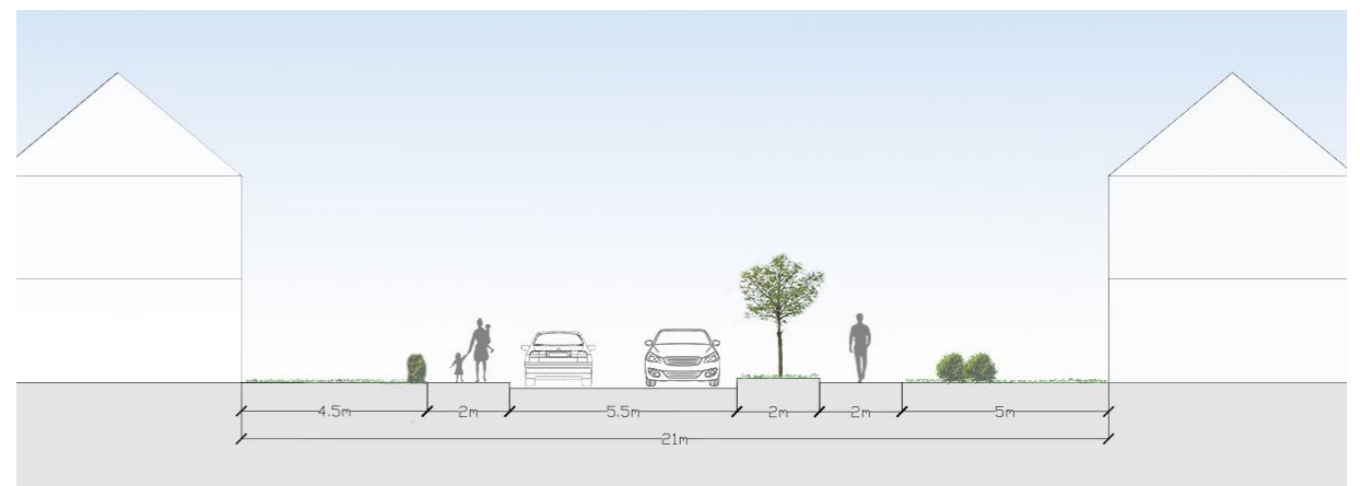


Figure 50: Village Street cross-section

STREET NETWORK (VL)

Village Lane

Village Lanes will lead from the Village Street, providing access into lower traffic flow areas. They support active travel and encourage the use of streets as social spaces. Village Lanes can take the form of standard highway or shared surfaces.

Code VL7: Village Lane

1. Provide a Village Lane as either a standard highway or shared surface.
2. Standard highway design must comprise of
 - a. A carriageway width of 5.5m with a 2m foot way provided on both sides of the carriageway.
3. Shared surface carriageway design must comprise of
 - a. A minimum carriageway width of 6m + 0.5m hardened maintenance margin.
 - b. And include a 1.8m service corridor within the carriageway.
 - c. A change in surface material complementary to the village character to distinguish the street type and signal to users that a change in behaviour is required.
4. To ensure the provision of street trees that are not within private ownership, landscaping pockets of a minimum of 50m² must be provided within Lanes. See code N5.

5. Resting places to be provided every 200m (approx) see Code N6.
6. Pedestrian only connections must be provided between neighbouring Village Lanes, e.g. modal filters (see page 51).

Code VL8: Village Lane Setback

1. Whilst allowing for the separation distances set out in HB1, a minimum set back of 2m must be provided from the carriageway.
2. Projections are permitted.
3. Variation: the front face of buildings must not vary more than 3m from the building line.



Example shared surface Lane with planting.



Figure 51: Village Lanes cross-section

STREET NETWORK (VL)

Village Lane Guidance

- Cycling should be provided for within the carriageway
- Crossings should be located on or close to desire lines so that pedestrians find them convenient and pleasant to use.
- The design of the street should ensure that parking is controlled and only taking place in designated areas.
- Visitor parking should be clearly marked.
- Tree planting should be utilised within the street to break up front of plot parking. This can be achieved through:
 - » Landscaping pockets or
 - » In landscaped areas between parking bays.
- Tree planting can be used as a means of traffic calming through diverting the carriageway.
- Changes in material can include block paving or coloured tarmac for the whole of the street or as a 2m transition strip.
- Block paving and coloured tarmac will be subject to a commuted sum for maintenance.



Figure 52: Village Lanes example layout

Private Drives

Provide connections for a small group of properties and are in private ownership. The streets are encouraged as private social spaces for the properties they serve and have no through traffic.

Code VL9: Private Drives

1. Must serve no more than 5 properties.
2. A maximum of 25m in length.
3. Minimum of 3.7m in width.
4. Bin collection points to be located a maximum of 25m from the adopted highway.
5. A change in surface material must be provided to distinguish the street type and enhance the character of the street.

Code VL10: Private Drive Landscaping

1. Each property on a Private Drive must be provided with a landscaped front garden following the house type plot ratio in Code VL14.
2. Tree planting must be provided within front gardens for at least 2 properties per Private Drive.

Code VL11: Private Drive Setback

1. Whilst allowing for the separation distances set out in HB1, a minimum set back of 2m must be provided from the carriageway.
2. Projections such as bay windows, porches and balconies are permitted when not located adjacent the Village Core.
3. Variation: the front face of buildings must not vary more than 3m from the building line.

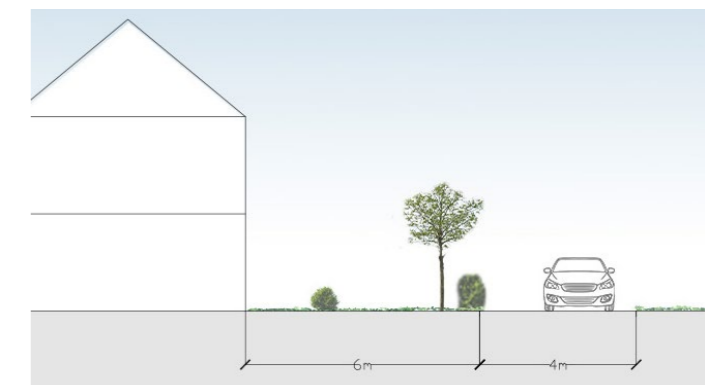


Figure 53: Private Drive cross-section

PARKING (VL)

You Said:

- » The design of parking in new housing could be better and inappropriate parking should be discouraged

The following parking codes and guidance only apply to residential development outside of the Village Core.

Parking Provision

A balance is to be made within residential areas on the provision of car parking and the creation of attractive streets.

Parking within new residential developments must sit comfortably within the street scene and not detract from the overall visual aesthetic of the residential area. To ensure this is achieved within new residential developments the following codes must be adhered to.

Code VL12: Parking Provision Standards

1. Development proposals must provide as a minimum the parking standards as set out below.

Unit Size	Number of spaces
1 bedroom	2
2 bedroom	2
3 bedroom	2
4 bedroom	3
5 bedroom+	3
Flatted development	1.5

2. The above parking standards adhere to the Highways Design Guide & Specification – Residential and Industrial Estates Development (amended May 2021).

Parking Methods

There are a range of parking solutions which are considered appropriate for the Village Area Type, these include:

- In curtilage to the front of properties.
- In curtilage to the side of properties.
- Garage.
- On street allocated and unallocated.
- Courtyard.

The parking dimensions specified within the following codes align with the Highways Design Guide & Specification – Residential and Industrial Estates Development (amended May 2021).

Code VL13: Parking Methods

1. A range of parking methods must be provided with a maximum of 60% of parking proposed to the front of properties. This includes in curtilage (driveway) and allocated on-street parking bays.

Parking to plot ratio

Car parking has a negative impact on the street scene when there is also a lack of landscaping and greenery within the street.

Code VL14: Parking to Plot Ratio

2. The following minimum requirements must be adhered to:
 - a. Detached: A minimum of 40% of the front of the plot must be provided as landscaped garden.
 - b. Semi detached: A minimum of 30% of the front of the plot must be provided as landscaped garden.

Visitor Parking

The provision for visitor parking is encouraged within proposals to prevent inappropriate parking.

Visitor Parking Guidance

- Where visitor parking is provided it should:
 - » Be spread throughout the proposal.
 - » Not reduce the provision of verge in accordance with the relevant street type codes.
 - » Be screened by planting when located adjoining areas of open space.

PARKING (VL)

In Curtilage - Front of Property

Providing parking in plot to the front or side of properties was considered the most popular during consultation. To ensure that parking does not dominate the street scene and the creation of green streets, the following code and guidance are provided.

Code VL15: In Curtilage - Front of Property

1. No more than 4 consecutive front of plot spaces.
2. Where more than 4 spaces are required, an area of soft landscaping with tree and / or hedge planting measuring a minimum of 5m in length x 2m in width must be provided after every 4th space. This includes front gardens.
3. Single space dimensions: 6m in length x 3m in width.
4. Double space dimensions: 6m in length x 5.5m in width.
5. A maximum of 60% of parking proposed can be to the front of properties (see Code VL13)

Front of Property Guidance

- The inclusion of hedge planting to the side of parking bays is encouraged to act as screening.

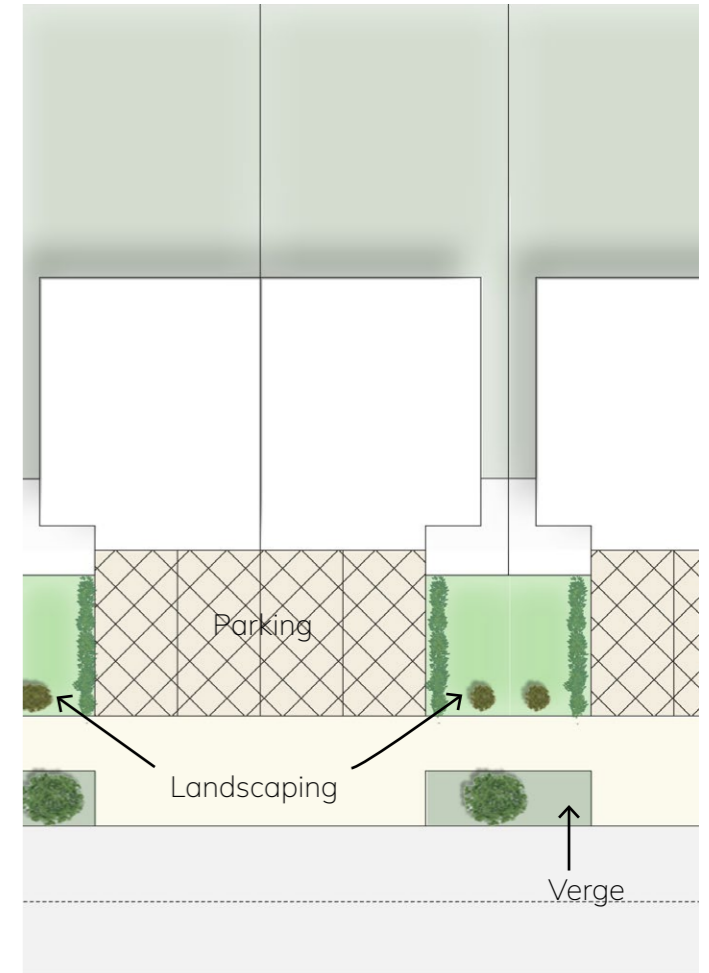


Figure 54: Front of plot parking and landscaping

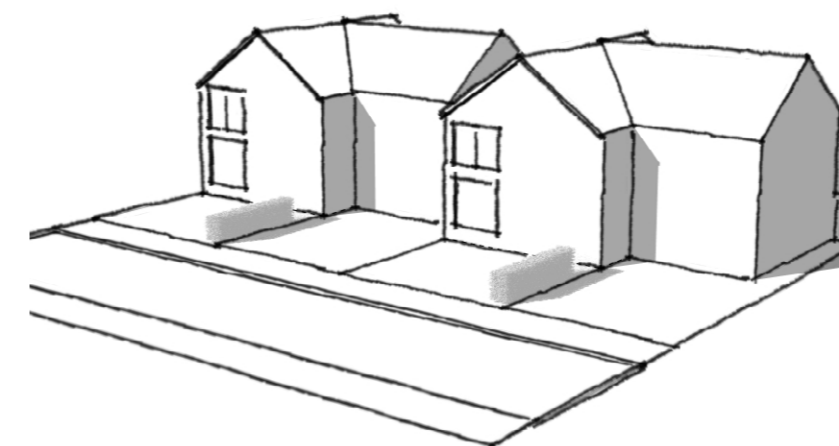


Figure 55: Front of plot parking



Screening through planting

PARKING (VL)

In curtilage - Side of Property

Providing parking to the side of properties enables the provision of front gardens and tree planting and will also reduce the dominance of parking on the street scene.

Code VL16: In Curtilage - Side of Property

1. Single space dimensions: 6m in length x 3m in width.
2. Double space dimensions: 11m in length x 3m in width.

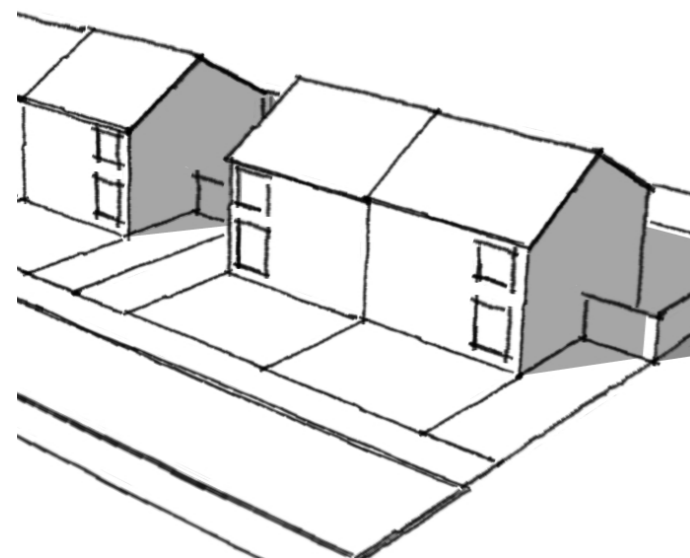


Figure 56: Side of plot parking

Garages

Public consultation indicated that garages are seen as a desirable form of parking in new residential developments. To ensure that they do not impact upon the street scene and encourage active frontages, the following code and guidance are provided:

Code VL17: Garages

1. To count toward parking provision garages must have minimum internal dimensions of 6m x 3m.
2. Detached garages must be a minimum of 2m behind the building line of the associated residential dwelling.
3. No more than 2 consecutive garages. This applies to both integrated and detached.

Garage Guidance:

- Integrated garages can impact upon the street scene through increasing the levels of inactive elevations. It is therefore important to ensure that the level of integrated garages does not dominate the street scene.
- Integrated garages should be recessed with the residential aspect of the dwelling being the primary focus of elevations.

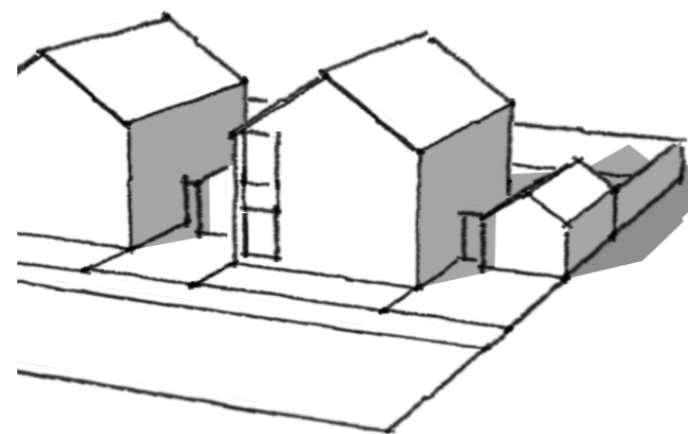


Figure 57: Detached garage

PARKING (VL)

Courtyard Parking

The provision of courtyard parking can be a useful parking method in providing allocated on street parking in a positive setting.

It is important to ensure that courtyards are designed appropriately and must be in the form of small courtyards with landscaping, feature paving and street furniture.

Code VL18: Courtyard Parking

1. Proposals for courtyard parking must incorporate the following:
 - a. Safe pedestrian and cycle movement.
 - b. Natural surveillance from adjoining houses, or by buildings entered from the parking area.
 - c. Landscaping as either; a central space with parking around or, as landscaped areas measuring 2m in width and 4.8m in length occurring after every 4th space and include tree / hedge planting.
 - d. Central landscaped areas should include planting and be designed as social spaces.
 - e. Provide a careful balance between the desire of car owners to park as near to their homes as possible and the need to maintain the character of the overall setting.
 - f. Prevent inappropriate parking through careful placement of street furniture / planting.



Courtyard Parking - Hatlepool

On Street Parking

Providing parking within the street can take the form of either allocated or unallocated space.

Code VL19: On Street Allocated & Unallocated

1. Proposals for on-street parking must:
 - a. Be 2.4m in width x 6m in length, for central spaces. End spaces must be 2.4m in width by 4.8m in length..
 - b. Provide a maximum of 4 consecutive spaces.
 - c. Provide an area of soft landscaping with tree planting measuring a minimum of 2.4m in width x 4.8m in length after every 4th space. Where more than 4 spaces are provided.
 - d. Clearly define the space as private for allocated bays.



On-street parking with landscaping - St Andrews

PARKING (VL)

Terraced Properties Parking

Where terraced properties are provided, parking within courtyard is the preferred method of parking for the Village Area Type. The provision of front of plot parking for all properties can have a negative impact on the street scene and a reduction in the provision of front gardens.

Code VL20: Terraced Properties Parking

1. Parking for short runs of terraced properties (up to 4 units) must consist of:
 - a. End properties provided with side of plot parking and front gardens.
 - b. Central units provided with front of plot parking (see image below).
2. Parking for more than 4 terraced properties must:
 - a. Take the form of courtyard parking (see Code VL18).
3. Rear parking will not be considered appropriate.



Figure 58: Terraced parking (short runs)

Parking Materials

The choice of materials used for areas of parking will have an impact upon the over all street scene created. The over-use of black tarmac can create an urban feel and is therefore not encouraged in the Village Area Type.

Materials Guidance

- Parking within the curtilage of the dwellings should consist of the following:
 - » Permeable paving.
 - » Blocks.
 - » Coloured tarmac (Village Edge only).
- Proposals should provide a different surface material to that proposed on footways and roads to clearly distinguish between public and private space.

BUILT FORM DETAIL (VL)

You Said:

- » New housing should be terraced around a public green.

Urban form

Development should take the form of perimeter block development with housing facing onto streets/public realm and gardens facing inward, creating a clear distinction between public and private space.

Perimeter blocks can be provided in either formal or informal styles.

- Formal perimeter blocks consist of regular rectangular blocks with consistent building lines and street geometry.
- Informal perimeter blocks are irregular in shape with variations in building lines and curved street geometry.

Code VL21: Urban Form

1. Built form must successfully integrate into the existing developed area by:
 - a. Providing frontage onto the public realm.
 - b. Follow the existing building line (see Codes BF4 and BF5)
 - c. Create perimeter block developments.
 - d. Where the development will be adjoining existing built form, developments must create perimeter block style layouts, where feasible, and ensure separation distances are maintained (see Code HB1).

Urban Form Guidance:

- The Site and Character Analysis (Code SA1 & BF1) as well as any relevant built heritage analysis (Code BF6) should inform the urban form of proposals.
- Formal perimeter blocks should be used for Village Streets. Informal perimeter blocks should be used for development on Village Lanes, Village Private Drives and adjacent to areas of public open space.
- The analysis has identified village green's as a key feature of the Area Type. New residential developments should provide a central green space with the built form providing frontage and activation over it.

Building Line

The shape of the building line will contribute to the character of the area, the Village Area Type is characterised by predominance of straight building lines, however more recent developments have create curved building lines toward the rural area.

Frontage Guidance:

- A mix of straight and curved building lines should be created. Curved building lines should be focused on the rural edge or adjacent to green spaces.
- Houses with side parking can be placed closer to the highway, however this should not be at the detriment of front gardens, tree planting and residential amenity.



Housing around green, Nantwich, Cheshire



Housing around green, Sandycote, Exeter

BUILT FORM DETAIL (VL)

Building Heights

Within the Village Core there is a predominance of 2 storey properties, however, there is variation in the overall scale and ridge height between the properties. At the edge of the villages there is a variety of building heights between one and 2.5 stories. The changes in scale throughout the villages adds interest, creates varied roof lines and variety to the street scenes.

Code VL22: Building Heights and Roofscape

1. New residential dwellings adjacent to or within the Village Core must
 - a. Be consistent with the existing heights within the direct vicinity.
2. Changes of scale must be provided through variation in storey height and ridge height.
3. A varied roof line must be provided.

Building Heights Guidance:

- Properties within the Village Area Type should be up to 2 stories in height. 2.5 will be considered in key locations or to reflect the Character Analysis.
- Key locations include:
 - » Entrances.
 - » Corner locations.
 - » View terminus points.
 - » Focal points.
 - » Neighbouring areas for community facilities.



Figure 59: Variations in scale and detailing, Greatham

Housing Mix

The core of the villages is characterised by a predominance of terraced properties and detached farm houses. The village edge is characterised by semi-detached and detached properties.

There are elements of terraced properties at the edge of villages, particularly at Elwick and Greatham, which better reflect the housing mix of their Village Core's.

Code VL23: Housing Mix

1. A mix of detached, semi detached, terraced, bungalows and apartments must be provided to reflect the varied nature of the village.
2. Where proposals will adjoin a Village Core the house types must be consistent with those existing. For example, where terraced properties are adjoining the site, the proposal should include terraced properties closest to the existing built form with detached properties located toward the edge of the proposal.

Housing Mix Guidance

- Terraced properties should be designed with:
 - » Alleys for rear access which do not negatively impact upon the rear garden sizes of adjoining properties and should be clearly defined as private space. Gates must be provided at their entrances.

BUILT FORM DETAIL (VL)

You Said:

- » History of the buildings is a valued quality of the villages.
- » Buildings should be varied in appearance and have detailing.

Materials and Elevations

It is not the intention of this code to be prescriptive in terms of setting materiality or elevational detailing of proposals or to stifle innovative designs.

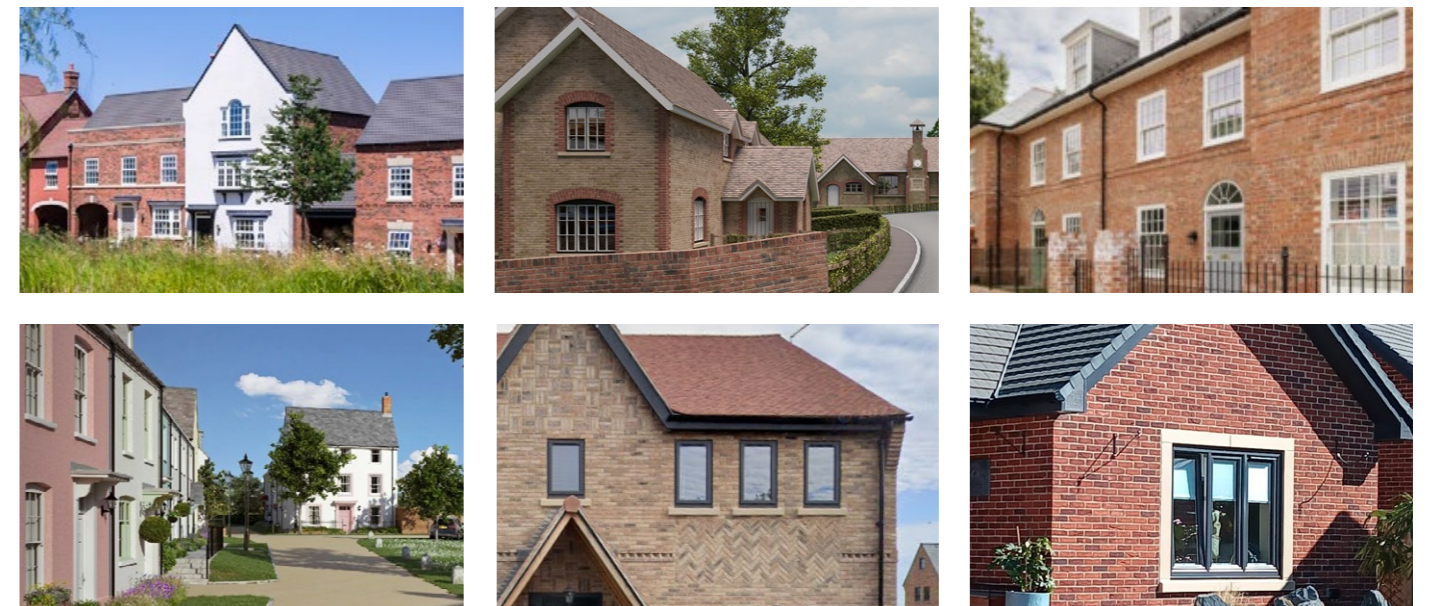
Proposals within the Village Area Type must sit comfortably with and reflect the distinctive character of the surrounding built form.

Code VL24: Elevational Treatment

1. House types must reflect the positive architectural detailing of the associated Village Core as identified through Code BF1.
2. Standard house types must be altered to reflect the characteristics of the Village.
3. Variety in elevational treatments and materials must be provided within the same street type to replicate the varied nature of the existing village streets.
4. Proposals within the setting of built heritage assets must be informed by the relevant CAMPs, Character Appraisals and/ or visual assessments.

Elevational Guidance

- In accordance with Code BF1 any proposals should have identified the key features of the existing built form, which have a positive influence on the character of the Village Area Type. Proposals should clearly set out how the identified key features and materials are reflected within the architectural detailing of proposals.
- Key elevational details of the Village Area Type which can be referenced within design proposals include:
 - » Window proportions and pattern.
 - » Head and cill detailing.
 - » Door surround detailing.
 - » Materials.
 - » Pattern of materials.
- Elevational treatment should sit comfortably when read within the context of the street.
- The below images are provided as examples of potential elevational treatment and materials in the Village Area Type.



Examples of potential elevational treatment and materials in the Village Area Type

BUILT FORM DETAIL (VL)

Boundary Treatment

Boundary treatments must be used to clearly define changes from public to private space as well as enhancing the character and appearance of the area.

Code VL25: Boundary Treatment

1. Where a boundary treatment is required for proposals adjacent to or within the Village Core. This must be a brick or stone wall up to 1m in height.
2. Boundary treatments must be used sparingly in the Village Edge unless the Character Assessment (Code BF1) has identified them as a defining feature of the area or for locations where definition between public and private space is required.
3. Boundary treatments to the front of plots must not exceed 1.2m to ensure that the level of natural surveillance from the associated property can be provided over the street.
4. When proposing boundary treatment to the front of plots consideration must be given to visibility of vehicles entering and exiting the plot.
5. Plots which adjoin areas of public open space must clearly distinguish between public and private space through the provision of a boundary treatment of either:
 - a. Knee rail.
 - b. Hedging.
 - c. Low wall.

Boundary Treatment Guidance:

- Where boundary treatments are provided, changes in the type of treatment used between street typologies is encouraged.
- Boundary treatments should not reduce the overall permeability of the development or reduce the level of openness of the street.
- Appropriate boundary treatments for the Village Edge include:
 - » Hedge.
 - » Low brick or stone wall (material to compliment the primary material of the village).
 - » Post and rail timber fence.
 - » Knee rail.
- For the established street types the following boundary treatments should be used, where required:
 - » Village Street – low wall, hedge.
 - » Village Lanes - low wall, hedge.
 - » Private Drive - hedge.

RURAL EDGE (RE)

Rural Edge

Proposals within the Village Area Type will adjoin the rural edge / open countryside of the borough. It is therefore important to ensure that future residential developments provide an appropriate response which enables developments to graduate into the rural landscape and respond to potential views.

Code RE1: Rural Edge Street Types

1. Street typologies must include Village Lanes and Village Private Drives. Village Streets will not be permitted on the rural edge.

Code RE2: Rural Edge Parking

1. The appropriate parking solutions for the rural edge include:
 - a. Front of plot with a front garden following the ratio's set within Code VL14.
 - b. Side of plot.
 - c. Garage.

Code RE3: Rural Edge Connections

1. To encourage active travel and promote health and well-being, connections by modes of active travel must be provided into the countryside.
2. Pedestrian and cycle connections must connect into existing Public Rights of Way or Permissive Routes in accordance with Codes M4 - M7.

Code RE4: Rural Edge Frontage

1. Properties must provide frontage onto the rural edge. Where side elevations are proposed these must be corner turning properties with dual frontages (see Code BF2).
2. Presenting rear elevations and boundary fencing onto the rural edge will not be considered an appropriate response.

Rural Edge Guidance:

- Streets should be more curved in nature to create a graduation between the urban and rural characters.
- Where Private Drives are proposed on the rural edge these should not reduce the overall permeability of proposals. Where the predominant use of Private Drives prevents the movement of users, pedestrian connections to an adoptable standard should be placed within a verge or adjacent green space.
- Incorporate a tree lined landscaped edge to soften the visual impact between the built form and countryside.

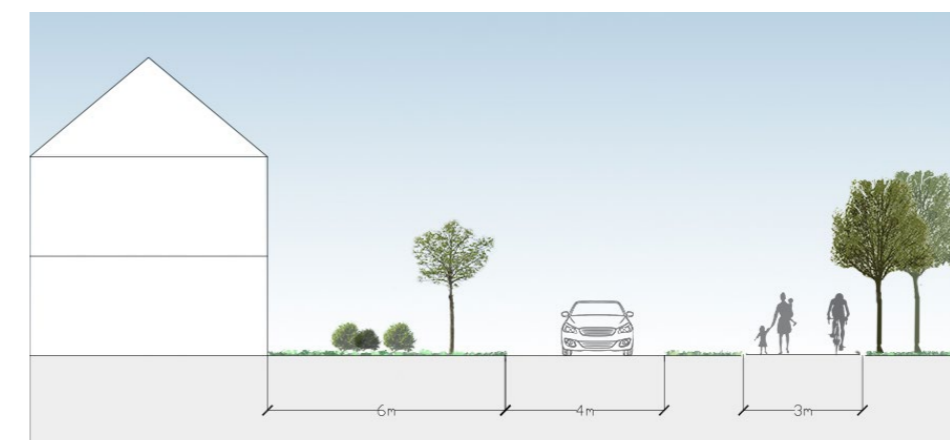



Figure 60: Rural Edge cross-section

VILLAGE MATRIX

Street type	Village Street	Village Lane	Village Private Drives
Verge	2m on one side of carriageway.	Non Mandatory	Non Mandatory
Landscaping	Planting and trees within verge.	Landscaping pockets min.50m ²	Front gardens
Street Trees	Yes in verge	Yes in landscaping pockets	Front gardens, minimum of 2 trees per street.
Minimum carriageway width.	5.5m	5.5m or 6.5m shared surface.	3.7m minimum.
Minimum footway width.	2m (both sides of carriageway)	2m (both sides of carriageway) or shared surface	N/A
Resting Places	Every 200m (approx)	Every 200m (approx)	No
Cycleway	Non defined in carriageway or defined within green space.	Non defined in carriageway.	No
Block Style	Formal	Informal	Informal
Building Line	Mix	Mix	Mix
Setback	Minimum of 2m	Minimum of 2m	Minimum of 2m
Variation	up to 3m	up to 3m	up to 3m
Cul-de-Sacs Permitted?	Yes, where appropriate	Yes	Yes
Permitted on Rural Edge?	No	Yes	Yes
House Types	Mix	Mix	Mix
Boundary Treatment	Low wall Hedge None	Low wall Hedge None	Hedge None
Building Heights	Variation in roof line 2 storey up to 2.5 at key locations	Variation in roof line 2 storey up to 2.5 at key locations	Variation in roof line 2 storey up to 2.5 at key locations

 Code requirement

Appendices

APPENDIX 1: LIST OF RELEVANT POLICIES

Relevant Hartlepool Local Plan Policies (2018)

- Policy CC1: Minimising and adapting to Climate Change
- Policy CC2: Reducing and Mitigating Flood Risk
- Policy INF1: Sustainable Transport Network
- Policy INF2: Improving Connectivity in Hartlepool
- Policy QP3: Location, Accessibility, Highway Safety and Parking
- Policy QP4: Layout and Design of Development
- Policy QP5: Safety and Security
- Policy QP6: Technical Matters
- Policy QP7: Energy Efficiency
- Policy HE1: Heritage Assets
- Policy HE3: Conservation Areas
- Policy HE4: Listed Buildings and Structures
- Policy HE5: Locally Listed Buildings and Structures
- Policy NE1: Natural Environment
- Policy NE2: Green Infrastructure
- Policy NE4: Ecological Networks

Relevant Rural Neighbourhood Plan Policies (2018)

- Policy GEN2: Design Principles
- Policy H4: Housing in the Countryside
- Policy NE1: Natural Environment
- Policy NE2: Renewable and Low Carbon Energy
- Policy HA1: Protection and Enhancement of Heritage Assets
- Policy HA2: Protection and Enhancement of Conservation Areas
- Policy HA3: Protection and Enhancement of Listed Buildings
- Policy HA4: Protection and Enhancement of Locally Important Buildings

APPENDIX 2: GLOSSARY

Term	Definition
Accessibility	The ability of people to move around an area and reach places and facilities.
Active frontage	The front of a buildings with windows and doors onto the space to generate activity and engagement between the inside of the building and the space outside.
Allocated	Lands which has been identified on the Local Plan (2018) for a specific type of development.
Amenity	The pleasant aspects of a location which contributes to its overall character and its enjoyment.
Amenity space	The outside space associated with a home or homes. It may be private or shared, depending on the building it serves.
Area Types	Parts of the local area that share common features and characteristics. For example, a suburban area type might bring together a number of different housing estates with common densities, heights, building line, party wall condition etc, under the umbrella term "outer suburbs".
Boundary treatments	The way the edges of developments are designed and enclosed. For example, the boundary treatment around a house will usually be a fence, wall or hedge.
Building line	The line formed by the frontages of buildings along a street. The individual features or appearance that give an identity to an area
Built form	The three-dimensional pattern or arrangement of development blocks, streets, buildings and open spaces.
Context	The physical and social setting of a site or area, including factors such as traffic, activities and land uses as well as landscape and built form.
Connectivity	The degree to which something has connections, for example road connections, footway connections or the connection between area A and area B.
Conservation area	An area of special historical and/or architectural interest which is designated by the Local Planning Authority as being important to conserve or enhance.
Defensible space	Areas of private space that have some form of demarcation so that people know the space is private and not public. This can often be a low fence or wall around a front garden.
Gateway	The design of a building, site or landscape to symbolise an entrance or arrival to a special district.
Green space	Grassed areas with landscaping within housing estates. It enhances the visual appearance and provides opportunities for informal recreational activities close to home such as; well-observed children's play, exercising dogs, and an area for walking and to relax.
Green infrastructure	All of the natural and managed green spaces, features and water bodies that together make up a multifunctional network across rural and urban areas. The network includes green spaces such as parks and gardens on private or public land, and green links between spaces such as hedgerows and rights of way, as well as features such as blue corridors, green roofs/walls and ponds.

APPENDIX 2: GLOSSARY

Term	Definition
Habitable room	Generally includes a room used for dwelling purposes including a kitchen but not a bathroom or utility room.
Heritage asset	A building, monument, site, place, area or landscape positively identified as having a degree of significance meriting consideration in planning decisions. Heritage assets are the valued components of the historic environment. Heritage assets are of two types, 1. Those which have been recognised as being nationally important and have been designated as such; these are designated heritage assets, and 2. Heritage assets that have not been designated, nevertheless some of which may be of national importance.
Landmarks	Buildings structures and spaces which create distinct visual orientation points that provide a sense of location to the observer within the neighbourhood.
Legibility	How easy it is for people to understand and find their way around a place and how memorable it is.
Natural surveillance	The ability of people to be seen from surrounding buildings or space without the need for CCTV.
Rainwater harvesting	The collection of water directly from the surface it falls on e.g. a roof or hard standing area. This water would otherwise have gone directly into the drainage system or been lost through evaporation and transpiration. Once collected and stored it can be used for flushing toilets, watering gardens and washing clothes using a washing machine.
Renewable energy	Those energy flows that occur naturally and repeatedly in the environment from the wind, the fall of water, the movement of the oceans, from the sun and also from biomass.
Roofscape	A view of roofs.
Scale	The impression of a building, or parts of a building when seen in relation to its surroundings.
Set-Back	The distance that buildings are set back from the pavement.
Street scene:	The appearance of all the elements of a street, including the carriageway, pavement, street furniture, planting, and the buildings or structures along its edges, particularly the composition of buildings on each side of the street.
Sustainable Drainage Systems (SuDS)	Efficient drainage systems which seek to minimise wastage of water including the use of groundcover to enable maximum penetration of run off into the ground and, where appropriate recycling grey water within the development.
Variation:	The extent to which buildings can be set forward or back from the building line.